

CAP MEETING – 14 DECEMBER 2022
ITEM 8.4

DEVELOPMENT NO.:	21027228
APPLICANT:	Michael Velonakis
ADDRESS:	31 Crescent Drive, Norton Summit
NATURE OF DEVELOPMENT:	Partial change of use of existing dwelling to include tourist accommodation and construction of outbuilding for tourist accommodation and studio workshop
ZONING INFORMATION:	Zones: <ul style="list-style-type: none"> Productive Rural Landscape Subzones: <ul style="list-style-type: none"> N/A Overlays: <ul style="list-style-type: none"> Environment and Food Production Area Hazards (Bushfire - High Risk) Hazards (Flooding - Evidence Required) Heritage Adjacency Limited Land Division Native Vegetation Prescribed Wells Area Traffic Generating Development Urban Transport Routes Technical Numeric Variations (TNVs): <ul style="list-style-type: none"> N/A
LODGEMENT DATE:	30 September 2021
RELEVANT AUTHORITY:	Assessment Panel at Adelaide Hills Council
PLANNING & DESIGN CODE VERSION:	2021.14
CATEGORY OF DEVELOPMENT:	Code Assessed – Performance Assessed
NOTIFICATION:	Yes
RECOMMENDING OFFICER:	Sebastien Paraskevopoulos
REFERRALS STATUTORY:	<ul style="list-style-type: none"> Country Fire Service
REFERRALS NON-STATUTORY:	<ul style="list-style-type: none"> Council Environmental Health Council Engineering

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DETAILED DESCRIPTION OF PROPOSAL:

The proposal includes two main elements for assessment. These are the partial change of use of the existing dwelling on site to include tourist accommodation, and the construction of a separate detached outbuilding also featuring tourist accommodation.

The envisaged change in use to tourist accommodation would apply to two bedrooms, one living area, and a bathroom within the dwelling. Of the building's 311m² floor area, the element of tourist accommodation would encompass up to 82m² (26 per cent) and accommodate a maximum of four guests at a time. No building work is proposed to the existing single-storey detached dwelling.

The other aspect of the proposal is the construction of an outbuilding detached from the main dwelling. While the use for this building is intended to include tourist accommodation, it is specified that the ground floor level would be capable of being utilised as a hobby workshop area and artist studio by the landowners in association with the main private residence, while also featuring a small kitchenette to assist in the preparation of meals for the guests above. On the mezzanine level are two bedrooms, one bathroom, and a living area, to accommodate a maximum of four guests at a time.

This new building utilises the slab of a partially constructed outbuilding, located forward of the dwelling in relation to Crescent Drive, toward the southern corner of the property. This building would have a combined internal floor area of 179m² across the ground and mezzanine levels atop a building footprint of approximately 102m². The wall height would be 4.92m to contain the mezzanine level, while the maximum building height would be 6.39m. The building orientation would be internal to the property, facing north toward the Lobethal Road frontage. At its closest points, it would be setback 4.3m from the front (south) boundary with Crescent Drive, 27.5m from the rear (north) boundary with Lobethal Road, and 7.4m from the side (east) boundary. No earthworks are required to accommodate the proposal, owing to the previously approved site works.

A combination of different finishes has been selected for the new building. This includes timber weatherboard cladding and galvanised iron walls, with a corrugated iron roof in Colorbond 'Ironstone'.

Off street parking with space for five vehicles is accommodated on site.

The anticipated length of stay for the venture will average 2-3 days, and check in and out times will match that of similar forms of accommodation in the industry. No advertisement signage has been included with this application.

BACKGROUND:

The land has been subject to two fully approved developments in the last five years. The retaining walls and associated earthworks approved in Development Application 18/1035/473 provide the benched site area for the new building subject of this application. The construction of the building slab that exists on-site was part of stage two of Development Application 15/517/473. It is proposed to build the current proposal on this existing concrete slab.

An extension of time for Development Application 15/517/473 was issued in June 2019, and substantial commencement (construction of the slab) was undertaken in October 2020, meeting the time constraints that were stipulated. Further details are provided in the table below.

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In 2016, the address of the subject land was changed from 6 Crescent Drive to 31 Crescent Drive.

APPROVAL DATE	APPLICATION NUMBER	DESCRIPTION OF PROPOSAL
02/04/2020	18/1035/473	Retaining walls (maximum height 3m) and combined fence and retaining wall (3.6m) & associated earthworks
LAPSED	18/338/473	Verandah (freestanding) over existing swimming pool
06/10/2017	15/517/473	Staged development – Stage One - dwelling alterations & additions & deck (maximum height 2.5m), Stage Two - demolition of shed & construction of studio/workshop with associated earthworks

SUBJECT LAND & LOCALITY:

Location reference: 31 Crescent Drive, Norton Summit SA 5136

Title ref.: CT: 6196/807 **Plan Parcel:** Lot 10 Sec: P1079 DP:115375

Site Description:

The allotment is regularly shaped with a total site area of 3643m² and used currently for residential purposes alone. It is one of six allotments in the locality which share street frontages to both Crescent Drive and Lobethal Road.

The current built form on the land includes the main dwelling, and an associated swimming pool and verandah. These existing elements are located centrally between the front and rear property boundaries, but favour the west side boundary.

The topography of the land descends down from Crescent Drive to Lobethal Road (south to north), at a gradient of approximately 1:5. The floor level of the existing dwelling is 4.6m below the concrete slab which occupies the proposed site area of the outbuilding.

The land is served by one existing vehicle access point, located off Crescent Drive near the south most corner of the allotment. On the opposite side of the reserve on Crescent Drive, are a stand of over a dozen tall, mature pine trees

The land is not connected to SA Water or Sewer Mains, and has an approved on-site waste system. The land is not subject to any easements or rights of way.

Locality

The property is located between Lobethal Road to the north, and Crescent Drive to the south, within the Productive Rural Landscape Zone.

This area is characterised by moderate to large rural living allotments with several large horticulture properties located throughout. These include a vineyard to the north of the subject land and the State Heritage listed Grove Hill estate to the south, which is South Australia's oldest orchard in continuous operation.

Norton Summit Primary School is situated further up Crescent Drive to the east, while a range of non-residential uses (including a church, hotel, medical clinic, post office, CFS station, and the Adelaide Hills Council Summit Community

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Centre) are located to the north-east, clustered around the five-way intersection of Norton Summit Road, Old Norton Summit Road, Lobethal Road, Debneys Road, and Colonial Drive.

CONSENT TYPE REQUIRED:

Planning Consent

CATEGORY OF DEVELOPMENT:

- **PER ELEMENT:**

- Change of use: Code Assessed – Performance Assessed
- Tourist accommodation: Code Assessed – Performance Assessed
- Outbuilding: Code Assessed – Performance Assessed

- **OVERALL APPLICATION CATEGORY:**

Code Assessed – Performance Assessed

- **REASON**

The Planning and Design Code provides no 'Accepted' or 'Deemed to Satisfy' pathway for any of the above elements –nor are they categorised as 'Restricted' under Table 4 of the zone. Therefore, as per sections 105 (b) and 107 of the *Planning Development & Infrastructure Act 2016*, the development is categorised as Code Assessed – Performance Assessed.

PUBLIC NOTIFICATION

- **REASON**

Tourist accommodation is exempt from public notification per Table 5 item 11 of the *Productive Rural Landscape Zone*, provided DTS/DPF 6.3 & DTS/DPF 6.4 are met. The proposal fails to meet the criteria of DTS/DPF6.3 & DTS/DPF6.4 so public notification was required. Specifically, the tourist accommodation is in a new building where the total floor area exceeds 100 square metres, and a 40m setback to all boundaries is not achieved.

Public notification was undertaken between 29 September 2022 and 20 October 2022.

- **LIST OF REPRESENTATIONS**

Four (4) representations were received during the public notification period. All four (4) were in opposition to the development, with two (2) electing to be heard by the Panel.

Representor Name	Representor's Property Address	Wishes to be heard	Nominated Speaker
Michael Richardson on behalf of the Giles family	412 Old Norton Summit Road, Norton Summit SA 5136	Yes	Michael Richardson
Anna McDonald	26 Crescent Drive, Norton Summit SA 5136	No	N/A
Paul Atkinson	44 Crescent Drive, Norton Summit SA 5136	No	N/A
Fabian Booth	15 Crescent Drive, Norton Summit SA 5136	Yes	Fabian Booth

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- **SUMMARY**

The following is a summary of the issues raised in the representations:

- Wastewater management and water quality impacts
- Potential use of the outbuilding
- Bushfire safety requirements
- Disability access
- Traffic generation/car parking
- Building height and associated visual impact
- Boundary setbacks
- Land use against the zone POs
- Impacts on adjacent State Heritage Place

The applicant has provided a written response and additional plans in the response to representations above.

A copy of the representations are included as **Attachment 4 – Representations** and the applicant's response is provided in **Attachment 5 – Response to Representations**.

AGENCY REFERRALS

- **Country Fire Service**

As per the Procedural Matters – Referrals table of the *Hazards (Bushfire - High Risk Overlay)*, the development was referred to the Country Fire Service, who provided their response on 23 June 2022. The Country Fire Service have no objection to the proposal, with four conditions and two advisory notes directed. Bushfire Attack Levels (BAL) of 19 and 29 were assigned to the existing dwelling and the new outbuilding, respectively.

For full details of the Country Fire Service's response, see **Attachment 6 – Referral Response**.

INTERNAL REFERRALS

- **Council Environmental Health**

Council Health Officers approved associated Wastewater Works Application 21/W222/473 on 2 March 2022, for the installation of a new septic system.

PLANNING ASSESSMENT

Desired outcomes

Desired outcomes are policies designed to aid the interpretation of performance outcomes by setting a general policy agenda for a zone, subzone, overlay or general development policies module. Where a relevant authority is uncertain as to whether or how a performance outcome applies to a development, the desired outcome(s) may inform its consideration of the relevance and application of a performance outcome, or assist in assessing the merits of the development against the applicable performance outcomes collectively.

Performance outcomes

Performance outcomes are policies designed to facilitate assessment according to specified factors, including land use, site dimensions and land division, built form, character and hazard risk minimisation.

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Designated performance features

In order to assist a relevant authority to interpret the performance outcomes, in some cases the policy includes a standard outcome which will generally meet the corresponding performance outcome (a designated performance feature or DPF). A DPF provides a guide to a relevant authority as to what is generally considered to satisfy the corresponding performance outcome but does not need to necessarily be satisfied to meet the performance outcome, and does not derogate from the discretion to determine that the outcome is met in another way, or from the need to assess development on its merits against all relevant policies.

The application has been assessed against the relevant provisions of the Planning & Design Code, which are contained in **Attachment 6 – Relevant P&D Code Policies**.

Productive Rural Landscape Zone

Desired Outcomes	
DO1	A diverse range of land uses at an appropriate scale and intensity that capitalise on the region's proximity to the metropolitan area and the tourist and lifestyle opportunities this presents while also conserving the natural and rural character, identity, biodiversity and sensitive environmental areas and scenic qualities of the landscape.
DO2	A zone that promotes agriculture, horticulture, value adding opportunities, farm gate businesses, the sale and consumption of agricultural based products, tourist development and accommodation that expands the economic base and promotes its regional identity.
DO3	The creation of local conditions that support new and continuing investment while seeking to promote co-existence with adjoining activities and mitigate land use conflicts.
Performance Outcomes (PO) & Deemed to Satisfy (DTS)/Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1; PO 2.1 & DPF 2.1; PO2.2 & DPF 2.2; PO 6.3 & DPF 6.3; PO 6.4 & DPF 6.4; PO 8.1 & DPF 8.1; PO 11.1	

With regard to DO 1, the proposal expands the range of land uses available in the locality, looking to take advantage of Norton Summit's close proximity to the Adelaide Plains. While it is of a relatively low scale in terms of guest capacity, it should be considered whether the nature of proposed outbuilding element satisfies the expectation of the DO in relation to the conservation of the natural scenic qualities of the landscape. This is discussed in further detail later in the report.

DO 2 seeks primary production value adding activities, including tourist accommodation land uses. While not in *direct* association with primary production on the allotment, the proposal would align with this desired use, and expand the economic base of the locale.

Tourist accommodation is an envisaged land use under DPF 1.1 (r) of the zone, and as such, satisfies PO 1.1. Further, the subject land is serviced by an all-weather trafficable public road, meeting PO 2.1. No elements of the proposal require earthworks, thereby satisfying PO 8.1 also.

While PO 6.3 states that tourist accommodation should be associated with the principal use of the land for primary production or primary production related value adding industry, the DPF broadens the criteria, ostensibly allowing for tourist accommodation where the desired primary production use is adjoining the land. On this basis, the proposal meets this aspect of DPF 6.3.

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DPF 6.3 further specifies that, when in a new building, the floor area for tourist accommodation should not exceed 100m². While the total floor area for the proposed new building is 179m², the main area for tourist accommodation is confined to the mezzanine level and would not exceed that limit. This is similarly the case for the element of tourist accommodation within the main dwelling, which is well under the 150m² stipulated for existing buildings. Further, while one facility is mentioned as being the desired amount by the DPF, the term *facility* is not defined by the Planning and Design Code, and no limit on buildings is specified by any zone provision. In an expanded context, the Oxford Dictionary defines *facility* as '*a place, usually including buildings, used for a particular purpose or activity*'.

DPF 6.4 lists provisions relating to height and setback for tourist accommodation, and while the new outbuilding is below 7m in total height, the setbacks proposed fall short of the 40m recommended by the DPF. When assessing against PO 6.4 itself, which seeks for tourist accommodation to maintain a pleasant rural character and amenity, the configuration of the site and the difference in levels should be considered in association with the shallow setbacks. The dimensions of the site could not facilitate 40m setbacks, no matter the location of the proposed development. Minimum allotment sizes for such a use are absent from the zone as well. Further, a difference in levels between the ground floor level of the proposed building and the roadside level are between 1m and 2m, with the site area being lower than that of the road. This means approximately a third of the height of the building would be obscured from view by the road reserve at the highest point. While the proposal does not present as an example of development which undoubtedly satisfies the intent of the PO, it is considered that these features contribute to extenuating the variance so that it does not *seriously* conflict with intent of PO 6.4.

With regard to PO 8.1, the proposal does incorporate the adaptive re-use of a building (the main dwelling) for small-scale tourist accommodation.

Similar to DPF 6.4, PO 11.1 directs that large buildings have substantial setbacks from boundaries and adjacent public roads. It further mentions that low reflective materials should be used for external finishes, and that buildings should be sited below ridgelines.

The proposed setbacks of the new building would not meet the expectation of the first point of PO 11.1. The development is at variance with the second point also, as the proposal incorporates a galvanised iron finish on the upper floor walls, which would be visible from roadside view. Thirdly, whilst the new building would be set below the roadside level, Crescent Drive itself is located on the ridgeline, and it is likely that the proposed built form would be visible above the skyline when viewed from the south. The proposal would not strictly satisfy PO 11.1 in these regards, however, the supporting auxiliary factors for the development need to be considered when assessing it against the criteria of this provision. These include the 13.7m length of the new building (excluding the ground floor verandah); its distance from neighbouring dwellings -the nearest of which is over 30m away and separated by neighbouring vegetation and an outbuilding; galvanised iron's tendency to dull over time and lose reflectivity; the building's height, which is below the maximum allowable 7m; the difference in levels between the roadside and the site area; and the row of mature pine trees that stand along the southern side of the road reserve which will provide some screening of the development for the representor to the south. While not all-encompassing, these factors are considered to restrain the proposal's potential visual impact and assist in the aim of reducing impacts on the scenic vistas of the area.

OVERLAYS

The Planning and Design Code does not draw upon any assessment provisions for *tourist accommodation* from the following Overlays: Environment and Food Production Area; Limited Land Division; Prescribed Wells Area; Traffic Generating Development. As such, they are absent among the overlays below.

Hazards (Bushfire - High Risk)

Desired Outcomes	
DO 1	Development, including land division is sited and designed to minimise the threat an impact of bushfires on life and property with regard to the following risks: a) Potential for uncontrolled bushfire events taking into account the increased frequency and intensity of bushfires as a result of climate change b) High levels and exposure to ember attack c) Impact from burning debris d) Radiant heat e) Likelihood and direct exposure to flames from a fire front.
DO 2	Activities that increase the number of people living and working in the area or where evacuation would be difficult is sited away from area of unacceptable bushfire risk.
DO 3	To facilitate access for emergency service vehicles to aid the protection of lives and assets from bushfire danger.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1; PO 1.2; PO 2.1; PO 3.1; PO 3.2 & DPF 3.2; PO 4.1; PO 4.2 & DPF 4.2; PO 4.3; PO 6.1 & DPF 6.1; PO 6.2 & DPF 6.2; PO 6.3	

As per the *Procedural Matters – Referrals* section of the Overlay, referral to the Country Fire Service was undertaken. The Country Fire Service has no objections to the proposal. Conditions relating to firefighting vehicle manoeuvring, water supply and access, and asset protection zones were stipulated. Within their advisory notes, the preparation of a Bushfire Survival Plan was recommended for the benefit of guests' safety. These recommended conditions are included as conditions 9 to 12 and notes 8 and 9 in the recommendation.

The south of the subject land is absent of any dense, mature vegetation, while the site area of the proposed new building has been levelled due to previous development. As such, its siting is not considered to conflict with relevant PO 2.1.

The building design, and the space afforded around the proposal would facilitate the maintenance and removal of potential burning debris, satisfying PO 3.1.

Water for firefighting use has also been accounted for, to the satisfaction of Ministerial Building Standard MBS 008, meeting PO 4.3.

Due to the proximity of the building to the roadside, it is not intended for firefighting vehicle to access and manoeuvre on site, although the Country Fire Service are satisfied that the driveway could be used in conjunction with the public road as a 'T' shaped turnaround. This would align with PO 6.2.

The proposal is considered to satisfy the relevant provisions of the Hazards (Bushfire - High Risk) Overlay.

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Hazards (Flooding - Evidence Required)

Desired Outcomes	
DO 1	Development adopts a precautionary approach to mitigate potential impacts on people, property, infrastructure and the environment from potential flood risk through the appropriate siting and design of development.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1	

Owing to the topography of Norton Summit, there is no flood mapping in proximity of the subject land or the greater locality.

Heritage Adjacency

Desired Outcomes	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1	

The subject land adjoins 412 Old Norton Summit Road, which contains the State Heritage listed Grove Hill estate. This listing includes several buildings, described by the SA Heritage Places Database as *'Dwelling ('Grove Hill'), including Coach House, Stables, 3 Hole Privy, Gatehouse, Fruit Store, Cider Cellars and Hothouses'*.

PO 1.1 states that new development should not 'dominate, encroach on or unduly impact on the setting of the (adjacent) Place'. The site area of the new building is over 500m from the nearest building on the adjoining land and separated by Crescent Drive. As such, the proposed development is not considered to impact the heritage context of the adjacent land.

Native Vegetation

Desired Outcomes	
DO 1	Areas of native vegetation are protected, retained and restored in order to sustain biodiversity, threatened species and vegetation communities, fauna habitat, ecosystems services, carbon storage and amenity values.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1; PO 1.2; PO 1.3 & DPF 1.3	

The development area is clear of native vegetation and the applicant made their declaration upon submission of the application, which complies with DTS/DPF1.1.

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Urban Transport Routes

Desired Outcomes	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1; PO 2.1 & DPF 2.1; PO 3.1 & DPF 3.1; PO 4.1 & DPF 4.1; PO 5.1 & DPF 5.1; PO 6.1 & DPF 6.1; PO 7.1 & DPF 7.1; PO 8.1 & DPF 8.1; PO 9.1 & DPF 9.1; PO 10.1 & DPF 10.1;	

The proposal does not propose any new vehicle access points, nor the alteration or re-location of the existing access used to service the development. State-owned Lobethal Road will not be altered or directly impacted by the proposal. Therefore, the proposal is considered to comply with the relevant provisions of the overlay.

GENERAL DEVELOPMENT POLICIES

Clearance from Overhead Powerlines

Desired Outcomes	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1	

A declaration was made by the applicant upon submission of the application that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the Electricity Act 1996. On this basis the proposal is in accordance with the above provisions.

Design

Desired Outcomes	
DO 1	Development is: <ul style="list-style-type: none">(a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality(b) durable - fit for purpose, adaptable and long lasting(c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors(d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.4 & DPF 1.4; PO 6.1 & DPF 6.1; PO 7.4; PO 7.5; PO 8.1 & DPF 8.1	

The new building does not incorporate any technical elements (plant, exhaust and intake vents etc.) beyond its roofline, thereby satisfying PO and DPF 1.4.

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The new septic system approved by Council's Environmental Health Unit in association with the proposed development does not impede the provision of private open space on the site, and it is located away from the areas required for on-site vehicle parking and driveway access. This satisfies PO and DPF 6.1.

The off-street parking in association with the development is between 6m and 7m below that of the road level of Crescent Drive, rendering PO 7.4 and PO 7.5 not relevant.

As discussed prior, the proposal includes no earthworks, thereby satisfying PO and DPF 8.1.

Infrastructure and Renewable Energy Facilities

Desired Outcomes	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 11.1 & DPF 11.1, PO 12.1 & DPF 12.1; PO 12.2 & DPF 12.2	

The subject land does not benefit from SA Water infrastructure for water supply, and instead relies on rainwater harvested on site, with bore water to supplement. This is considered to satisfy PO 11.1

PO 12.1 specifies that when development cannot be connected to a common wastewater disposal service such as sewer or CWMS, that wastewater is managed on-site. The new waste system will be wholly contained within the boundaries of the subject land, and it will comply with the requirements of the Public Health Act 2011 as approval for the septic system has been granted by Council Environmental Health.

The proposed building works would not encroach upon this approved septic system, satisfying PO 12.2.

Interface between Land Uses

Desired Outcomes	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 9.3 & DPF 9.3; PO 9.4 & DPF 9.4; PO 9.5 & DPF 9.5; PO 10.1 & DPF 10.1	

The applicant has stated they will adopt check-in and check-out times consistent with the industry standard for tourist accommodation, while the proposal is relatively small in scale accommodating a maximum of eight (8) guests at any one time. The operators of the proposal will continue to reside on the land, so will have influence in managing guest behaviour and possible noise issues. Tourist accommodation is listed as a desired land use in the Productive Rural Landscape Zone.

PO 9.3, PO 9.4, and PO 9.5 relate to limiting the impact on newly proposed sensitive receivers *from* lawfully existing uses. Specifically, land-based aquaculture, dairies, and bulk commodities. Similarly, PO 10.1 seeks to separate new sensitive receivers from existing mines. The proposal is not considered at variance with any of these POs.

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Site Contamination

Desired Outcomes	
DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site contamination.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1 & DPF 1.1	

The proposal is for a change of use to include tourist accommodation. Within *Practice Direction 14*, tourist accommodation is not a more sensitive use than the existing residential use. Therefore DPF 1.1 is considered met and no site investigations were required.

Tourism Development

Desired Outcomes	
DO 1	Tourism development is built in locations that cater to the needs of visitors and positively contributes to South Australia's visitor economy.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.1; PO 1.2; PO 2.1; PO 2.2; PO 2.3 & DPF 2.3; PO 2.4; PO 2.5; PO 2.6; PO 3.1; PO 3.2; PO 3.3; PO 3.4	

The subject land is located in the Productive Rural Landscape Zone, where tourist accommodation is a desired use. It is also located centrally within Norton Summit, and in close proximity to metropolitan Adelaide.

While the form of tourist accommodation proposed is not contained within conventional 'units', the activity would still be spread across two buildings in different areas of the site. PO 1.2 seeks that tourist accommodation in units be clustered together to minimise environmental and contextual impact. In the case of the proposal, the tourist accommodation within the existing dwelling and the proposed outbuilding would be separated by a distance of approximately 25m. The form of tourist accommodation proposed is arguably not in the spirit of what the PO intends.

Transport, Access and Parking

Desired Outcomes	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.
Performance Outcome (PO) and Designated Performance Feature (DPF) criteria	
PO 1.4 & DPF 1.4; PO 3.1 & DPF 3.1; PO 3.5 & DPF 3.5; PO 4.1; PO 5.1 & DPF 5.1; PO 6.1 & DPF 6.1; PO 6.2; PO 6.6 & DPF 6.6	

Access to the site will be via an existing single vehicle access to Crescent Drive. This road is sealed and connects directly to the main arterial road (Lobethal Road). Notably, Crescent Drive is closed between 8.30am and 6.00pm on school days, however, this closure point is located nearer to the Norton Summit Primary School, and traffic operates normally between the access point of the subject land and Crescent Drive's north junction with Lobethal Road during these times.

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Guest vehicles will be able to enter and exit the site in a forward motion, with room to manoeuvre in the car parking area. Off street parking with space for five vehicles is accommodated on site, without compromising the vehicle parking allowance required for the existing dwelling. The development complies with Table 1 – General Off-Street Car Parking Requirements of the Planning & Design Code.

PO 4.1 describes that development should be designed and sited so that access is safe, dignified, and convenient for people with disabilities. There are no DPF criteria associated with this PO to assist in determining compliance, however, the proposal will require a Building Consent assessment to obtain full Development Approval, in which access to premises for people with disabilities would be assessed.

Council Engineering raised no concerns with regard to increased traffic movements that could be associated with the proposal. Due to the envisaged use and limited maximum guest capacity, vehicle movements are expected to be infrequent. It is anticipated with tourist accommodation that guests will mostly be out during the day reducing vehicle movements relative to, or in comparison with residential uses.

CONCLUSION

The proposed development is for the partial change of use to an existing dwelling to incorporate tourist accommodation, along with the construction of an outbuilding to be used for tourist accommodation and a studio workshop

The subject land is in the Productive Rural Landscape Zone, where tourist development is an envisaged use, particularly when associated with or adjoining primary production land uses. The land adjoins a primary production use.

The proposal seeks to take advantage of Norton Summit's proximity to the Adelaide Plains, along with its central location in Norton Summit. The locality features a small range of non-residential and non-primary production uses.

The relevant provisions of the Productive Rural Landscape Zone are not wholly fulfilled by the proposal, and there are variances, especially in relation to boundary setbacks. There is potential for the development to change the nature of the current scenic and rural vistas afforded by the property. However, this impact is mitigated by a relatively small building footprint, a floor level below that of the road, along with well-established existing vegetated screening on the road reserve, that all would contribute to limiting these variances beyond the point of which they could be considered serious.

The Country Fire Service was satisfied with the proposal, subject to standard conditions and advisory notes being imposed. On-site car parking commensurate to the proposed use has also been accounted for in the application, and there is an approved waste system for the new building. Conditions restricting the use of the buildings for their intended purposes and capacity have been recommended, to prevent the new outbuilding from being used for long-term accommodation (or another unapproved use) without a separate Development Approval.

The proposal substantially complies with the provisions of the remaining relevant overlays and general development policies of the Planning & Design Code.

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RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

- 1) Pursuant to Section 107(2)(c) of the Planning, Development and Infrastructure Act 2016, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code; and
- 2) Development Application Number 21027228, for Partial change of use of existing dwelling to include tourist accommodation and construction of outbuilding for tourist accommodation and studio workshop by Michael Velonakis at 31 Crescent Drive, Norton Summit is granted Planning Consent subject to the following conditions:

CONDITIONS

Planning Consent

- 1) The development granted shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below.
- 2) The external finishes to the outbuilding herein approved shall be as follows:
WALLS: Timber weatherboard cladding in Solver 'Doeskin' and 'heritage' galvanised iron walls
ROOF: Colorbond 'Ironstone' or similar
- 3) The person(s) having the benefit of this consent shall refrain from permitting the use of the buildings (or any part thereof) for the provision of long-term accommodation or as a separate dwelling. The tourist accommodation elements shall be used and operated on a short-term rental arrangement with a maximum of a one (1) week stay per occupancy.

A logbook shall be kept of all occupancies for each calendar year and made available for inspection by the Council upon request.

- 4) At any one time, the overall number of guests for tourist accommodation shall be limited to a maximum of eight (8) people.
- 5) All car parking spaces, driveways and manoeuvring areas shall be designed, constructed, drained and line-marked in accordance with Australian Standard AS 2890.1:2004. Line marking and directional arrows shall be clearly visible and maintained in good condition at all times. Driveways, vehicle manoeuvring and parking areas shall be constructed of compacted gravel prior to commencement of the approved use and maintained in good condition at all times to the reasonable satisfaction of the Council.
- 6) The workshop studio within the outbuilding shall not be used for human habitation, commercial or industrial purposes. Any such activity may constitute a change in use and will require separate development approval.
- 7) All external lighting shall be directed away from residential development and shielded if necessary to prevent light spill causing nuisance to the occupiers of those residential properties.
- 8) All roof runoff generated by the development hereby approved shall be managed on-site to the satisfaction of Council using design techniques such as:
 - Rainwater tanks
 - Grassed swales

ITEM 8.4

- Stone filled trenches
- Small infiltration basins

All roof run-off generated by the development hereby approved shall be directed within one month of the roof cladding being installed to the existing stormwater management system on-site to the satisfaction of Council. Stormwater overflow management shall be designed so as to not permit trespass into the effluent disposal area. Stormwater shall be managed on site with no stormwater to trespass onto adjoining properties.

CONDITIONS IMPOSED BY THE SOUTH AUSTRALIA CONTRY FIRE SERVICE AS PER SECTION 122 OF THE PDI ACT

9) SITING

‘The Planning and Design Code’ Hazards (Bushfire – High Risk) Overlay (Performance Objective 2.1, 4.2, 4.3) details the mandatory requirements for buildings and structures to be located away from areas that pose an unacceptable bushfire risk in order to provide sufficient defensible space for occupants and fire fighters; ensure radiant heat levels at the buildings are minimised in line with the assessed bushfire attack level & construction level; whilst maintaining reduced fuel loads and ensuring it can be maintained in perpetuity by the occupants.

Outbuildings and other ancillary structures shall be sited no closer than 6m from the habitable building, unless built to relevant building standards for associated structures in Bushfire Prone Areas.

10) ACCESS TO HABITABLE BUILDING

‘The Planning and Design Code’ Hazards (Bushfire – High Risk) Overlay (Performance Objective 6.2) details the mandatory requirements for ‘Private’ roads and driveways to facilitate safe and effective use, operation and evacuation for firefighting and emergency personnel and evacuation of residents, occupants and visitors where required. These requirements apply when the furthest point of the building is more than 60m from the nearest public road.

SA CFS has no objection to utilising the driveway and the public road for a ‘T’ shaped turnaround with the following conditions:

- The driveway shall be connected to a formed, all-weather public road with the transition area between the road and driveway having a gradient of not more than 7 degrees (1-in-8).
- Access to the building site shall be of all-weather construction, with a minimum formed road surface width of 3 metres.
- Private access shall have minimum internal radii of 9.5 metres on all bends.
- Private access shall provide overhead clearances of not less than 4.0m horizontally and vertically between the driveway surface and overhanging branches or other obstructions, including buildings and/or structures.
- The gradient of the access road shall not exceed 16 degrees (1-in-3.5) at any point along the driveway. In steep terrain exceeding 10 degrees (1-in-5.5) the surface should be sealed.
- The all weather road is to be constructed such that it is protected from water erosion of the traffic surface. The road surface shall be profiled to manage storm water run -off to appropriate drains, at one or both sides of the traffic surface. The accumulated volumes of water shall be directed via:
 - a. open drains; or
 - b. culverts and pipes under the traffic surface, and / or away from same, without causing further soil erosion, silting of adjacent areas or water courses or instability of any embankment or cutting.

11) WATER SUPPLY & ACCESS (to dedicated water supply)

Ministerial Building Standard MBS008 “Designated bushfire prone areas - additional requirements” 2020, as published under the Planning, Development and Infrastructure Act 2016, provides the technical details of the dedicated water supply for bushfire fighting for the bushfire zone. The dedicated bushfire fighting water supply shall also incorporate the installation of a pumping system, pipe work and fire-fighting hose(s) in accordance with MBS008.

‘The Planning and Design Code’ Hazards (Bushfire – High Risk) Overlay (Performance Objective 4.3) details the mandatory requirements for the site to provide a dedicated hardstand area in a location that allows fire fighting vehicles to safely access the dedicated water supply.

SA CFS has no objection to the location of the existing dedicated water supply as detailed on drawing named PROPOSED SITE PLAN dated at last revision 02/04/2022, providing the outlet is positioned remotely to comply with the following conditions:

- The water supply outlet shall be easily accessible and clearly identifiable from the access way.
- The dedicated water supply and its location should be identified with suitable signage (i.e. blue sign with white lettering “FIRE WATER”).
- Access to the dedicated water supply shall be of all-weather construction, with a minimum formed road surface width of 3 metres.
- Provision shall be made adjacent the water supply for a nominally level hardstand area (capable of supporting fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes) that is a distance equal to or less than 6 metres from the water supply outlet.
- SA CFS appliance’s inlet is rear mounted; therefore the outlet/water storage shall be positioned so that the SA CFS appliance can easily connect to it rear facing.
- A gravity fed water supply outlet may be remotely located from the above ground tank to provide adequate access.
- All non-metal water supply pipes for bushfire fighting purposes (other than flexible connections and hoses for fire-fighting) shall be buried below ground to a minimum depth of 300mm with no non-metal parts above ground level.
- All water supply pipes for draughting purposes shall be capable of withstanding the required pressure for draughting.
- Ideally a remote water supply outlet should be gravity fed, where this is not possible the following dimensions shall be considered as the maximum capability in any hydraulic design for draughting purposes:
 - a. The dedicated water supply outlet for draughting purposes shall not exceed 5 metres maximum vertical lift (calculated on the height of the hardstand surface to the lowest point of the storage) and no greater than 6 metres horizontal distance.
 - b. The suction outlet pipework from the water supply shall be fitted with an inline non return valve of nominal internal diameter not less than that of the suction pipe and be located from the lowest point of extract from the tank. All fittings shall be installed to allow for easy maintenance.
- The minimum water supply required may be combined with domestic use, providing the outlet for domestic use is located above the dedicated fire water supply (in order for it to remain as a dedicated supply).

12) MAINTAIN AN ASSET PROTECTION ZONE (APZ) - VEGETATION MANAGEMENT

‘The Planning and Design Code’ Hazards (Bushfire – High Risk) Overlay (Performance Objective 4.2) details the mandatory requirements to establish and maintain an asset protection zone. As such, landscaping shall include bushfire protection features that will prevent or inhibit the spread of bushfires and minimise the risk to life and/or damage to buildings and property and maintain a fuel reduced zone for safe movement of occupants and fire fighters.

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The extent of an existing or proposed asset protection zone has not been detailed on supplied drawings.

- Vegetation management shall be established and maintained within 20 metres of the habitable building (or to the property boundaries – whichever comes first) as follows:
 1. The number of trees and understorey plants existing and to be established within the VMZ shall be reduced and maintained such that when considered overall a maximum coverage of 30% is attained, and so that the leaf area of shrubs is not continuous. Careful selection of the vegetation will permit the ‘clumping’ of shrubs where desirable, for diversity, and privacy and yet achieve the ‘overall maximum coverage of 30%’. SA CFS notes that coverage of vegetation within 20m, to the north west of the proposed habitable (studio) building as well as along the “front” boundary (adjacent Lobethal Road) is currently greater than 30%.
 2. Reduction of vegetation shall be in accordance with SA Native Vegetation Act 1991 and SA Native Vegetation Regulations 2017.
 3. Trees and shrubs shall not be planted closer to the building(s) than the distance equivalent to their mature height.
 4. Trees and shrubs must not overhang the roofline of the building, touch walls, windows or other elements of the building.
 5. Shrubs must not be planted under trees and must be separated by at least 1.5 times their mature height from the trees’ lowest branches.
 6. Grasses within the zone shall be reduced to a maximum height of 10cm during the Fire Danger Season.
 7. No understorey vegetation shall be established within 2 metres of the habitable building (understorey is defined as plants and bushes up to 2 metres in height).
 8. Flammable objects such as plants, mulches and fences must not be located adjacent to vulnerable parts of the building such as windows, decks and eaves
 9. The VMZ shall be maintained to be free of accumulated dead vegetation.

ADVISORY NOTES

General Notes

- 1) No work can commence on this development unless a Development Approval has been obtained. If one or more consents have been granted on this Decision Notification Form, you must not start any site works or building work or change of use of the land until you have received notification that Development Approval has been granted.
- 2) Appeal rights – General rights of review and appeal exist in relation to any assessment, request, direction or act of a relevant authority in relation to the determination of this application, including conditions.
- 3) This Planning Consent is valid for a period of twenty-four (24) months commencing from the date of the decision, subject to the below or subject to an extension having been granted by the relevant authority. If applicable, Building Consent must be obtained prior to expiration of the Planning Consent.
- 4) Where an approved development has been substantially commenced within 2 years from the operative date of approval, the approval will then lapse 3 years from the operative date of the approval (unless the development has been substantially or fully completed within those 3 years, in which case the approval will not lapse).
- 5) Management of the property during construction shall be undertaken in such a manner as to prevent denudation, erosion, or pollution of the environment.

ITEM 8.4

- 6) The applicant is reminded of their general environmental duty, as required by Section 25 of the Environment Protection Act 1993, to take all reasonable and practical measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes, or may cause, environmental harm.
- 7) A separate development application is required for any signs or advertisements (including flags and bunting) associated with the development herein approved.

ADVISORY NOTES IMPOSED BY THE SOUTH AUSTRALIA COUNTRY FIRE SERVICE AS PER SECTION 122 OF THE PDI ACT

8) BUILDING CONSIDERATIONS

Ministerial Building Standard MBS008 “Designated bushfire prone areas - additional requirements” 2020, as published under the Planning, Development and Infrastructure Act 2016 applies to this site.

Please refer to the National Construction Code (NCC), relevant standards and state provisions for construction requirements and performance provisions.

A site Bushfire Attack Level (BAL) assessment was conducted in accordance with the NCC and Australian Standard™3959 (AS3959) “Construction of Buildings in Bushfire Prone Areas”.

Category of Bushfire Attack Level:

EXISTING DWELLING - PART CONVERSION TO TOURIST ACCOMMODATION: BAL 19

This BAL rating is conditional upon the establishment and maintenance of a 20 metre Asset Protection Zone, (or to the property boundaries, whichever comes first) in accordance with the Asset Protection Zone – Vegetation Management condition of consent placed on the planning consent with the same application reference.

SELF-CONTAINED STUDIO CONVERSION TO TOURIST ACCOMMODATION: BAL 29

This BAL rating is conditional upon the establishment and maintenance of a 20 metre Asset Protection Zone, (or to the property boundaries, whichever comes first) in accordance with the Asset Protection Zone – Vegetation Management condition of consent placed on the planning consent with the same application reference.

This report is considered relevant at the date of assessment with respect to the elevations detailed on proposed Site Plan, dated 02/04/2022 and shall not be considered as SA CFS endorsement of any subsequent development.

9) TOURIST ACCOMMODATION - BUSHFIRE SURVIVAL PLAN

CFS further recommends:

- The applicants to prepare and display a BUSHFIRE SURVIVAL PLAN (BSP) designed specifically for the purpose of any guests that may be in residence during a bushfire event, especially during the Fire Danger Season. The BSP:
 1. should provide clear directions to persons that may be unfamiliar with the area/locality and unfamiliar with what protective actions they may need to take to protect their lives during a bushfire event, including when to take such protective actions; and
 2. should address the possibility that the owners may not be present at the time of the bushfire event; and
 3. should not expect guests to be involved in fire-fighting operations.

The SA CFS ‘Bushfire Safety Guide For Business’ document (refer to CFS website) should be utilised as a basis for information and the drafting of the (GUEST) BSP.

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The applicant should consider reducing operating hours and restrictions on days of heightened bushfire danger and/or bushfire events and consider including any alterations to bookings and services offered due to actual or predicted conditions during the Fire Danger Season in any booking/refund policy.

OFFICER MAKING RECOMMENDATION

Name: Sebastien Paraskevopoulos

Title: Statutory Planner

REAL PROPERTY ACT, 1886



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 6196 Folio 807

Parent Title(s) CT 5081/167**Creating Dealing(s)** RTD 12735138**Title Issued** 15/09/2017 **Edition** 1 **Edition Issued** 15/09/2017

Estate Type

FEE SIMPLE

Registered Proprietor

MANI WHITE
OF 6 CRESCENT DRIVE NORTON SUMMIT SA 5136

Description of Land

ALLOTMENT 10 DEPOSITED PLAN 115375
IN THE AREA NAMED NORTON SUMMIT
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
11564584	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.

Notations

Dealings Affecting Title NIL**Priority Notices** NIL**Notations on Plan** NIL**Registrar-General's Notes** NIL**Administrative Interests** NIL

D115375

SHEET 2 OF 2

56675_pland_1_V05_Version_7

BEARING DATUM: ①-④ 157°16'20"

DERIVATION: D48774 ADOPTED

TOTAL AREA:

REFERENCE MARKS

CNR	BEARING	FROM	DIST	PM NO
1	252°21'	PM FD	1.01	6628/36923
2	54°52'	PM FD	0.10	6628/36896
3	41°06'	PM FD	10.12	6628/36938



VIDE CONTINUATION
BELOW LEFT

VIDE CONTINUATION
ABOVE RIGHT

GILBERT SURVEYORS PTY LTD

2 CREMORNE STREET
FULLARTON SA 5063
ref: C112/15

tel 82719639
mob 0428719639
gilbert@esc.net.au

PROPOSED B&B TOURIST ACCOMMODATION

AND EXISTING RESIDENCE

MICHAEL VELONAKIS

31 CRESCENT DRIVE

NORTON SUMMIT SA

SHEET	DRAWING LIST
01	GENERAL NOTES
02	PROPOSED SITE PLAN
03	FIRE FIGHTER SERVICES PLAN
04	FLOOR PLANS
05	SEWER FLOOR PLAN LAYOUT
06	PROPOSED B&B FLOOR PLAN (EXISTING HOUSE)
07	CONCRETE PLAN
08	JOIST LAYOUT
09	ELEVATIONS
10	ROOF PLAN
11	SECTION A-A
12	ELECTRICAL LAYOUT
13	WINDOW AND DOOR SCHEDULE
14	WET AREA DETAILS
15	BRACING DETAILS

SITE PLAN NOTES

ALL SITE DIMENSIONS ARE IN MILLIMETRES UNLESS SPECIFIED OTHERWISE .

REFER TO PLAN OF SUBDIVISION OR CERTIFICATE OF TITLE FOR VERIFICATION OF ALL BEARINGS, DISTANCES AND OTHER SITE INFORMATION.

FOR DETAILS OF ALL EASEMENTS REFER TO THE PLAN OF SUBDIVISION OR CERTIFICATE OF TITLE AND OTHER INFORMATION BY THE RESPONSIBLE AUTHORITY.

INSTALLATION OF ALL SERVICES INCLUDING ALL STORMWATER, WASTE WATER AND SEWER DETAILS SHALL COMPLY WITH THE RESPECTIVE SUPPLY AUTHORITY REQUIREMENTS.

BUILDER TO VERIFY ALL SET DETAILS ON SITE.

NEW WATER METERS ARE TO BE INSTALLED. ENSURE METERS ARE EQUIPPED WITH A BACKFLOW PREVENTION DEVICE & STOP TAP. CONNECT TO BUILDING IN ACCORDANCE WITH LOCAL AUTHORITIES REQUIREMENTS

CONNECT BUILDING WITH TELSTRA CABLES IN STRICT ACCORDANCE WITH TELSTRA'S REQUIREMENTS

CONNECT WATER SUPPLY TO THE SANITARY ITEMS WITHIN THE PROPOSED BUILDING & HWS WHERE REQUIRED IN ACCORDANCE WITH LOCAL AUTHORITIES REQUIREMENTS. SUPPLY HOT & COLD WATER TO NEW SANITARY ITEMS AS REQUIRED

PROVIDE UNDERGROUND POWER SUPPLY IN ACCORDANCE WITH POWER SUPPLIERS REQUIREMENTS

ALL DOWN PIPES TO BE CONNECTED TO WATER TANKS. ALL WATER TANKS TO HAVE OVER FLOW PIPE DISCHARGING INTO STORMWATER DRAINAGE. ALL CONNECTIONS TO BE IN ACCORDANCE WITH LOCAL AUTHORITIES REQUIREMENTS, REFER TO ENGINEERS PLANS FOR ALL DETAILS & SPECIFICATIONS.

THE CONTRACTOR AND SUBCONTRACTOR SHALL ENSURE THAT ALL STORMWATER DRAINS, SEWER PIPES AND THE LIKE ARE LOCATED AT A SUFFICIENT DISTANCE FROM ANY BUILDINGS FOOTINGS AND / OR SLAB EDGE BEAMS SO AS TO PREVENT GENERAL MOISTURE PENETRATION, DAMPNESS, WEAKENING AND UNDERMINING OF ANY BUILDING AND ITS FOOTING SYSTEM.

STORMWATER NOTES

ALL 90mmø DOWN PIPE CONNECTIONS SHALL BE CLASS 6 UPVC SOLVENT WELDED JOINTED, LINE LAID TO A MINIMUM GRADE OF 1:100 & CONNECTED TO THE LEGAL POINT OF DISCHARGE TO THE RELEVANT AUTHORITIES APPROVAL. PROVIDE INSPECTION OPENINGS @ 9m MAX CTRS & @ EACH CHANGE OF DIRECTION. THE CONTRACTOR SHALL PROVIDE APPROVED DOWN PIPE TO STORM WATER ADAPTERS WHERE REQUIRED. THE TOP OF THESE ADAPTERS TO FINISH ABOVE EXIST GROUND / PAVING LEVEL. THE COVER TO UNDERGROUND STORM WATER DRAINS SHALL BE NOT LESS THAN

- 100mm UNDER SOIL
- 50mm UNDER PAVED OR CONCRETE AREAS
- 100mm UNDER UNREINFORCED CONC. OR PAVED DRIVEWAYS
- 75mm UNDER REINFORCED CONC. DRIVEWAYS

GENERAL NOTES

SCALE 1:100 @ A3

COMPACT FILL NOTES

FILL USED ON SITE SHOULD BE PLACED AT OR NEAR OPTIMUM MOISTURE CONTENT AND COMPACTED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6 OF AS 2870-1996, AS SUMMARISED BELOW:

(A) NON-SAND FILL UP TO 300mm IN DEPTH OR SAND FILL UP TO 600mm DEEP, COMPACTED IN NOT MORE THAN 150mm LAYERS BY REPEATED ROLLING BY AN EXCAVATOR, IS CLASSED AS ROLLED FILL AND MAY BE USED FOR SUPPORT OF SLAB PANELS AND INTERNAL BEAMS. NOTE THAT EDGE BEAMS MUST BE SUPPORTED ON ORIGINAL GROUND OR ON CONTROLLED FILL AS DEFINED IN (B) BELOW:

(B) NON-SAND FILL UP TO 400mm IN DEPTH OR SAND FILL UP TO 800mm DEEP, COMPACTED BY MECHANICAL ROLLER OR VIBRATING PLATE RESPECTIVELY, IN LAYERS NOT MORE THAN 150mm THICK AND AT CLOSE TO OPTIMUM MOISTURE CONTENT, IS DEEMED TO COMPLY AS CONTROLLED FILL. IT IS DEEMED TO COMPLY AS SUITABLE FOR FOUNDING THE SLAB, EDGE AND INTERNAL BEAMS.

THE REQUIREMENTS OF AS 2870-1996 FOR CONSTRUCTION OF THE FOOTINGS AND LANDSCAPING OF THE SITE SHOULD BE FOLLOWED, WITH PARTICULAR ATTENTION PAID TO SURFACE DRAINAGE AND AVOIDANCE OF PONDING NEXT TO FOOTINGS. TREES SHOULD BE PLANTED NO CLOSER TO THE FOOTINGS THAN 1.0x MATURE HEIGHT.

SHOULD SOIL CONDITIONS DIFFERING THAN DESCRIBED BE FOUND DURING EXCAVATION WORKS, ESPECIALLY THE PRESENCE OF UNREPORTED FILL OR POORLY COMPACTED MATERIAL, PLEASE CONTACT THIS OFFICE

TERMITE PROTECTION NOTES

WHERE THE BUILDING (EXCLUDES A DETACHED CLASS 10) IS LOCATED IN A TERMITE PRONE AREA, THE AREA TO UNDERSIDE OF BUILDING AND PERIMETER IS TO BE TREATED AGAINST TERMITE ATTACK. A TERMITE BARRIER OR COMBINATION OF BARRIERS MUST BE INSTALLED IN ACCORDANCE WITH AS 3660.1. THE AREA BENEATH THE SUSPENDED FLOOR MUST BE VENTILATED IN ACCORDANCE WITH THE BCA & INSPECTED IN ACCORDANCE WITH AS 3660.1. ALL TIMBER COMPLETELY OR PARTICALLY PROTECTED FROM WATHER TO BE H2 TREATED ALL TIMBER OUTSIDE AND ABOVE GROUND SHALL BE H3 TREATED. ALL TIMBER THAT IS NON-STRUCTURAL THAT IS IN CONTACT WITH GROUND OR WATER SHALL BE H4 TREATED.

RAINWATER TANK NOTES

SUITABLE WATER TANK/S SHALL BE INSTALLED COMPLETE WITH PRESSURE PUMP, SUPPLY PIPE AND GATE VALVE. RAINWATER SHALL BE PROVIDED TO THE ENTIRE RESIDENCE INCLUDING HWS. TANK OVERFLOW MUST BE DISCHARGED INTO THE EXIST PADDOCK IN ACCORDANCE WITH LOCAL AUTHORITIES REQUIREMENTS. INSTALLATION TO COMPLY WITH RELEVANT REGULATIONS AND MANUFACTURER'S RECOMMENDATIONS

KITCHEN NOTES

ALL KITCHEN DETAILS BY OWNER -VERIFY ALL LAYOUTS, APPLIANCE SIZES & LOCATION ON SITE PRIOR TO CONTRUCTION

GAS COOKTOPS ARE TO BE 200mm OR GREATED FROM A STAINLESS STEEL OR GLASS SPLASHBACK WALL.

IF CLEARANCE IS LESS THAN 200mm, THE FOLLOWING MUST APPLY:

STAINLESS STEEL-

(A) THE WALL MUST NOT CONTAIN COMBUSTIBLE MATERS OR

(B) THE SURFACE OF THE WALL IS TO BE PROTECTED AS PER AS 5601/AG601, CLAUSE 5.12.1

TOUGHENED GLASS-

(A) A LETTER IS TO BE SUPPLIED BY GLASS SUPPLIER OR GLASS MANUFACTURER INDICATED THAT THE GLASS IS FIT FOR PURPOSE AT THE CLEARANCE STATED AND;

(B) IF THE GLASS IS FIXED TO A COMPUSTIBLE SURFACE, THAT SURFACE IS TO BE PROTECTED AS PER AS 5601/AG 601 CLAUSE 5.12.1

THIS MAY ALSO BE MEET IF DOCUMENTATION IS PROVIDED TO THE EFFECT THAT THE FIXING METHODS WITH ENSURE THE TEMPERATURE OF THE COMPUSTIBLE SURFACE WILL NOT EXCEED 65°C

HEATING APPLIANCE

TO BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS TO COMPLY WITH BCA PART 3.7.3

ENERGY EFFICIENCY

CONSTRUCTION TO COMPLY WITH BCA PART 3.12 REFER TO ENERGY RATER'S REPORT.

BATHROOM & ENSUITE

ALL INSITU SHOWER ENCLOSURE FLOOR TO BE GRADED TOWARDS OUTLET WITH GRADE NOT LESS THAN 1:60 FALL REFER TO BCA PART 3.8.1.14

FLOOR TO BE TILED & TO BE NON-SLIP

ALL AREAS TO BE CONSTRUCTED IN ACCORDANCE WITH BCA PART 3.8.1

GLAZED SCREEN & DOORS AS SELECTED TO COMPLY WITH BCA PART 3.6 & 3.8.1.27

ALL FIXTURES, FITTINGS & FINISHES AS SELECTED

VANITY WITH SELECTED FINISH OPENING DOORS WITH CENTRAL SHELF

TILE ABOVE VANITY TO MIN. 150mm

INSTALL FRAMED MIRROR AS SELECTED TO WALL ABOVE VANITY

BATH AS SELECTED SET IN TILED HOB UNLESS OTHERWISE NOTED

ALL WORK IN ACCORDANCE WITH BCA PART 3.8.1

LAUNDRY

STAINLESS STEEL TROUGH INSET IN BENCH AS SELECTED

SELECTED FINISH TO BENCH & CUPBOARDS

TILE BEHIND BENCH & WASHER TO MIN. HEIGHT OF 500mm

WET AREA TO COMPLY WITH BCA PART 3.8.1

PRIVIDE NOGGING FOR WALL MOUNTED DRYER OVER WASHER

PROVIDE FLOOR OUTLET.

GENERAL NOTES

FOR CONSTRUCTION DEC 22

ALL CONSTRUCTION TO CONFORM TO B.C.A AND AUSTRALIAN STANDARDS

VERIFY ALL FINISHED FLOOR LEVELS BEFORE THE COMMENCEMENT OF ANY BUILDING WORK.

ALL SANITARY PLUMBING WORK TO BE CARRIED OUT BY FULLY QUALIFIED AND LICENCED TRADES PERSONS. AS PER AS 3500

ALL ELECTRICAL WORK CARRIED OUT TO BE IN ACCORDANCE WITH AS 3000.

THIS DRAWING TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS, DOCUMENTS, SCHEDULES AND SPECIFICATIONS.

TERMITE PROTECTION SHALL COMPLY WITH AS3660, 1-2000, PROVIDE CERTIFICATE IN ACCORDANCE WITH AS3660, 1-2000 STATING METHOD OF APPLICATION AND CERTIFICATE OF COMPLETION

ROOF CONTRACTOR SHALL PROVIDE ALL NECESSARY FLASHINGS, CAPPINGS AND OTHER ITEMS REQUIRED TO MAKE THE ROOF WATERTIGHT AND COMPLETE.

BUILDER TO CHECK AND CONFIRM ALL SITE AND SET OUT DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

REFER TO ENGINEERS DOCUMENTATION FOR DETAILS ON FOOTING, STRUCTURE, SITEWORKS AND STORMWATER DETAILS.

ALL STEEL WORK TO COMPLY WITH BCA-PART 3.4.2.

ALL WET AREA DETAILS TO COMPLY WITH AS 3740-2004 AND MINISTERS SPECIFICATION SA F1.7 (MAY 2005)

OWNERS RESPONSIBILITY TO WASH DOWN ALL PRE-PAINTED AND PAINTED SURFACES ON A REGULAR BASIS FOR LONGEVITY OF PRODUCT.

DRAUGHT PROTECTION DEVICES TO BE INSTALLED TO ALL EXTERNAL DOORWAYS

HOT WATER SUPPLY SYSTEM DESIGNED AND INSTALLED IN ACCORDANCE WITH SECTION 6A OF AS/NZS 3500.4.2 OR CLAUSE 3.38 OF AS/NZS 3500.5

NOTE: ALL EXISTING DIMENSIONS TO BE CONFIRMED ON SITE PRIOR TO COMMENCEMENT OF CONSTRUCTION

NOTE: FINISHED FLOOR LEVEL TO BE A MINIMUM OF 300MM ABOVE TOP OF ROADSIDE KERB LEVEL

CONSTRUCTION TO COMPLY WITH BCA PART 3.12 REFER TO ENERGY RATER'S REPORT.

NO VARIATION MAY BE MADE TO THIS DRAWING WITHOUT THE PRIOR APPROVAL OF THE PROPRIETOR AND/OR DESIGNER.

WRITTEN DIMENSIONS TO BE TAKEN IN PREFERENCE TO SCALED.

GROUND LEVELS AND FINISHED FLOOR LEVELS INDICATED ARE APPROXIMATE ONLY AND ARE TO BE CONFIRMED ON SITE OR BY ENGINEER AND TO BE READ IN CONJUNCTION WITH ANY SURVEY PLANS AVAILABLE. ALL DIMENSIONS ARE TO BE CHECKED WITH EXISTING AND PROPOSED SITE CONDITIONS.

CLIENT:
MICHAEL VELONAKIS
PROPOSED:
PROPOSED B&B TOURIST ACCOMMODATION AND EXISTING RESIDENCE
SITE ADDRESS:
31 CRESCENT DRIVE NORTON SUMMIT
DATE:
DECEMBER 2022
DRAWN:
GR
SHEET:
1 OF 15
JOB No.



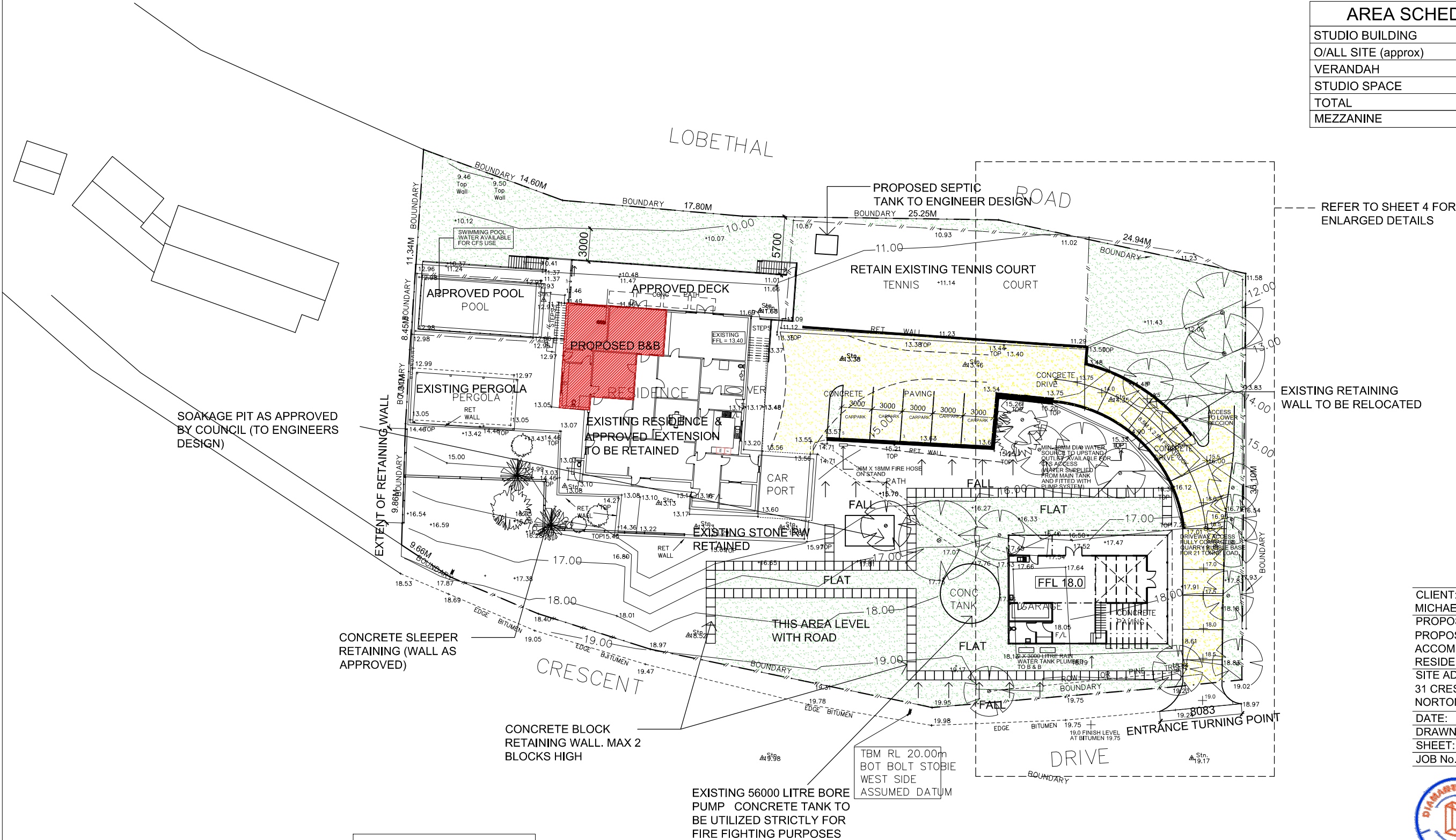
**BUILDING
DESIGN &
PROJECT
MANAGEMENT**
PO Box 551, Marleston
SA, 5033

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WEBSITE: www.diamantidesign.com.au
EMAIL: jim@diamantidd.com.au

THESE DESIGNS ARE COPYRIGHT. LEGAL ACTION WILL BE TAKEN AGAINST ANY INFRINGEMENT IN PART OR WHOLE UNLESS WRITTEN AUTHORITY IS GIVEN BY THE DESIGNER.

FIGURED DIMENSIONS SHALL TAKE PREFERENCE OVER SCALED DRAWINGS. VERIFY DIMENSIONS AND LEVELS BEFORE COMMENCING ANY BUILDING WORK. ANY DISCREPANCY TO BE REPORTED TO THE DESIGNERS IMMEDIATELY.

AREA SCHEDULE	
STUDIO BUILDING	
O/ALL SITE (approx)	3800 m2
VERANDAH	35.5 m2
STUDIO SPACE	96.7 m2
TOTAL	132.2 m2
MEZZANINE	38.5 m2



CLIENT:
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RESIDENCE
SITE ADDRESS:
31 CRESCENT DRIVE
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DATE: DECEMBER 2022
DRAWN: GR
SHEET: 2 OF 15
JOB No.

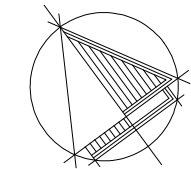


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PROPOSED SITE PLAN

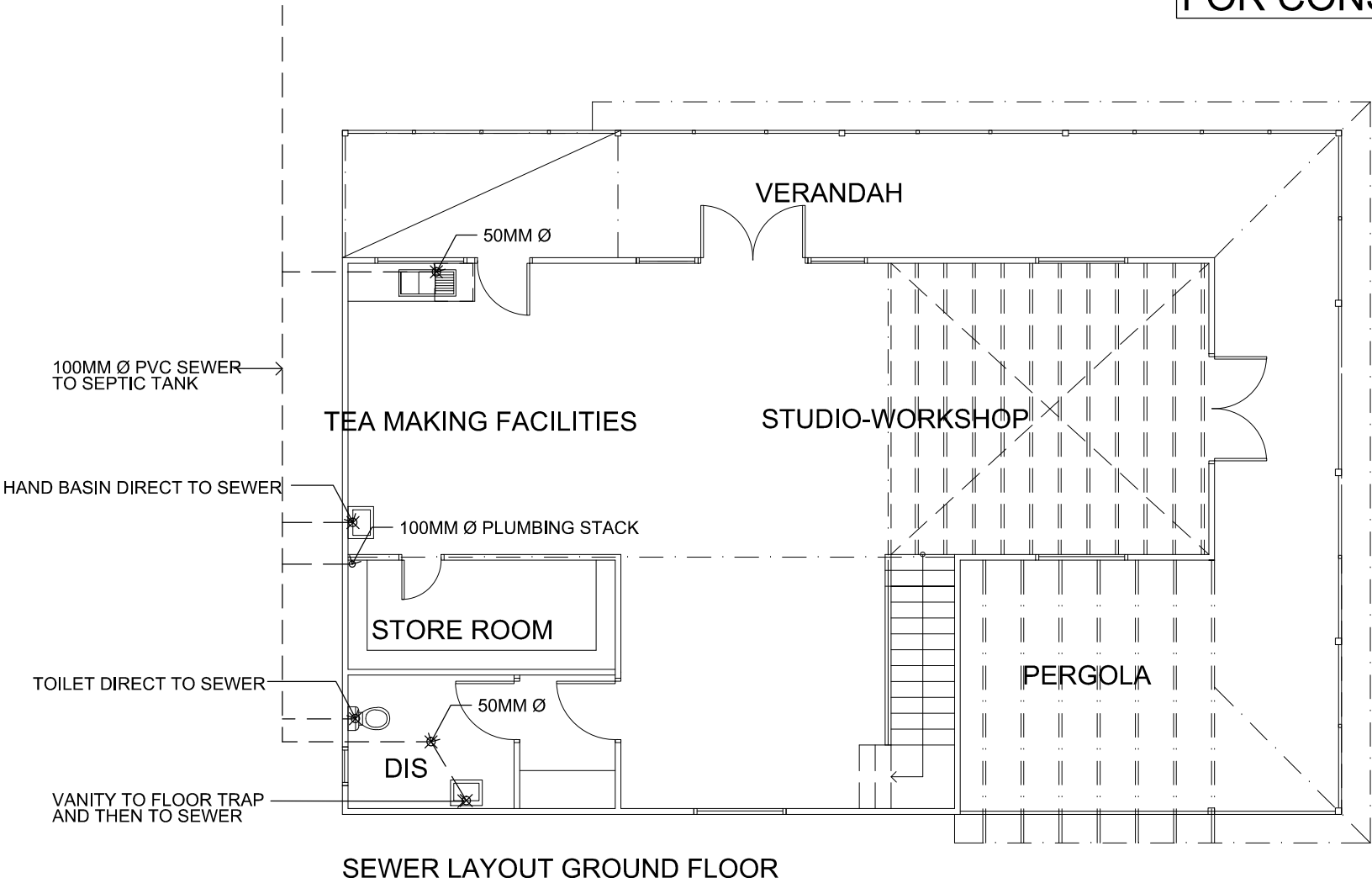
SCALE 1:400 @ A3



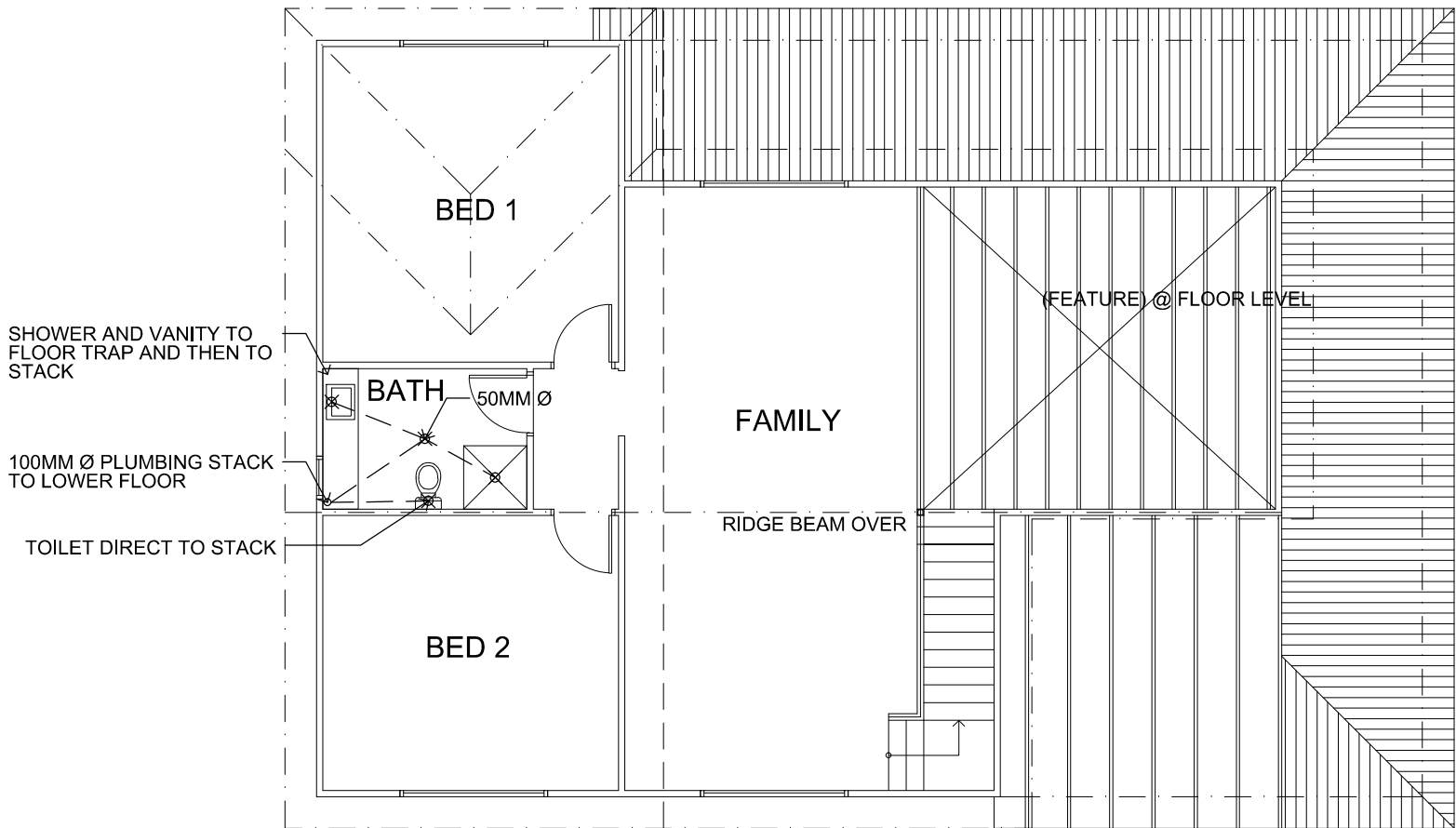
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BUILDING AND COLOUR SCHEDULE		
ITEM	CONSTRUCTION	COLOUR
FLOOR	CONCRETE RAFT SLAB & FOOTING TO ENGINEERS DESIGN	--
WALLS	TIMBER FRAME, WITH GALVANIZED HORIZONTAL CUSTOM ORB	NATURAL COLLOR
	HARDIES WEATHERBOARD CLADDING	DOESKIN
ROOF	COLOURBOND	IRONSTONE
WINDOWS	ALUMINIUM	CREAM

SCALE 1:100 @ A3



SEWER LAYOUT GROUND FLOOR



SEWER LAYOUT UPPER FLOOR

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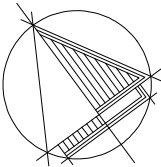
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SEWER LAYOUT

SCALE 1:100 @ A3



THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS, DOCUMENTATION, SCHEDULES AND SPECIFICATIONS

ALL PORTABLE FIRE EXTINGUISHERS TO TO COMPLY WITH TABLE E1.6 OF THE BCA AND TO BE SELECTED, LOCATED AND DISTRIBUTED IN ACCORDANCE WITH AS2444

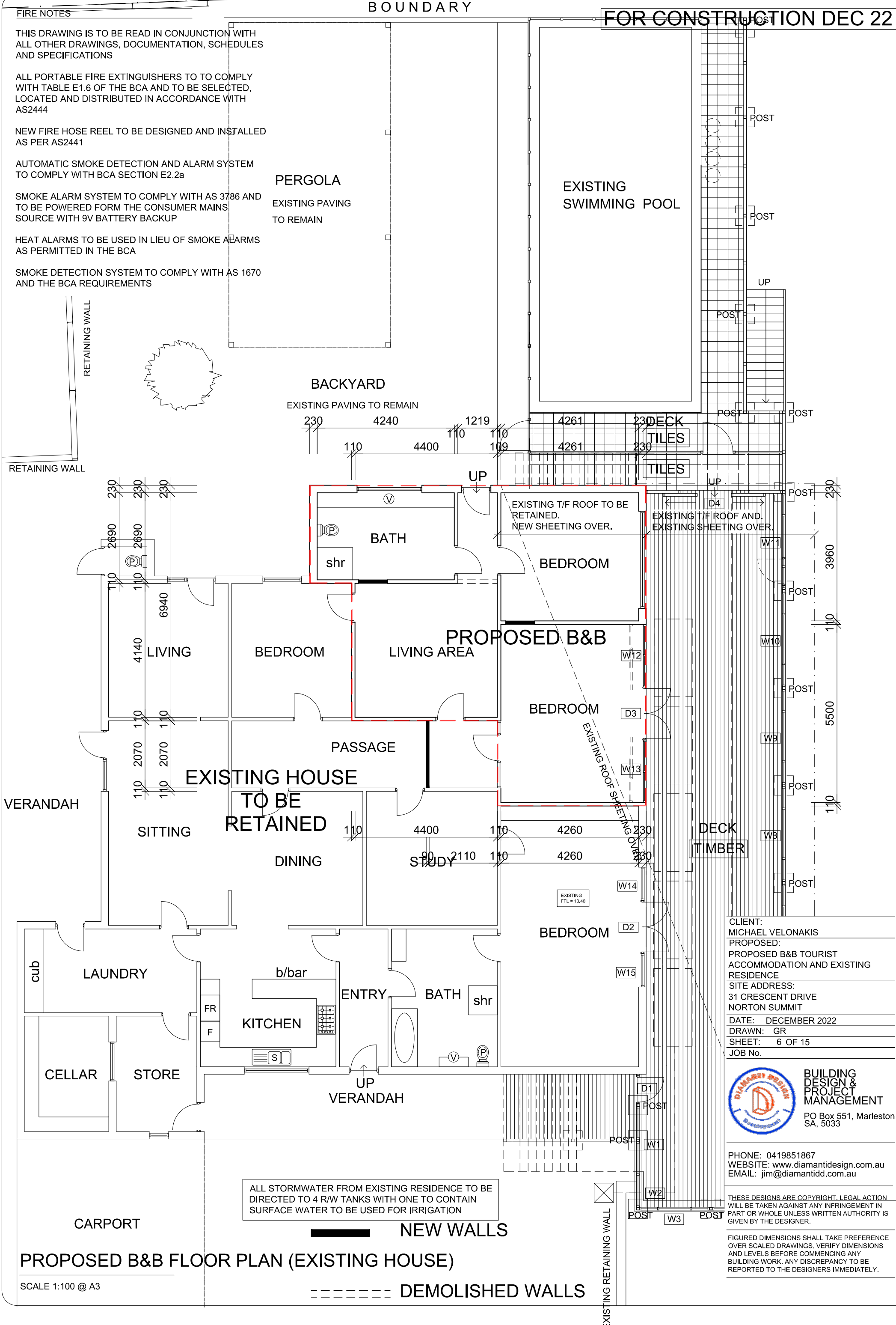
NEW FIRE HOSE REEL TO BE DESIGNED AND INSTALLED AS PER AS2441

AUTOMATIC SMOKE DETECTION AND ALARM SYSTEM TO COMPLY WITH BCA SECTION E2.2a

SMOKE ALARM SYSTEM TO COMPLY WITH AS 3786 AND TO BE POWERED FORM THE CONSUMER MAINS SOURCE WITH 9V BATTERY BACKUP

HEAT ALARMS TO BE USED IN LIEU OF SMOKE ALARMS AS PERMITTED IN THE BCA

SMOKE DETECTION SYSTEM TO COMPLY WITH AS 1670 AND THE BCA REQUIREMENTS



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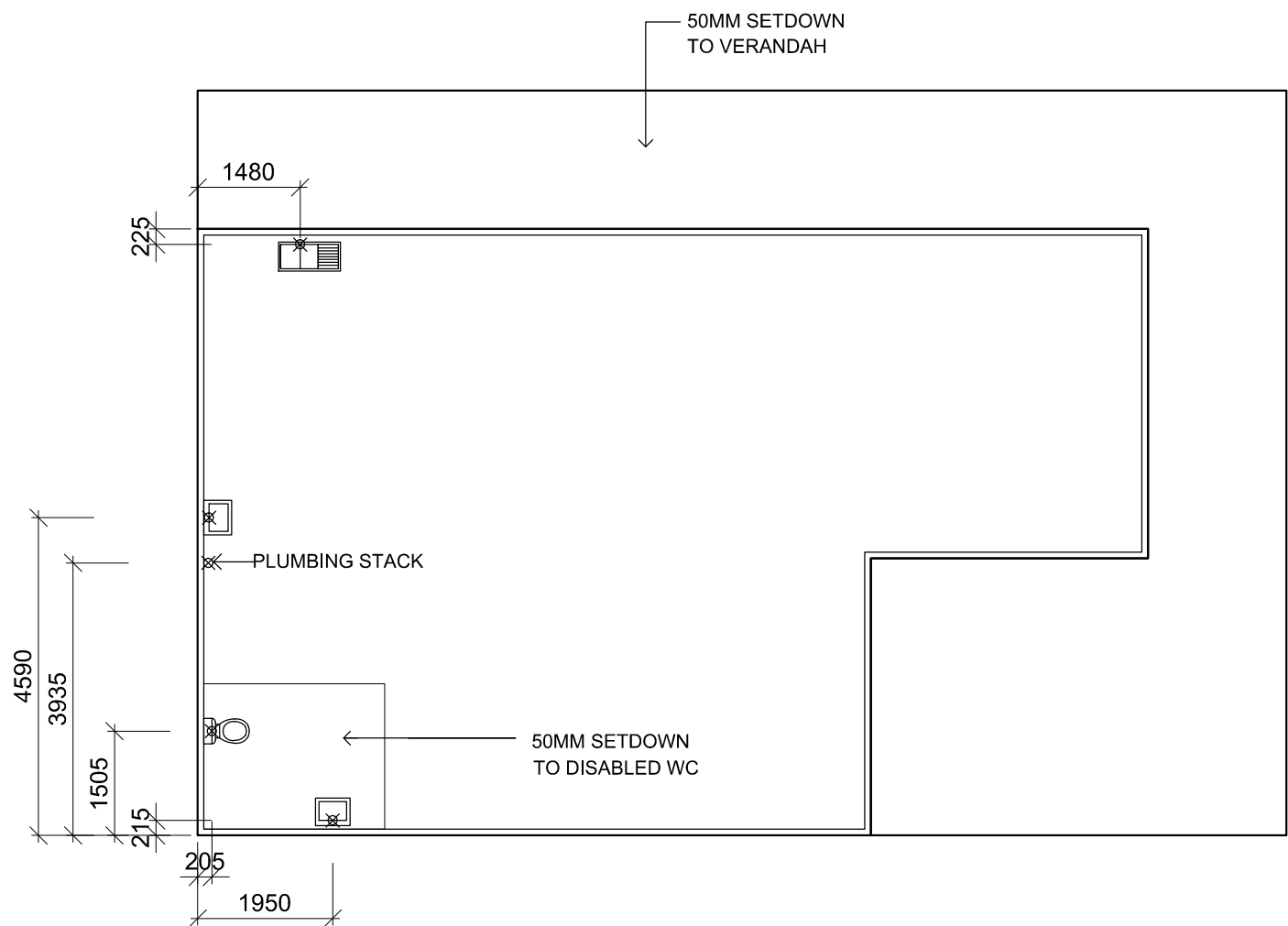


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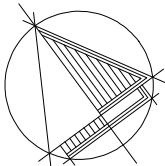
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CONCRETE FOOTINGS TO
ENGINEERS DESIGN AND DETAIL



CONCRETE PLAN

SCALE 1:100 @ A3

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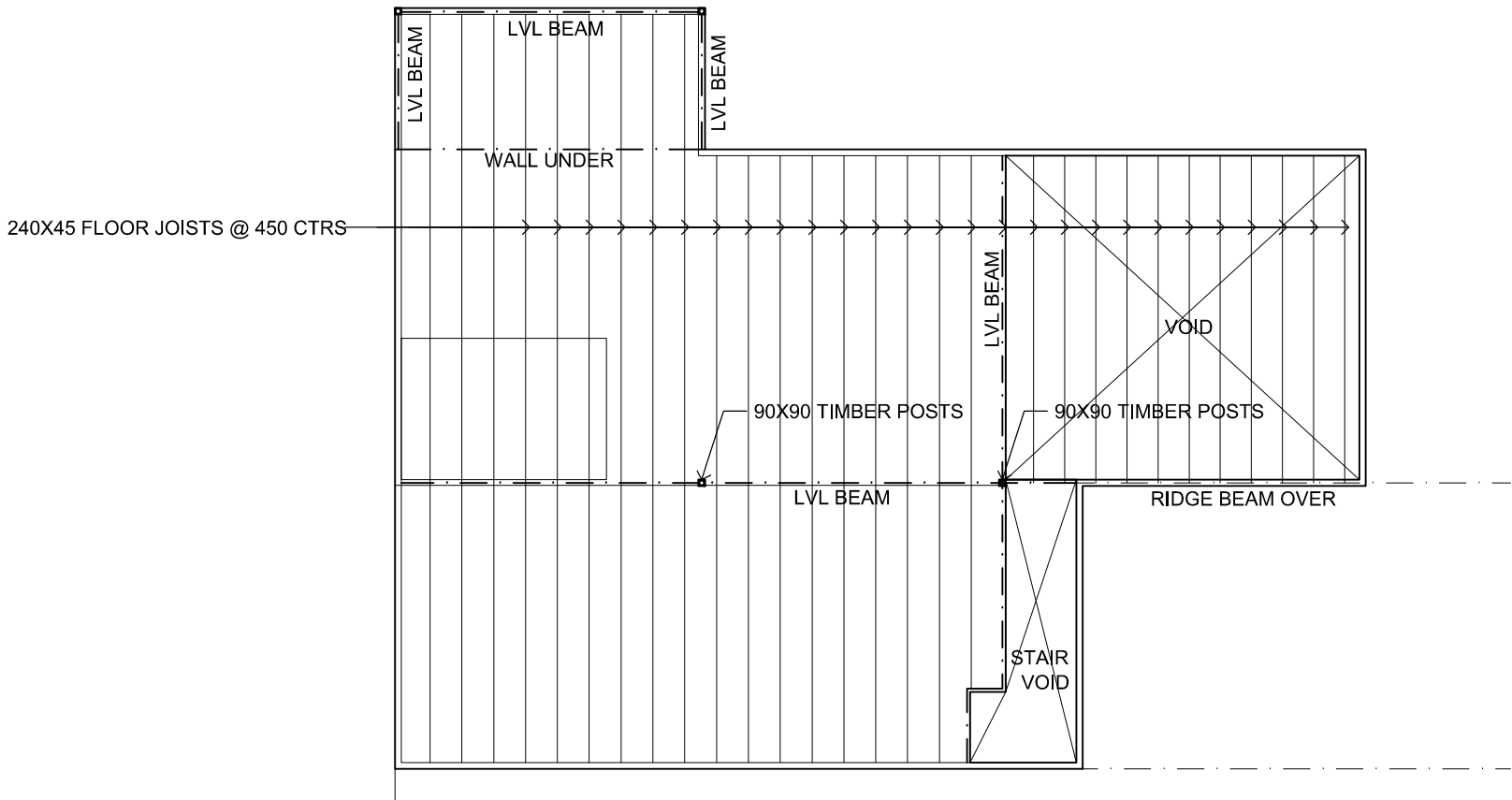


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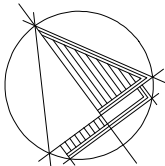


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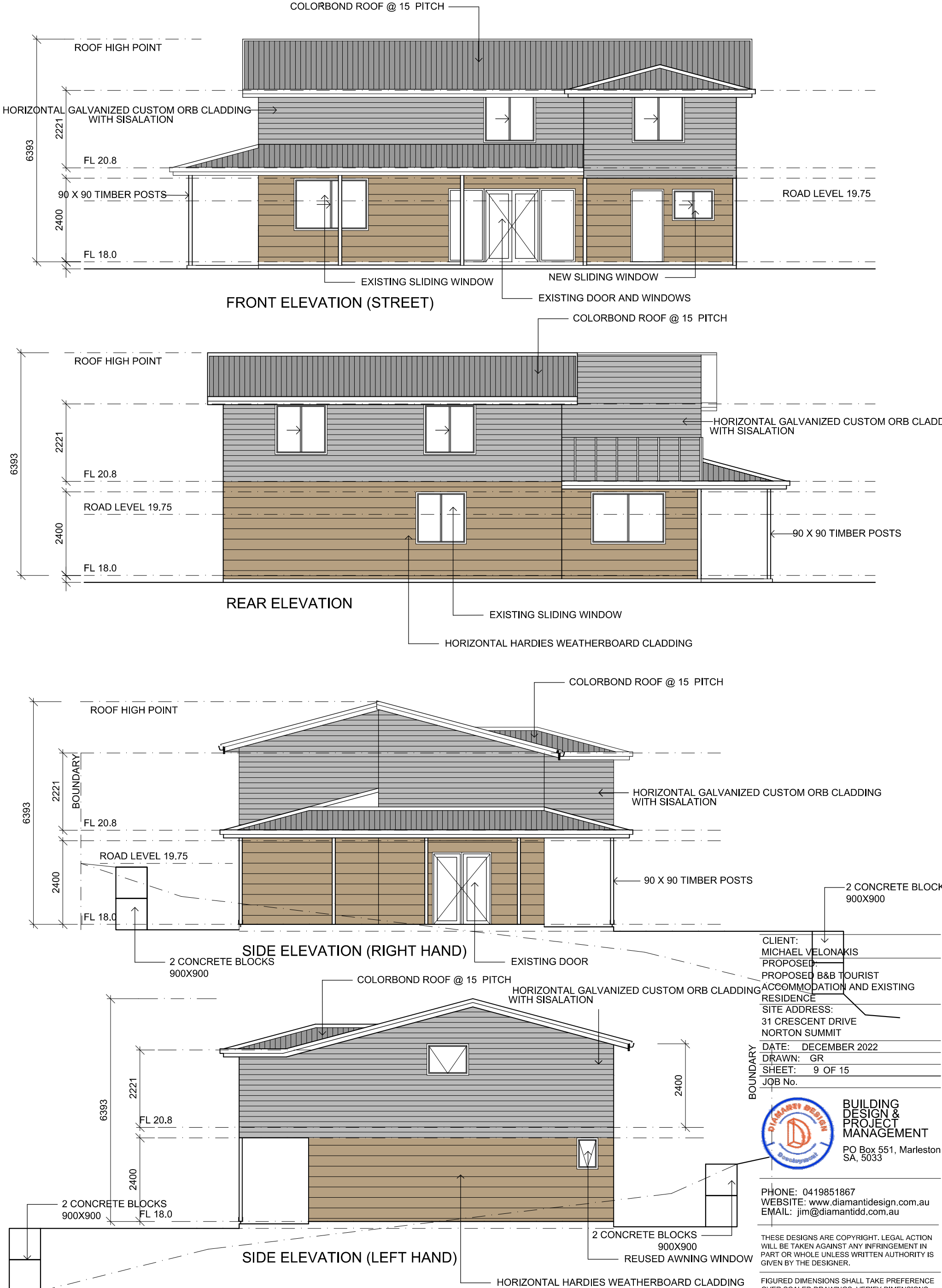
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FLOOR JOIST LAYOUT

SCALE 1:100 @ A3



ELEVATIONS

SCALE 1:100 @ A3

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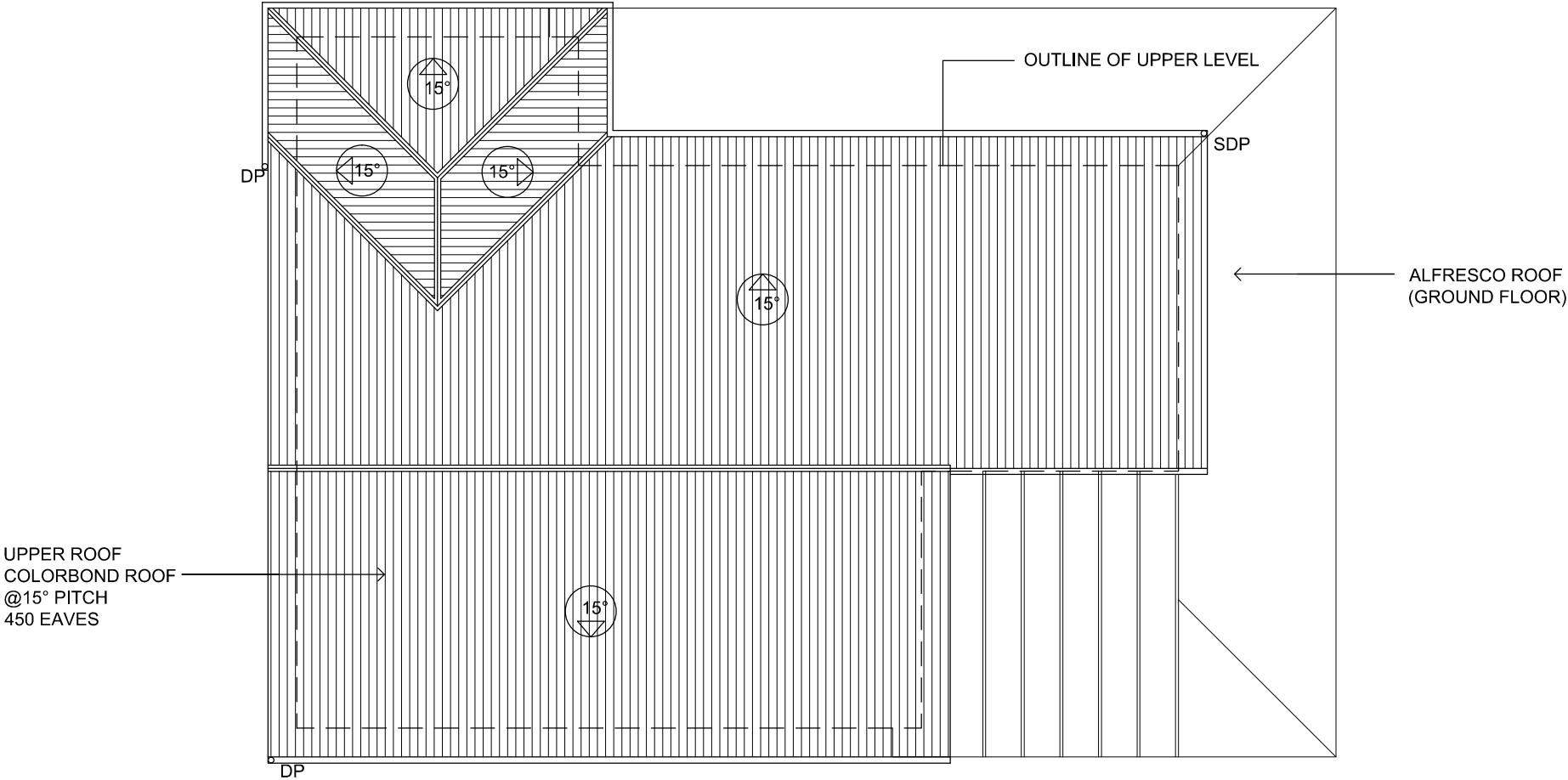


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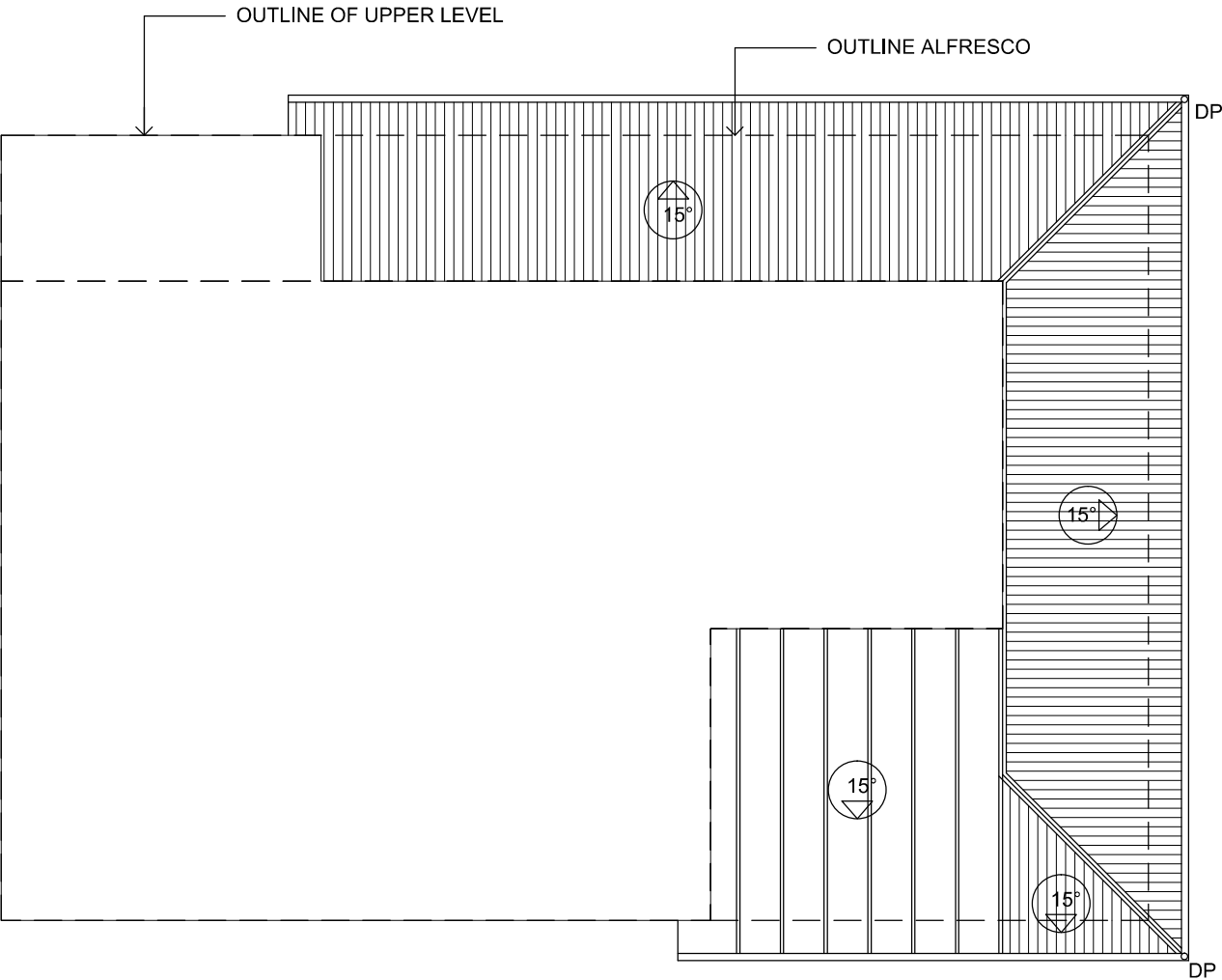
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UPPER LEVEL



ALFRESCO ROOF
LOWER LEVEL

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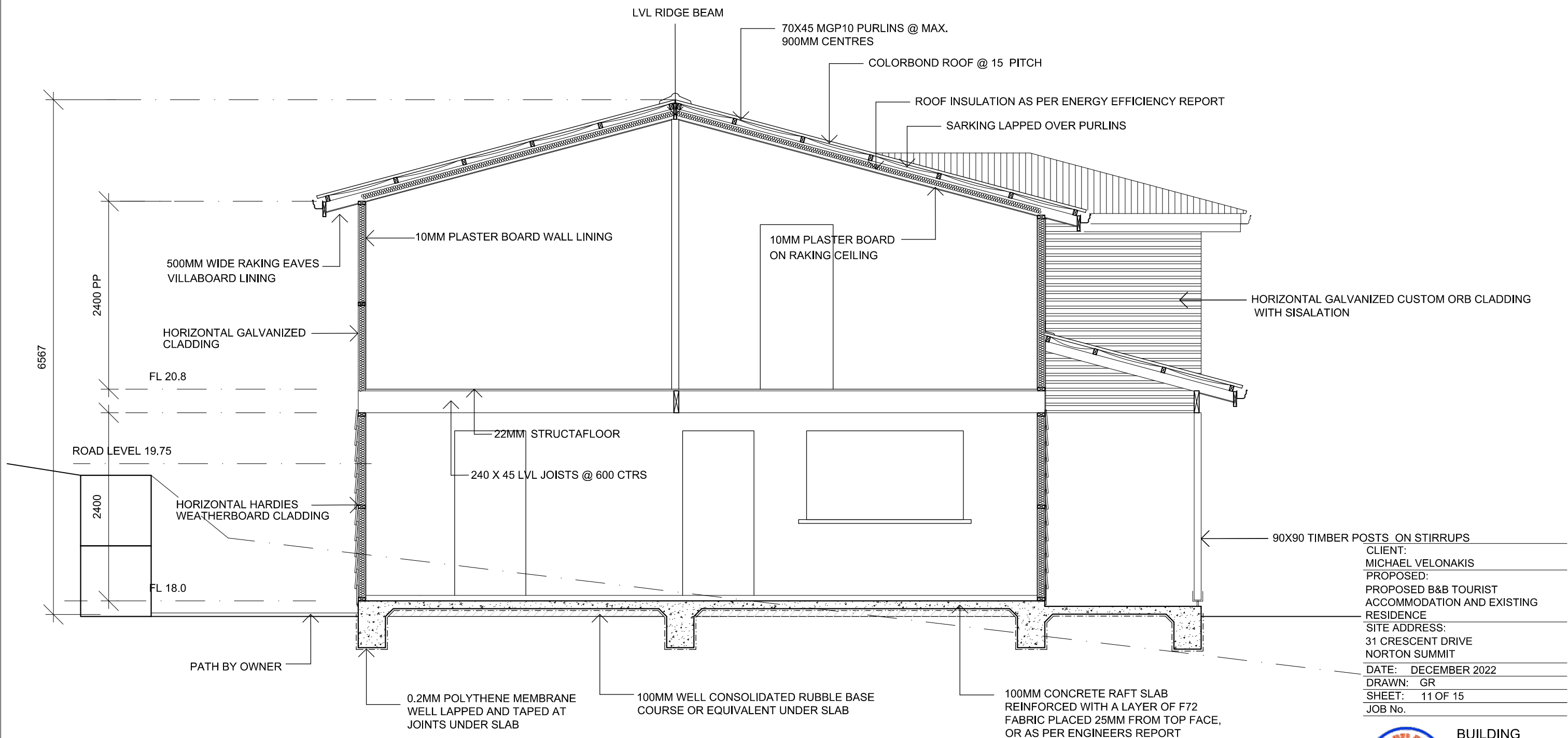
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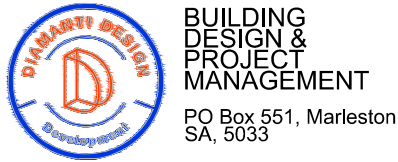
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ROOF PLAN

SCALE 1:100 @ A3



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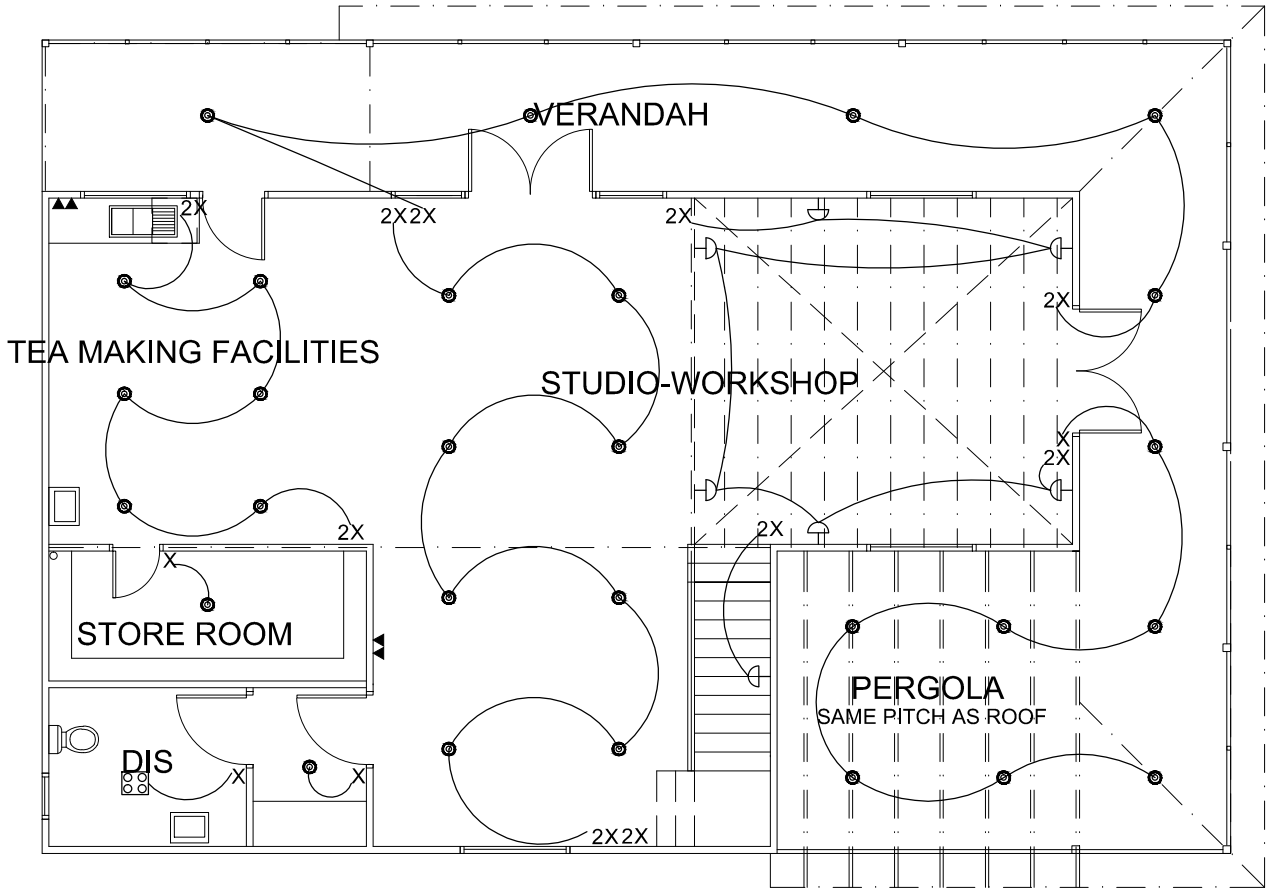
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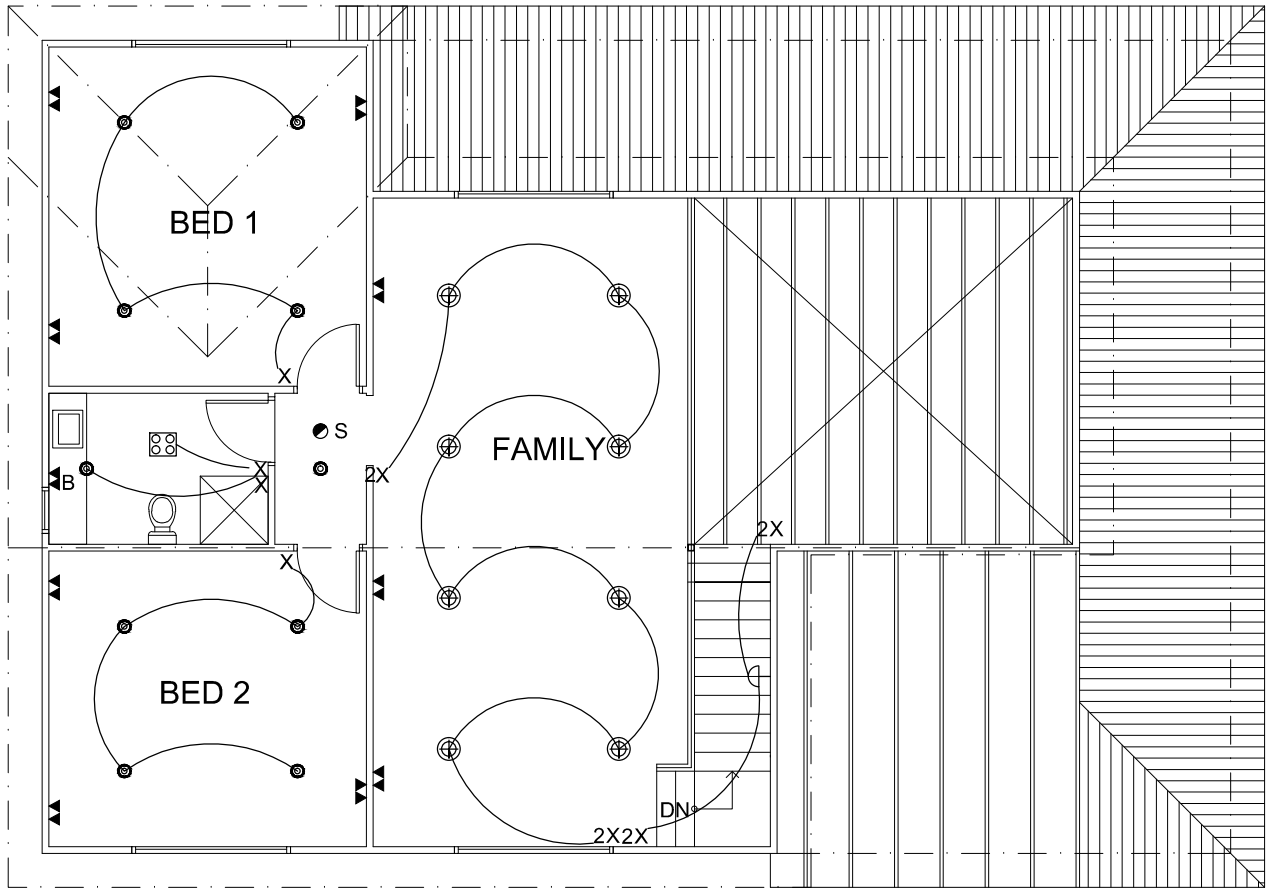
SECTION A-A

SCALE 1:50 @ A3



LOWER FLOOR PLAN

ELECTRICAL LEGEND	
ICON	ITEM
▲▲	300 AFL★ DOUBLE POWER OUTLET
▲	300 AFL★ SINGLE POWER OUTLET
▲WP	300 AFL★ WEATHER PROOF GPO
▲B	1200 AFL★ BENCH HEIGHT GPO
▲UB	600 AFL★ UNDER BENCH GPO
△TV	300 AFL★ TV POINT
△P	300 AFL★ PHONE POINT
△DATA	300 AFL★ DATA POINT
●	DOWN LIGHT LOW VOLTAGE
⊕	LIGHT BATTEN HOLDER
⌋	WALL LIGHT
⊕	PENDANT LIGHT
◇	SENSOR LIGHT
X 2	TWO WAY CIRCUIT
X	LIGHT SWITCH
⊞	3 IN 1 BATHROOM LIGHTS
■MB	METER BOX
●S	SMOKE DETECTOR HARD WIRED WITH BATTERY BACKUP
—	FLUORESCENT LIGHT
⊗	CEILING FANLIGHT
⊗	CEILING FAN
▨HWS	HOT WATER SYSTEM
⊕	EXHAUST FAN
⊕	SECURITY CAMERA
▲F	1800 AFL★ FRIDGE GPO
▲FZ	1800 AFL★ FREEZER GPO
⊞INT	INTERCOM
▲WM	600 AFL★ WASHING MACHINE GPO
▲DR	600 AFL★ DRYER GPO
▲C	GPO ON CEILING
▲DW	600 AFL★ DISH WASHER GPO
★ UNLESS OTHERWISE NOTED	



MEZZANINE FLOOR PLAN

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ELECTRICAL LAYOUT

SCALE 1:100 @ A3

WINDOW AND DOOR NOTES

CONTRACTOR MUST SEAL & FLASH ALL GAPS & CRACKS AROUND WINDOW & DOOR FRAMES PRIOR TO FIXING PLASTERBOARD

INTERNAL DOORS TO BE TIMBER FRAMED UNLESS STATED OTHERWISE.

WINDOWS TO BE ALUMINIUM FRAMED, WITH SUITABLE MATCHING FLY SCREENS TO ALL OPENABLE SASHES UNLESS STATED OTHERWISE.

ALL OPENABLE WINDOWS SHALL HAVE KEYED ALIKE LOCKS

VERIFY ALL SIZES PRIOR TO ORDERING & CONSTRUCTING

DOORS & WINDOWS TO BE HANDLED, FINISHED & INSTALLED IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS

ALL DOOR HARDWARE/FURNITURE SHALL BE AS SELECTED

WINDOW SIZES NOMINATED ARE NOMINAL ONLY. ACTUAL SIZE MAY VARY ACCORDING TO MANUFACTURER.

ALL WINDOWS & DOORS TO BE INSTALLED & FLASHED ALL AROUND IN STRICT ACCORDANCE TO MANUFACTURERS SPECIFICATIONS & THE BCA & ENERGY RATING REQUIREMENTS.

ROBE & CUPBOARD DOORS SHALL BE ALUMINIUM STEEL FRAMED SLIDING DOORS WITH PAINTED PLASTERBOARD INSERTS UNLESS STATED OTHERWISE IN SCHEDULE

ALL WC DOORS TO COMPY WITH BCA PART 3.8.3.3

GLAZING, INCLUDING SAFETY GLAZING, SHALL BE INSTALLED TO A SIZE, TYPE & THICKNESS SO AS TO COMPLY WITH
-BCA PART 3.6 FOR CLASS 1 AND 10 BUILDINGS WITHIN A DESIGN WIND SPEED OF NOT MORE THAN N3, AND;
-BCA PART B1.4 FOR CLASS 2 TO 9 BUILDINGS

SAFETY GLAZING TO BE USED IN THE FOLLOWING CASES:
ALL ROOMS
- WITHIN 500mm VERTICAL OF FLOOR LEVEL

BATHROOMS
- WITHIN 1500mm VERTICAL FROM BATH BASE
- WITHIN 50mm HORIZONTAL FROM BATH/SHOWER TO SHOWER DOORS, SHOWER SCREENS AND BATH ENCLOSURES

LAUNDRY
- WITHIN 20mm VERTICAL FROM FLOOR LEVEL AND/OR WITHIN 300mm VERTICAL OF TROUGH AND BATH ENCLOSURES

DOORWAY
- WITHIN 300mm HORIZONTAL FROM ALL DOORS

1. WINDOW GLAZING SHALL COMPLY WITH AS 1288 (2006).
2. FOR GLASS LESS THAN 500mm ABOVE FLOOR LEVEL USE ORDINARY ANNEALED GLASS AS FOLLOWS:
-UP TO 0.1msq - 3mm THICK
-0.1 TO 0.3msq - 4mm THICK
-0.3 TO 2.0msq - 5mm THICK
3. OR USE GRADE 'A' SAFETY GLAZING MATERIALS MATERIALS FROM THE GLAZING CODE AS 1288 (2006)
4. FOR SHOWER SCREENS, SHOWER DOORS OR BATH ENCLOSURES USE LAMINATED SAFETY AS FOLLOWS:
-UP TO 2.0msq -5.38mm THICK
-2.0 TO 3.0msq -6.38mm THICK
-3.0 TO 5.0msq -8.38mm THICK
OR OTHER GRADE 'A' SAFETY GLASS MATERIAL

WINDOW SCHEDULE				HEAD HEIGHT 2400 AFL (UNLESS OTHERWISE NOTED)
No.	ROOM	SIZE	TYPE	
W01	STUDIO-WORKSHOP	2140 x 1450	ASW CLEAR REUSED	
W02	STUDIO-WORKSHOP	1500 x 1500	ASW CLEAR NEW	
W03	DISABLED WC	600 x 900	AAW OBSCURE REUSED	
W04	TEA MAKING FACILITIES	1500 x 1200	ASW CLEAR NEW	
W05	STUDIO-WORKSHOP	2140x 1450	ASW CLEAR REUSED	
W06	FAMILY	1450 x 1500	ASW CLEAR REUSED	
W07	BED 2	1450 x 1500	ASW CLEAR REUSED	
W08	BATH	1200 x 900	AAW CLEAR REUSED	
W09	BED 1	1500 x 1500	ASW CLEAR REUSED	
W10	FAMILY	1500 x 1500	ASW CLEAR NEW	

DOOR SCHEDULE				HEAD HEIGHT 2400 AFL
No.	ROOM	SIZE	TYPE	
D01	ENTRY	2 (760 x 2025)	EXTERNAL FULL GLASS DOORS	
D02	TEA MAKING FACILITIES	860 x 2040	EXTERNAL	
D03	STUDIO-WORKSHOP	2 (760 x 2120)	EXTERNAL FULL GLASS DOORS WITH SIDELIGHT	
D04	LAUNDRY	920 x 2040	INTERNAL	
D05	DISABLED WC	920 x 2040	INTERNAL	
D06	STORE ROOM	860 x 2040	INTERNAL	
D07	BED 2	820 x 2040	INTERNAL	
D08	WC	820 x 2040	INTERNAL	
D09	BED 1	820 x 2040	INTERNAL	

AFW	ALUMINIUM FIXED WINDOW
AAW	ALUMINIUM AWNING WINDOW
ASW	ALUMINIUM SLIDING WINDOW
ADH	ALUMINIUM DOUBLE HUNG WINDOW
ASH	ALUMINIUM SINGLE HUNG WINDOW
ACW	ALUMINIUM CASEMENT WINDOW
TFW	TIMBER FIXED WINDOW
TAW	TIMBER AWNING WINDOW
TSW	TIMBER SLIDING WINDOW
TDH	TIMBER DOUBLE HUNG WINDOW
TSH	TIMBER SINGLE HUNG WINDOW
TCW	TIMBER CASEMENT WINDOW

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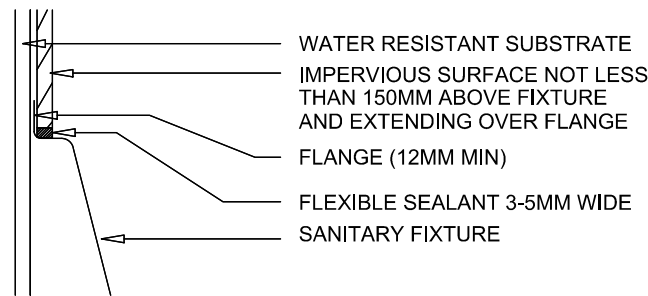
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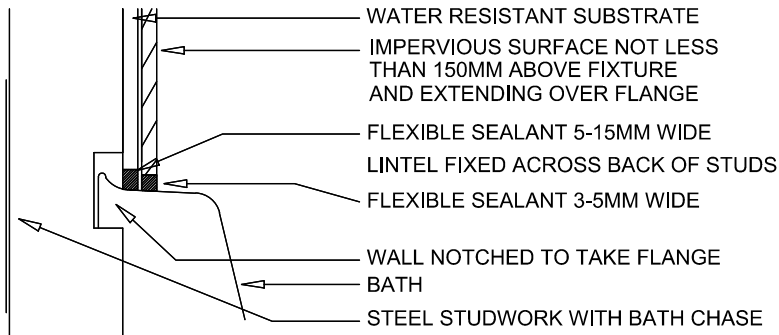
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WINDOW AND DOOR SCHEDULE

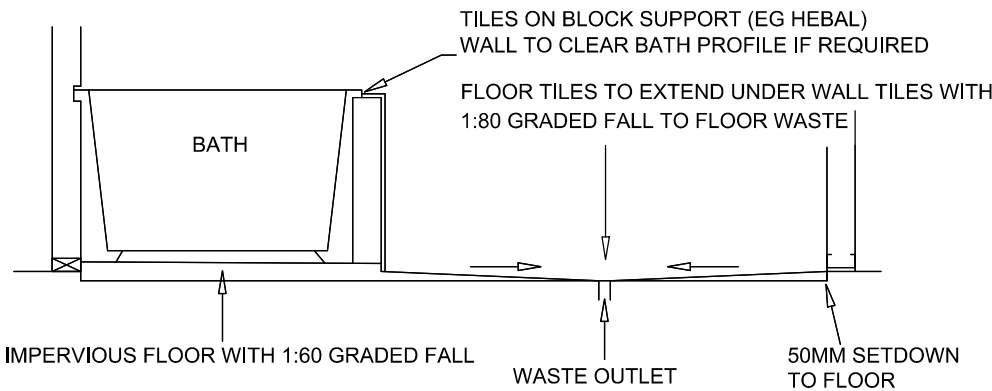
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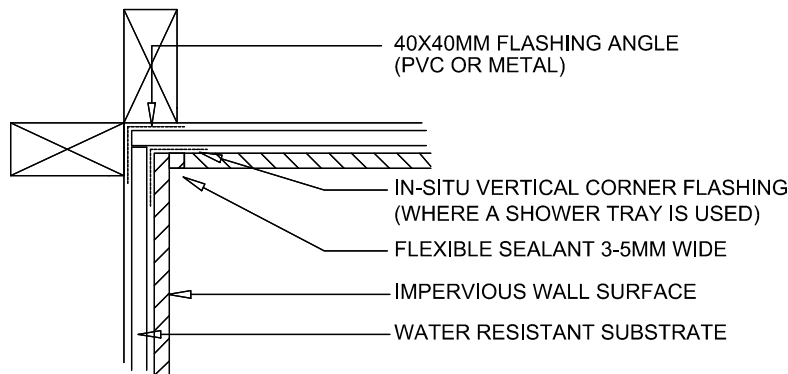
SANITARY FIXTURES ABUTTING WALLS



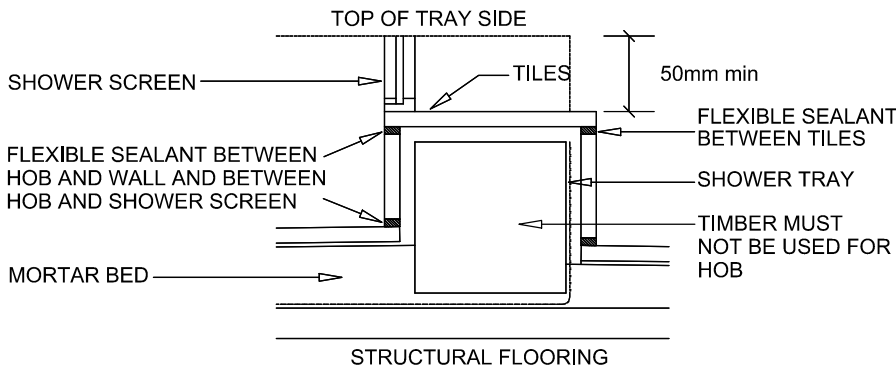
BATH CHASE DETAIL



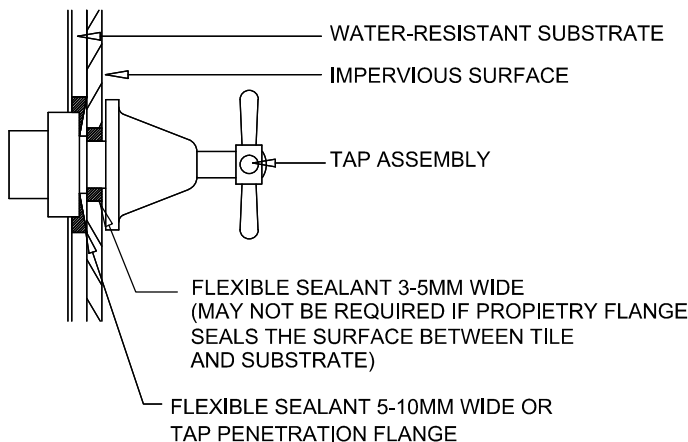
FLOOR GRADES TO WASTE OULETS



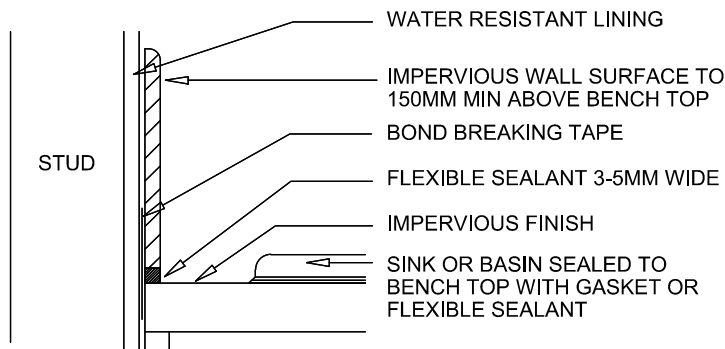
WALL JUNCTION



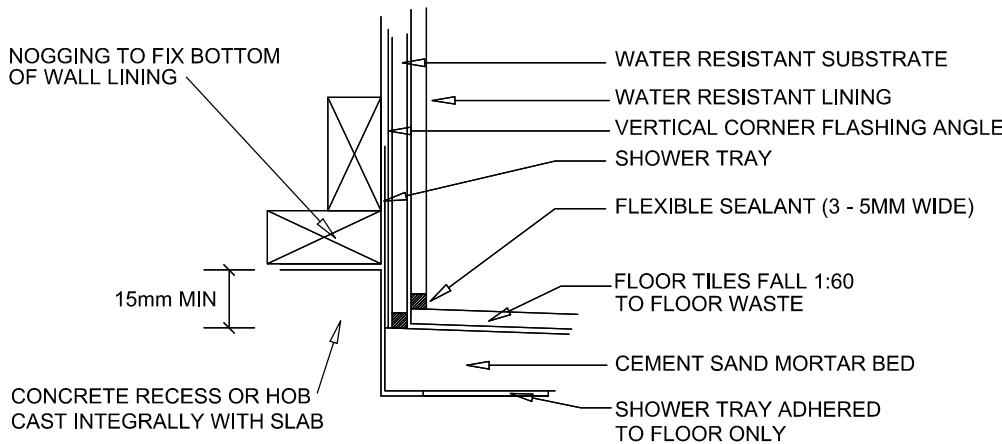
HOB AT SHOWER TRAY



PENETRATIONS THROUGH WALLS
IN SHOWER AREAS



WALL AND BENCH TOP JUNCTION
WITH INSET FIXTURE



WATERPROOFING OF SHOWER
WALL AND FLOOR JUNCTIONS


WET AREA NOTATIONS
TO COMPLY WITH VOLUME TWO OF BCA 2015,
REQUIREMENTS OF BCA PART 3.8.1 (Wet Areas)

SHOWER AREA (Cat. 1)
FLOOR - IMPERVIOUS WITH SHOWER TRAY OR
BASE
DRAINAGE - 1:60 MIN GRADE TO SHOWER
TRAP. SHOWER TRAYS DRAINED TO FLANGE IN
SHOWER TRAP
BATH AND LAUNDRY
FLOOR - IMPERVIOUS
DRAINAGE - BATH - 1:80 MIN TO FLOOR
TRAP. MAY BE DRAINED TO SHOWER TRAP
WHERE HOB NOT USED.
DRAINAGE - LDRY - 1:80 MIN TO TRAP
SINGLE ROOM CONTAINING WC
FLOOR - IMPERVIOUS
DRAINAGE - NO REQUIREMENT

NOTE: FLOORING TO BE OF IMPERVIOUS FLOOR
SHEETING OR SIMILAR APPROVED FLOORING AS
SPECIFIED

INSTALLATION OF PREFORMED SHOWER
BASES SHALL COMPLY WITH THE
REQUIREMENTS OF THE MANUFACTURERS
SPECIFICATIONS. ANY ALTERNATIVE
WATERPROOFING SYSTEM SHALL BE AN
ACCREDITED INSTALLATION SYSTEM IN
ACCORDANCE WITH THE MANUFACTURERS'

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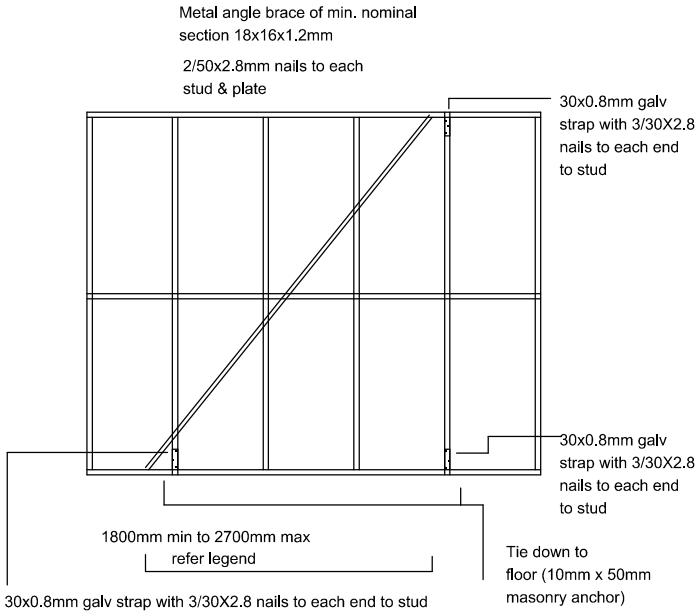
PHONE: 0419851867
WEBSITE: www.diamantiddesign.com.au
EMAIL: jim@diamantidd.com.au

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OVER SCALED DRAWINGS. VERIFY DIMENSIONS
AND LEVELS BEFORE COMMENCING ANY
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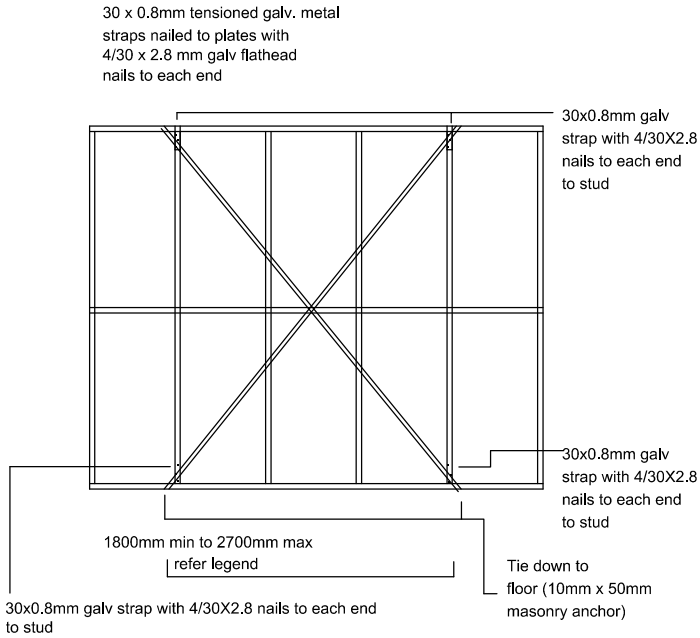
WET AREA DETAILS

TYPE A BRACING (1.5kN/m)
METAL ANGLE BRACE. THE MAXIMUM DEPTH
OF SAW CUT NOT TO EXCEED 20mm



TYPE B BRACING (3.0kN/m)

DOUBLE DIAGONAL TENSION BRACE



STANDARD FIXING & TIE-DOWN REQUIREMENTS

28 m/s SHEET ROOF

BOTTOM PLATES TO CONCRETE SLABS INCLUDING WALLS WITH TYPE A BRACES:

- 1/75 mm MASONRY NAIL AT MAX. 1200 C

BOTTOM PLATES TO CONCRETE SLAB FOR WALLS WITH TYPE B BRACES:

- ADDITIONAL 1/M10 BOLTS OR EQUIVALENT MASONRY ANCHOR AT EACH END OF BRACING UNIT (MAX. 1200 mm C)

NOTE: THESE FIXINGS ARE ONLY REQUIRED TO TIE DOWN BRACED PANELS.

BOTTOM PLATES TO STUDS:

- 2/75 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 38 mm THICK
- 2/90 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 50 mm THICK

BOTTOM PLATES TO STUDS:

- 2/75 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 38 mm THICK
- 2/90 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 50 mm THICK

ADDITIONAL 30 X 0.8 mm G.I. STRAP FIXED WITH 3/25 X 2.5 mm NAILS

INTO THE SIDE OF EACH MEMBER

NOTE: MAX. TIE DOWN SPACING SHALL BE 1800 mm OR THREE STUD SPACINGS

WHICHEVER IS THE LESSER TO A MAX. TIE DOWN DISTANCE OF 6000 mm

FOR TIE-DOWN DISTANCE OVER 6000 mm A TIE-DOWN IS REQUIRED

FOR EACH MEMBER

RAFTERS TO TOP PLATES:

- 2/75 mm SKEW NAILS
- ADDITIONAL 30 X 0.8 mm G.I. STRAP FIXED WITH 3/25 X 2.5 mm NAILS

INTO THE SIDE OF EACH MEMBER

NOTE: MAX. TIE DOWN SPACING SHALL BE 1800 mm OR THREE STUD SPACINGS

WHICHEVER IS THE LESSER TO A MAX. TIE DOWN DISTANCE OF 6000 mm

FOR TIE-DOWN DISTANCE OVER 6000 mm A TIE-DOWN IS REQUIRED

FOR EACH MEMBER

SECONDARY TOP PLATES (RIBBON PLATES TO TOP PLATES):

- 2/75 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 38 mm THICK
- 2/90 mm NAILS AT MAXIMUM 600 mm C FOR PLATES TO 50 mm THICK

ROOF TRUSSES TO TOP PLATES:

- IN A/W MANUFACTURES RECOMMENDATIONS (GENERALLY 1 FRAMING ANCHOR)
- ADDITIONAL ROOF TRUSS TIE-DOWN IN A/W TIE DOWN REQUIRED FOR CONVENTIONAL ROOFS

ROOF PURLINS TO RAFTERS:

- FOR A SHEET ROOF WITH RAFTER SPACING OF 1200 mm, 2/75 GROOVED ANNULAR NAILS SKEW NAILED

COLLAR TIES:

- TO 4.2 m LONG, 2/75 mm NAILS EACH END
- OVER 4.2 m LONG, 1 M10 BOLT EACH END

REFER TO APPENDIX G AS 1684.2-1999 TO OBTAIN DETAILS OF FIXINGS
REFERRED TO. INTERMEDIATE TIE-DOWN IS NOT REQUIRED WHERE
TIE-DOWN CONNECTIONS ARE CONTINUED THROUGH TO A LOWER MEMBER
IN THE FRAME

TYPE A HARDBOARD BRACING (6.4mm) 3.4kN/m

- A P.9 3.0 kN (2 type A top fixings)
or (1 type D (a) top fixing)
- A P1.2 4.0 kN (2 type A top fixings)
or (1 type D (b) top fixing)
- A 1.8 (2.7kN) (2 type A top fixings)
or (3 type C top fixings)
- 2.1 (3.1kN) (2 type A top fixings)
or (3 type C top fixings)
- 2.4 (3.6kN) (2 type A top fixings)
or (4 type C top fixings)
- 2.7 (4.0kN) (2 type A top fixings)
or (4 type C top fixings)

NOTE:

All bracing capacities up to 2700mm high walls. For walls over 2700mm refer to page 145 table 8.19 of AS1684.2-1999

TYPE B HARDBOARD BRACING (6.4mm) 6.0kN/m

- B P.9 5.4 kN (1 type B top fixing)
or (2 type D (a) top fixings)
- B P1.2 7.2 kN (1 type B top fixing)
or (2 type D (a) top fixings)
- B P.6 2.7 kN (1 type B top fixing)
or (1 type D (a) top fixing)
with M10 x 50mm coachscrew & 38x38 sq. washer to each corner
- B P.45 2.7 kN (1 type B top fixing)
or (1 type D (a) top fixing)
with M10 x 50mm coachscrew & 38x38 sq. washer to each corner
- B 1.8 (5.4kN) (1 type B top fixing)
or (2 type D (a) top fixings)
- 2.1 (6.3kN) (1 type B top fixing)
or (2 type D (a) top fixings)
- 2.4 (7.2kN) (1 type B top fixing)
or (2 type D (a) top fixings)
- 2.7 (8.1kN) (1 type B top fixing)
or (2 type D (a) top fixings)

Hardboard shall be nailed to frame using minimum 30x2.8mm galvanised nails or equivalent.
Nails shall be located a minimum of 10mm from the vertical edges and 15mm from the top and bottom edges panel edges shall be supported by studs

Fastener spacing
top plate 50mm
bot plate 50mm
vertical edges 100mm
intermediate studs 300mm

Tie down to slab
2/10mm x 50mm masonry
anchors to each side of
bracing panel

Hardboard shall be nailed to frame using minimum 25x2.8mm galvanised nails or equivalent.
Nails shall be located a minimum of 10mm from the vertical edges and 20mm from the top and bottom edges
panel edges shall be supported by studs

30 X 0.8MM METAL STRAP
LOOPED OVER PLATE AND
FIXED TO STUD WITH
4/30 X 2.8MM
FLATHEAD NAILS TO
EACH END

Fastener spacing
top plate 100mm
bot plate 100mm
vertical edges 100mm
intermediate studs 300mm

Tie down to slab
10mm x 50mm masonry
anchor to each side of
bracing panel

BRACING DETAILS

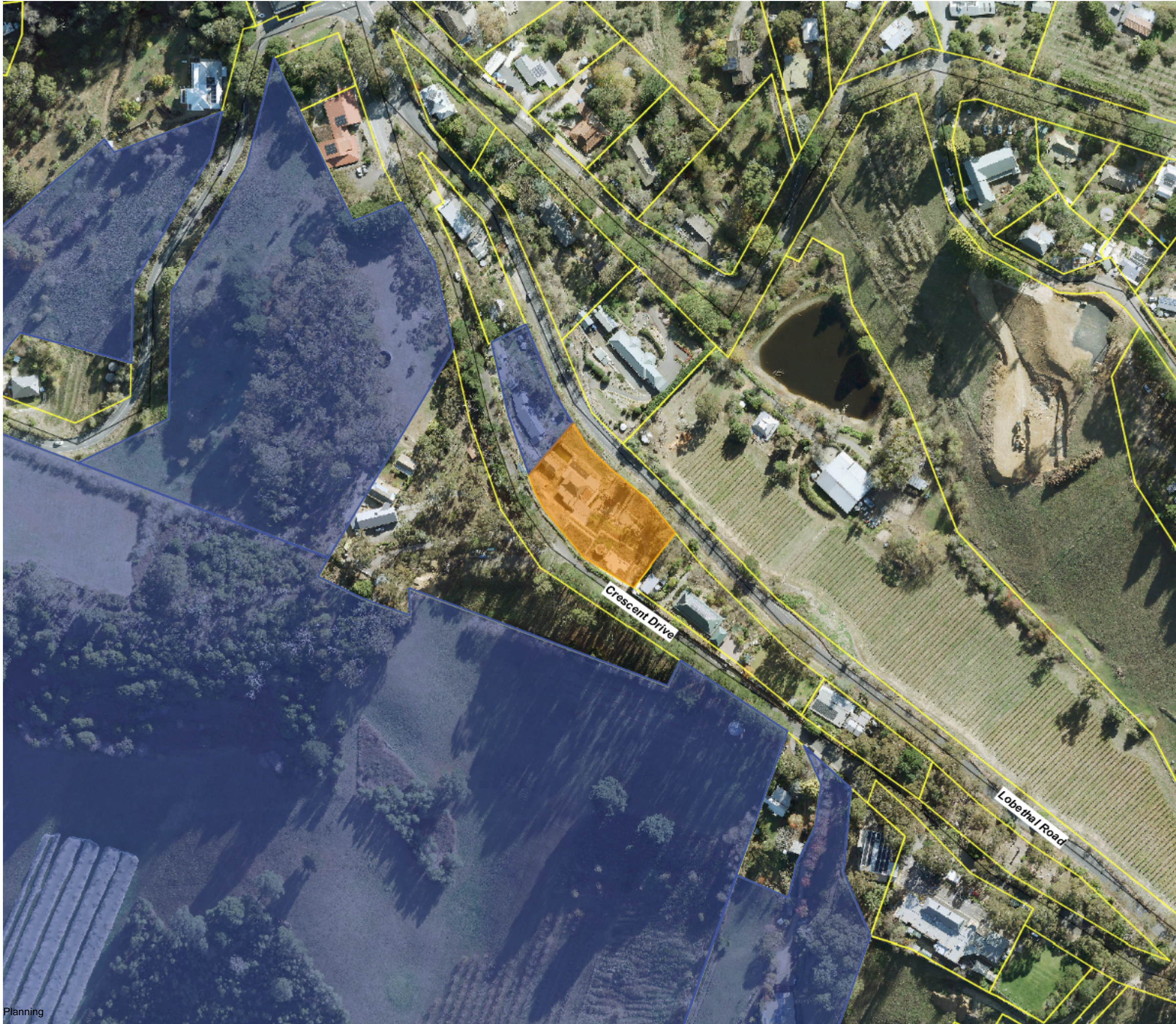
CLIENT:
MICHAEL VELONAKIS
PROPOSED:
PROPOSED B&B TOURIST
ACCOMMODATION AND EXISTING
RESIDENCE
SITE ADDRESS:
31 CRESCENT DRIVE
NORTON SUMMIT
DATE: DECEMBER 2022
DRAWN: GR
SHEET: 15 OF 15
JOB No.

 BUILDING
DESIGN &
PROJECT
MANAGEMENT
PO Box 551, Marleston
SA, 5033

PHONE: 0419851867
WEBSITE: www.diamantidesign.com.au
EMAIL: jim@diamantidd.com.au

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REPORTED TO THE DESIGNERS IMMEDIATELY.





31 Crescent Drive,
Norton Summit



Annotations

-  Subject land
-  Representor's land - 15 Crescent Drive
-  Representor's land - 44 Crescent Drive
-  Representor's land - 467A Old Norton Summit Road
-  Representor's land - 412 Old Norton Summit Road

AHC Core

-  Property_Owner
-  Roads

Scale = 1:2000

50 m

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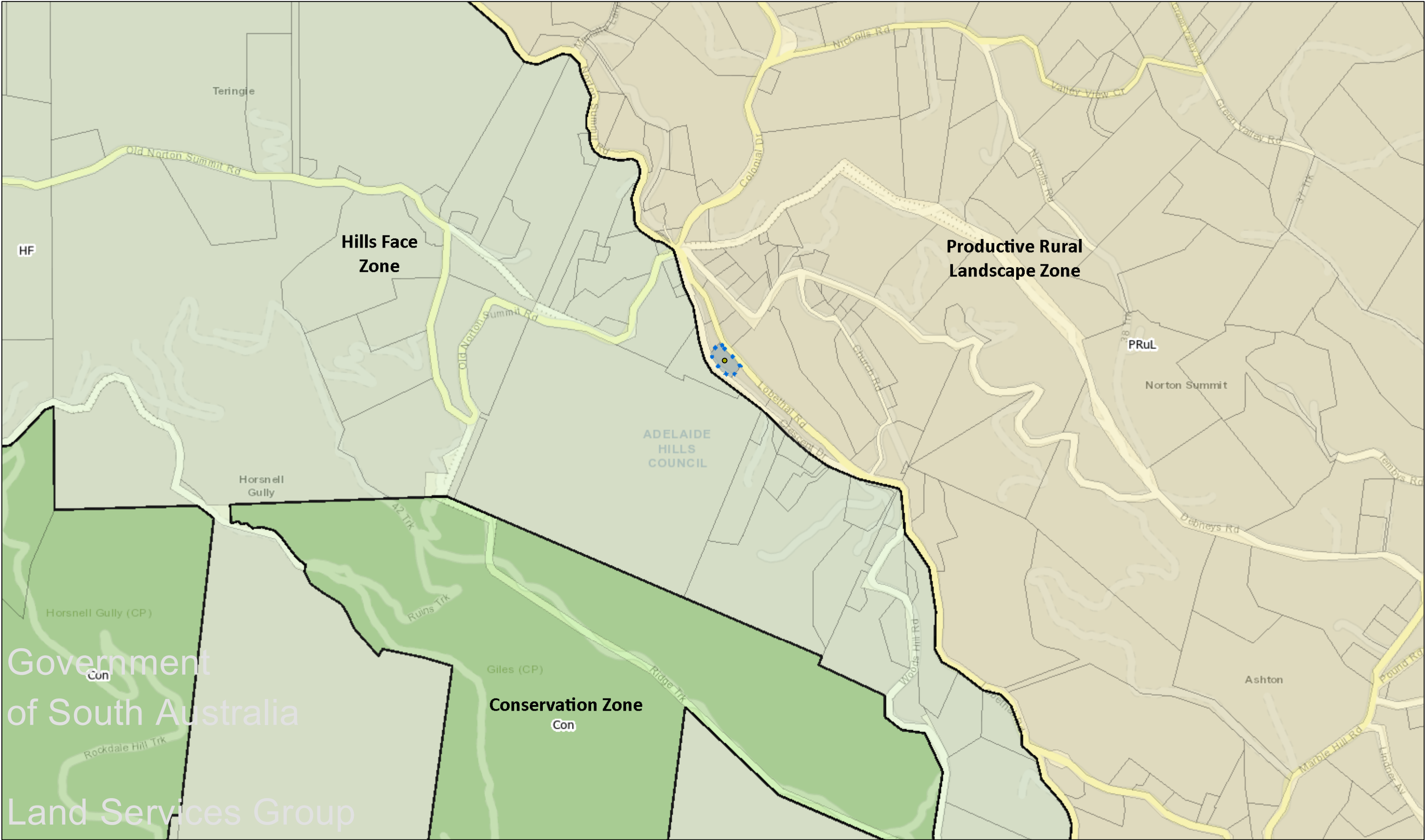
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SAPPA Report

The SA Property and Planning Atlas is available on the Plan SA website: <https://sappa.plan.sa.gov.au>

Date created:
December 7, 2022

Zoning Map



Disclaimer: The information provided above, is not represented to be accurate, current or complete at the time of printing this report. The Government of South Australia accepts no liability for the use of this data, or any reliance placed on it.

Details of Representations

Application Summary

Application ID	21027228
Proposal	Partial change of use of existing dwelling to include tourist accommodation and construction of 2 storey tourist accommodation building.
Location	31 CRESCENT DR NORTON SUMMIT SA 5136

Representations

Representor 1 - Fabian Booth

Name	Fabian Booth
Address	PO Box 309 MAGILL SA, 5072 Australia
Submission Date	17/10/2022 09:51 AM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development

Reasons

Thankyou for the opportunity to view and reply to the development submission for 31 Crescent Drive Norton Summit. While I would love to see positive change at this property I feel the plan is lacking in a number of areas. 1. Effluent and Waste Water Management – I noted that the plan has a “Fully Engineered Septic Tank” but was surprised that I couldn't find any details about it. Who engineered it? What are its specifications? While there is a mention of a soakage pit there is no further details of a dedicated irrigation area or offsets from bores and buildings. Am I to assume the idea with this system is to have the effluent soak straight down into the water table? As a “Primary Production Watershed” allotment this should be of utmost importance. I and many others on the street rely on bore water, so I'm very interested about how or where this waste water will end up, let alone any surface effluent that may end up in the catchment. It is my understanding that all changes to Adelaide Hills properties need to have an Aerobic Waste System as a minimum. Please correct me if I'm wrong. 2. New building is a house - The new building for the workshop/bed and breakfast could easily become a second house without any structural changes. By putting some kitchen cabinetry in the “tea making area” and changing “workshop” to “lounge” it would be a house in its own right. What is to stop the owner closing the Bed and Breakfast in a few months time and renting out the second house as a full-time residence? 3. CFS Requirements - I noticed the application has a CFS truck drawn on the driveway but I was unable to see how the truck would be able to do a 3 point turn the leave the property, even if the 5 car carpark was empty. Again, correct me if I'm wrong but it is my understanding that the CFS require a space large enough to turn around in an emergency. 4. Disability Access - This plan does not mention anything about wheel-chair ramps or disability access. Is this a strategic part of Mr Velonakis' business plan that disabled people will be excluded from his establishment? In summary, I found myself asking where is the compelling reason for this development? When I think of a Bed and Breakfast in the beautiful Adelaide Hills I think of a stone cottage, open fire place, rammed earth or hay bale, clean/sustainable/green. Instead I feel the application is just a maze of concrete blocks with a 2 storey house, 5 car carpark and a septic system that has nowhere for the effluent/grey water to go other than down into the aquifer below. This development could only be described as “concrete jungle with a thinly veiled attempt to sub-divide by stealth”. Fabian Booth

Attached Documents

Representations

Representor 2 - Paul Atkinson

Name	Paul Atkinson
Address	PO Box 180 NORTON SUMMIT SA, 5136 Australia
Submission Date	18/10/2022 03:48 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

Proposed shop (cellar door) The document entitled "VerificationOutcome-PlanningConsent-Application21027228-1205047.pdf" describes the proposed development as: "Tourist accommodation, shop (cellar door) and ancillary car parking". None of the other documents, including the Working Drawings, make any reference to a shop or cellar door. There is a concern the presence of a retail business on the premises will generate additional traffic on Crescent Drive and result in cars being parked either on the grass verges or the roadway itself. Intended use of the new building The application description on the PlanSA website describes the proposed development as "Partial change of use of existing dwelling to include tourist accommodation and construction of 2 storey tourist accommodation building." The Floor Plans (Working Drawing Sheet 4 of 15) shows that the lower floor of the new building will be a studio/workshop with tea making facilities and a store room, which appear to be unrelated to the proposed use as tourist accommodation. There is a concern the lower floor will be used for other activities which will generate additional traffic on Crescent Drive and result in cars being parked either on the grass verges or the roadway itself. There is also a concern that the new building may be converted to a separate dwelling in the future. Appearance from Crescent Drive The total height of the new building to the roof high point is approximately 6.4m from floor level or about 5.4m from the road level and the building is located only about 7m from the edge of the road. This will present an imposing feature to the street that is not in keeping with the existing streetscape which has single-storey buildings set partially or entirely below street level. The proposed building will obstruct views from the street to the north-east. In addition, the side of the new building facing the street is relatively featureless with only four windows and no doors. This is not an "inviting" view from the street and has the appearance of being the rear of the building. Level area adjacent to the road The Proposed Site Plan (Working Drawing Sheet 2 of 15) shows an area adjacent to Crescent Drive labelled "this area level with the road". It is not clear what this area will be used for but there is a concern it may be used for additional car parking or another structure such as a shed or garage.

Attached Documents

Representations

Representor 3 - Michael Richardson

Name	Michael Richardson
Address	33 Carrington Street ADELAIDE SA, 5000 Australia
Submission Date	20/10/2022 01:28 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons Please see attached correspondence.	

Attached Documents

51621LET04-1128136.pdf

20 October 2022

Adelaide Hills Council

Via: PlanSA Portal

Attention: Mr Sebastian Paraskevopoulos

Dear Mr Paraskevopoulos

**Re: Representation on Development Application 21027228
31 Crescent Drive, Norton Summit**

MasterPlan have been engaged by The Giles Family, the owners 'Grove Hill', being the allotment located at 412 Old Norton Summit Road, Norton Summit, to review the abovementioned development application for a partial change of use to include tourist accommodation and the construction of a two-storey tourist accommodation building. We provide herein, our planning opinion with respect to the merits of the proposal when considered against the relevant requirements of the Planning and Design Code ('the Code').

For reference, the Locality Plan **enclosed** shows the location of Grove Hill and the subject site of the proposed development.

The following summarises the deficiencies and failures of the proposal under a number of key considerations of the Code, namely, the desired siting, design and visual prominence of buildings proposed to be located within the Productive Rural Landscape Zone and the desirability of the land use.

On this basis, we encourage the relevant authority to refuse the development in its current form, on the basis that the application does not warrant Planning Consent.

Building Height and Visual Impact

The proposed new studio and tourist accommodation building is to be two storeys in height. Whilst it is noted that it was previously approved as a 'studio' with the same, or similar footprint, the previous approval was for a single-storey building. The application now lodged proposes that the height of the building is to be a two-storey building.





PO 11.1 of the Productive Rural Landscape Zone seeks that large buildings do not disturb the scenic and rural vistas of a locality by:

- a. *having substantial setbacks from boundaries and adjacent public roads*
- b. *using low reflective materials and finishes that blend with the surrounding landscape*
- c. *being located below ridgelines.*

Whilst it is noted that policy does envisage that a maximum building height of two storeys can be appropriate in the Production Rural Landscape Zone, such a proposal must be carefully balanced against the context within which such a building is proposed, and the other quantitative and qualitative policy relevant to the proposal.

Existing development along Crescent Drive achieves this through its location below the ridge line, restricting visual impacts to the Lobethal Road frontages of each property. Each building is also single storey in height, minimising the visual impacts by maintaining an appropriately contextual scale. The proposed development adopts a wholly different approach, without due regard to the existing context.

The existing visual condition of the subject site and the locality is exhibited in the images **below**.



Image 1: View of the affected area of the subject land.



Image 2: Pattern of existing development under ridge line from Crescent Drive (south-west).



Image 3: Pattern of existing development on Crescent Drive (from the north).



Whilst built form is visible throughout the locality, the height and siting of the dwellings are predominantly subservient to the rural and scenic vistas that are a significant feature of the area. The consistency of siting and height (particularly along Crescent Drive) further reinforces the relationship between the built and natural environment and reflects a deliberate desire to maintain these scenic vistas.

The aims of PO 11.1 reflect this existing pattern of development and seek that buildings are well setback from allotment boundaries and located below ridgelines.

The proposed two-storey development, being visible above the ridgeline and with a limited setback, would be highly visible from Crescent Drive and the locality as a whole. The plans provided by the applicant do not clearly include a finished floor level for the two-storey tourist accommodation building, but it is considered likely, based on the site levels and the existing concrete slab, that it would be between 1.0 metres to 1.5 metres lower than the level of Crescent Drive. This leaves potentially 5.5 metres of built form that will be visible above the street level height of Crescent Drive, creating a substantial visual impact on an otherwise largely unobstructed view when it is viewed from Grove Hill, contrary to the aims of PO 11.1.

Image 3 above, shows the existing development along Crescent Drive, viewed from the north across the valley on Church Road. The image demonstrates that existing single-storey development is set predominantly below the ridgeline, with only the dwelling and shed on the allotment to the east of the subject site and the dwelling on the subject site having small portions of their roof structures protruding above the ridge line. Built form along Crescent Drive is, for the most part, backed by ground, rather than 'skylining' above the height of the ridge. This siting provides for a character of buildings being located within the landscape, rather than being dominant development sited upon the ridgeline itself.

Further, PO 6.4 of the Zone outlines that:

'Tourist accommodation proposed in a new building or buildings are sited, designed and of a scale that maintains a pleasant rural character and amenity.'

The proposed development, through its height and siting on the high side of the site, does not adequately achieve this. As previously discussed, the consistency of built form and the subservience of development to the pleasant vistas available are significant factors in the maintenance of this character and amenity for both the locality as a whole and our client's property at Grove Hill.

The visibility of the building will interrupt these views and consistency in a manner that is inconsistent with both PO 6.4 and PO 11.1. The development is not significantly setback from any allotment boundary and, by virtue of its location, occupies a visually prominent location in the locality.

Having regard to the existing context of the locality, particularly in respect of the design and siting of dwellings on the northern side of Crescent Drive, we consider that the proposed siting and two-storey form of the proposed building are significantly inconsistent with the policy requirements for buildings which integrate with and preserve the scenic amenity of landscapes within the Productive Rural Landscape Zone.



Unlike any other existing built form in the locality, the proposed building will extend substantially above the ridgeline, with a substantial portion of the height of the dwelling, including more than one storey of the façade and the entire roof form, protruding above the ridge line and being backed by sky when viewed from the north. The form of the proposed dwelling will be jarringly inconsistent with all other built form in the locality. Considering the position of the subject site, where development is highly prominent to the views extending to the north and east, and to Grove Hill in the south-west, the siting and form of the proposed building are considered to be so significantly inconsistent with PO 11.1 and PO 6.4 that the development application should be refused Planning Consent on this issue alone.

State Heritage Place

Our clients' land, 412 Old Norton Summit Road, is also known as Grove Hill and is listed as State Heritage Place 15165. Grove Hill is listed in recognition of its status as likely the oldest continuously operating orchard in South Australia.

The proposed development is therefore to be assessed against PO 1.1 of the Heritage Adjacency Overlay to ensure it *"does not dominate, encroach on or unduly impact on the setting of the Place"*.

Part of the existing setting of Grove Hill, incorporating the portion with view up the gully toward Crescent Drive, is shown in **Image 4** below.



Image 4: View towards Crescent Drive ridge from Grove Hill (south).



Due to the topography of the land, it is considered highly likely that the proposed two-storey element of the development will be visible from Grove Hill. The presently undisturbed vista will be impacted upon by the proposed developments' height and siting adjacent to the ridgeline. The visibility of the development will be even greater from positions higher up the slope, with the position in **Image 4** above likely to be the "*least*" visually impactful. This again reinforces the rationale for building below ridgelines in a manner that is consistent with the pattern of development in the locality.

It is considered that due to the minimal measures taken by the applicant to accord with the relevant siting and land use policies of the Zone, that the impacts upon the setting are greater. The lack of policy support for the development also makes the impacts more "*unduly*" and contrary to PO 1.1 of the Heritage Adjacency Overlay.

Intensity and Traffic

The proposed plans show 'tea-making facilities' being located on the ground floor of the two-storey tourist accommodation building. Whilst we do not seek to question the validity of the applicant's proposal, further clarification should be sought from Council regarding the nature of the facilities and their availability to those using the accommodation.

The provision of a kitchenette-type facility alters the nature of the operations of the accommodation and would likely encourage longer-term stays and potentially invite visitors to effectively provide meals for themselves. This is outside of what is typically termed a 'Bed and Breakfast', which has been specified and repeatedly referenced by the applicant and would potentially allow for a greater intensity of development than what is contemplated.

Furthermore, the increase in traffic volumes on Crescent Drive, a narrow, low-volume service road that the development will result in, appears not to have been contemplated by the applicant. Whilst the required car parking is provided on-site, the increase in traffic in the locality places additional pressure on Crescent Drive and may detrimentally affect the amenity of adjacent properties.

We further note that the Norton Summit Primary School is located at the end of Crescent Drive, with many vehicles accessing the school during morning and afternoon periods. The proposal has the potential to add significant additional vehicle movements to Crescent Drive. No information has been provided in the documents notified to confirm that Crescent Drive is capable of accepting the additional vehicle movements which the proposal will generate. It is also noted that Crescent Drive is typically closed during school hours, which the applicant has failed to address.



Therefore, we seek that Council further clarifies the intended operations of the development to prevent long-term stays and gives consideration to the impacts upon the local street network, and whether there is sufficient capacity to accept the vehicle movements that will be generated by the proposed development.

Land Use

PO 6.3 of the Productive Rural Landscape Zone outlines that tourist accommodation is appropriate where it:

'is associated with the primary use of the land for primary production or primary production related value adding industry to enhance and provide authentic visitor experiences.'

The proposed tourist accommodation is not associated with primary production in any way. The authenticity of the visitor experience sought by the provision is therefore lacking and leveraging off land uses on adjacent properties and the general character of the locality. Value adding, whilst envisaged within the Zone, is intended to support primary producers, rather than residents seeking to gain additional income.

The associated DPF 6.3 gives an indication of the expected intensity of tourist accommodation that is deemed supportable within the Zone. Specifically, DPF 6.3(c) outlines that no more than one (1) facility should be located on the same allotment. Whilst the application states that both operations will be managed together, they are within separate structures and are therefore separate facilities. The provision clearly seeks to limit the intensity of tourist accommodation which it is considered that the proposed development does not adequately satisfy.

We contend that development is contrary to the provisions relating to tourist accommodation within the Productive Rural Landscape Zone.

Closure

Having regard to the provisions of the Code, we conclude that the proposal does not warrant Planning Consent. The applicant has not demonstrated that the development will satisfy the Desired Outcomes and PO 2.2, PO 6.3, PO 6.4 and PO 11.1 of the Productive Rural Landscape Zone due to its height, visual impact and non-existent links of the tourist accommodation use to primary production.

The failures in all these respects result in a proposal that is very likely to have a substantial detrimental impact on the locality, the character of the area and the setting of a State Heritage Place. Ultimately, the proposed development is deficient and contrary to the policies of the Planning and Design Code.

For these reasons, the proposal should be refused Planning Consent.

Should you require any further information or clarification in respect of our submission, please contact the writer.



Appearance at Council Assessment Panel

Our client has instructed that we attend the Council Assessment Panel meeting at which the application is considered to support this representation with a verbal submission.

Please advise the writer of the timing and location of the relevant meeting.



Yours sincerely

Michael Richardson
MasterPlan SA Pty Ltd

enc: Locality Plan.

cc: Grove Hill (by email).



-  Grove Hill
-  Subject Site - DA21027228

LOCALITY PLAN

31 CRESCENT DRIVE
NORTON SUMMIT

for THE GILES FAMILY

Representations

Representor 4 - Anna McDonald

Name	Anna McDonald
Address	PO Box 99 NORTON SUMMIT SA, 5136 Australia
Submission Date	21/10/2022 11:05 AM
Submission Source	Email
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

- The proposed 2 storey b&b tourist building does not comply – given the size and layout of this proposed building it could easily be converted to a residential dwelling which is not in line with the policies that apply to this site - Can you confirm the proposed uses on site – 1 residential dwelling, 2 x b&b units (sleeping 4 people each) and a café/tea room? - Further details on the sewage system are required to ensure that the system can cope with all the people that could be on the property at one time – the 8 people in the b&bs and the people in the residential dwelling - The car parking provision needs to be checked to ensure that it is sufficient for the proposed development uses – Crescent Drive is a narrow road with no footpaths – it forms part of the Heyson Trail, is close to a school and forms part of a regular walking loop for local residents – it also is busy with school traffic and with parking from the Scenic Hotel during their busy times – there is little capacity on the road for more on-street parking with causing safety and amenity issues for people that use the street - Can Council confirm whether the proponent has resolved the previous issues with the concrete blocks placed on site that ‘collapsed’? Unfortunately, at the time of preparing this submission the PlanSA website is undergoing maintenance and I am unable to access the policies that apply to this site – so am unable to provide any further detail other than the above.

Attached Documents

AnnaMcdonald-Representation_on_application_-21027228-4101257.pdf

AnnaMcdonald-Representation_on_application_-21027228-Email-4101258.pdf

REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

Planning, Development and Infrastructure Act 2016

Applicant:	Michael Velonakis <i>[applicant name]</i>
Development Number:	21027228 <i>[development application number]</i>
Nature of Development:	Partial change of use of existing dwelling to include tourist accommodation and construction of 2 storey tourist accommodation building ■ <i>[development description of performance assessed elements]</i>
Zone/Sub-zone/Overlay:	Click here to enter text. <i>[zone/sub-zone/overlay of subject land]</i>
Subject Land:	31 CRESCENT DR NORTON SUMMIT SA 5136 <i>[street number, street name, suburb, postcode]</i> <i>[lot number, plan number, certificate of title number, volume & folio]</i>
Contact Officer:	Click here to enter text. <i>[relevant authority name]</i>
Phone Number:	Click here to enter text. <i>[authority phone]</i>
Close Date:	20 October 2022 <i>[closing date for submissions]</i>

My name*: Anna McDonald	My phone number: 0402 468 926
My postal address*: PO Box 99 Norton Summit 5136	My email: annamcd5136@icloud.com

* Indicates mandatory information

My position is:	<input type="checkbox"/> I support the development <input type="checkbox"/> I support the development with some concerns (detail below) <input checked="" type="checkbox"/> I oppose the development
-----------------	--



Government of South Australia

Department for Trade
and Investment

The specific reasons I believe that planning consent should be refused are:

- *The proposed 2 storey b&b tourist building does not comply – given the size and layout of this proposed building it could easily be converted to a residential dwelling which is not in line with the policies that apply to this site*
- *Can you confirm the proposed uses on site – 1 residential dwelling, 2 x b&b units (sleeping 4 people each) and a café/tea room?*
- *Further details on the sewage system are required to ensure that the system can cope with all the people that could be on the property at one time – the 8 people in the b&bs and the people in the residential dwelling*
- *The car parking provision needs to be checked to ensure that it is sufficient for the proposed development uses – Crescent Drive is a narrow road with no footpaths – it forms part of the Heyson Trail, is close to a school and forms part of a regular walking loop for local residents – it also is busy with school traffic and with parking from the Scenic Hotel during their busy times – there is little capacity on the road for more on-street parking with causing safety and amenity issues for people that use the street*
- *Can Council confirm whether the proponent has resolved the previous issues with the concrete blocks placed on site that ‘collapsed’?*

Unfortunately, at the time of preparing this submission the PlanSA website is undergoing maintenance and I am unable to access the policies that apply to this site – so am unable to provide any further detail other than the above.

[attach additional pages as needed]

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
 - [Click here to enter text.](#) *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission*
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally
☐ being represented by the following person: [Click here to enter text.](#)

**You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature: Anna McDonald

Date: 20 October 2022

Return Address: [Click here to enter text.](#) *[relevant authority postal address]* or

Email: [Click here to enter text.](#) *[relevant authority email address]* or

Complete online submission: planninganddesigncode.plan.sa.gov.au/haveyoursay/

Kim Sharp

From: McDonald, Anna (DIT) <Anna.McDonald@sa.gov.au>
Sent: Thursday, 20 October 2022 9:14 PM
To: Development Admin
Subject: Comments on DA 21027228
Attachments: Representation_on_Application_-_Performance_Assessed_Development.docx

Categories: Kim

[EXTERNAL]

UNOFFICIAL

Please find attached comments on DA 21027228 – these comments are being emailed as the PlanSA website is currently undergoing maintenance and is therefore unavailable.

Thanks

Anna

0402 478 926

Lou Fantasia PLANNING

9 November 2022

Sebastien Paraskevopoulos
Adelaide Hills Council
PO Box 44
WOODSIDE SA 5244

Email: sparaskevopoulos@ahc.sa.gov.au

Dear Sebastian

Response to Representation - Development Application 21027228 – 31 Crescent Drive, Norton Summit

Thank you for forwarding a copy of the representations received in relation to this development application. We have now reviewed these representations and provide the response below and the amended site plan

Representations

Four representations have been received opposed to the proposed development by:

- Fabian Booth – PO Box 309 Magill
- Paul Atkinson – PO Box 80 Norton Summit
- Michael Richardson of Masterplan on behalf of the Giles Family of 412 Old Norton Summit Road Norton Summit
- Anna McDonald – PO Box 99 Norton Summit

The representors raise a number of issues and for simplicity these items have been grouped together. The issues raised are:

- Nature of development
- Appearance, bulk, scale and materials of two storey building and impact on the adjacent State Heritage Place
- Traffic and level of parking on The Crescent Drive
- Wastewater management
- CFS compliance
- Retaining Walls

Response

Nature of development

The application by letter dated 23 January 2022 was amended to now comprise the change of use of the existing detached dwelling to dwelling and tourist accommodation in the nature of a B &B, and converting the approved outbuilding/studio to an outbuilding/studio and tourist accommodation.



Planning
Institute
Australia

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ABN 71 105 719 211

The proposal comprises two parts as follows:

1. the utilisation of several rooms within the existing detached dwelling for B&B accommodation; and
2. the conversion of and alterations to the approved outbuilding/studio provide an upper level within the building to be used for B&B accommodation.

The outbuilding/studio gained Development Approval in October 2017 and the period of consent was subsequently extended by Council in 2019 with the slab of the building laid in 2019 ie substantial commencement.

The proposal therefore involves the adaptive reuse or repurposing of existing and approved buildings to provide tourist accommodation in the nature of B&B totally within the footprint of each building.

The applicant decided to place the construction of the outbuilding/studio on hold pending the outcome of this development application as he preferred to construct the building with the upper level as an integrated structure instead of applying for a mezzanine structure as later separate application.

The B&B in the approved outbuilding will located at an upper level area and comprise 2 bedrooms a living area with amenities. There will be some changes to the approved building to accommodate the B&B area.

In our opinion Performance Outcome 8.1 and DTS/PDF 8.1 are most relevant as they envisaged the provision of tourist accommodation where it involves the ‘adaptive reuse’ and ‘within’ an existing building.

<i>PO 8.1</i> <i>Adaptive reuse of existing buildings for small-scale shops, offices, tourist accommodation or ancillary rural activities.</i>	<i>DTS/DPF 8.1</i> <i>Development within an existing building is for any of the following:</i> <ol style="list-style-type: none"><i>1. a shop</i><i>2. office</i><i>3. tourist accommodation.</i>
--	---

While construction of the outbuilding ie walls and roof has not commenced, the applicant/owner will complete the building regardless of the outcome of this application. The applicant prefers to modify the approved building and adapt the internal arrangements to accommodate a B&B before proceeding with the construction, and not have to make alterations to the completed the building at a later time, subject to Council approval.

We note and acknowledged that the subject land is not associated with land used for primary production however the subject land and the other properties between the Crescent Drive and Lobethal Road are nestled between land that is used for primary production orchards and vineyards. The subject land is uniquely located with safe and convenient access from a local street that also serves to provide as access to Council’s

community facility, a shop and medical centre at its northern end and a primary school at its southern end. Further Crescent Drive is located less than 100 metres from the intersection of Old Norton Summit Road, New Norton Summit Road and Lobethal Road which is recognised as one of the key gateways to some of South Australia's best food, wine arts and crafts and scenery in the Adelaide Hills Region.

Concern has been also raised that the outbuilding and B&B will be used as a separate dwelling. The applicant will be using the ground floor area including the kitchenette for his personal and private use as a studio, and for the preparation of meals for the guests using the B&B above, in view of the distance of the building from the kitchen in the main house.

The proposed merely seeks to change of several rooms within the existing dwelling and create an upper level within the approved outbuilding/studio building for tourist accommodation in the nature of B&Bs.

The adaptive reuse of the existing dwelling and approved outbuilding (as amended) will cater to the needs of visitors to access and enjoy the region's landscapes, produce and functions connected to the vineyards, orchards, and market gardening.

The proposed adaptive use of an existing building (detached dwelling) and the approved building, in my opinion sufficiently satisfies the intent of the relevant Desired and Performances outcomes of the Code.

Appearance, bulk, scale and materials of two storey building and impact on the adjacent State Heritage Place

The changes to the approved outbuilding to accommodate the B&B area essentially involve:

- (i) the construction of a 2nd level within the building with an increase in wall height of 1.2 metres.
- (ii) the increase in the overall building height by 375mm. The approved building has an overall height of 6.025metres with the proposed alterations to 6400mm;
- (iii) the inclusion of upper level windows to the rear (south-western) elevation;
- (iv) change to the portion of the roof section to create an internal feature voids area.

The attached drawings show the approved building overlayed in Red over the proposed build form. The key change to the built form is the two storey component whereas the approved building is single storey. The notable change in appearance is the increase in rear wall height by 1200mm and the introduction of windows at the upper level. However overall height and size of the building is only marginally larger than the approved building is similar in proportion in terms of height and scale and appearance to the approved building.

The issue of the desired materials for a B&B within the Adelaide Hills Region is not a relevant consideration in view of the materials and finishes of the adjoining and nearby buildings and that site is not an historic area.

The Giles Family raises concern the potential impact the two storey building will have on the 'State Heritage Orchard'.

The proposed building will have a:

- a height and scale similar to the approved outbuilding; and
- a finished floor level (existing concrete slab) that is setdown 2400mm below street level with a minimum setback of 4.2 metres from the street boundary.

The building will also be screened from the views from within the Orchard by the stand of mature pine trees and juvenile eucalypt trees along the road verge on the western side of the Crescent Drive; and the slope of land on the western side of Crescent Drive to the south-west.

The images below show the stand of trees along the western side of Crescent Drive.



Pic 1: View of Crescent Drive looking towards subject land with stand of trees on the western side of the street.



Pic 2: View looking through stand of trees on the western side of the street

The amended building, taking into account contours and ground levels on both sides of the Crescent Drive, the width of the road reserve, the stand of trees within the western verge of the Crescent Drive and the stand of screening trees along the eastern edge of the Orchard will, in my opinion, not ‘*dominate, encroach on or unduly impact on the setting*’ of State Heritage place ie Orchard.

Traffic and level of parking on The Crescent Drive

The existing access point from Crescent Drive will remain and provides entry to the all-weather access driveway to the parking area for the residents and guests.

The proposed development will utilise the existing large paved area presently used for the storage of miscellaneous materials and equipment and carparking for parking by the B&B guests. This area can accommodate five (5) vehicles with ample reversing and maneuvering space to enable vehicles enter and leave the site in a forward direction.

The existing access point can accommodate these movements, and presently caters for private vehicles associated with the dwelling at the site. The entrance access point is located with good sightlines and the driveway is concrete paved to provide all weather access.

Table 1 - General Off-Street Car Parking Requirements seeks the provision of parking at the rate of *1 car parking space per accommodation unit / guest room*.

A total of five parking spaces are provided for the B&Bs which is considered more than adequate to cater for the Lilley parking demands of guests. Separate parking is provided for the existing dwelling.

The additional land use and low scale nature of the development is unlikely to generate high volumes of traffic that will have an adverse traffic and carparking impacts on the Crescent Drive to the nearby adjoining neighbours or any other owners or occupiers within the zone, or the environment.

Other Matters

Wastewater management

RFE Consulting was engaged to undertake a site and soil assessment and prepare an effluent and wastewater management plan (EWMP). The EWMP was prepared to accompany the original application which sought to make alterations and additions, and the change of use of the approved studio/workshop to a cellar door/studio and B&B with associated outdoor areas, on-site waste control, car parking and landscaping.

The application was subsequently amended to only involve a change of use of the dwelling to dwelling and tourist accommodation in the nature of a B&B utilising several rooms within the existing house and converting the approved outbuilding/studio to an outbuilding/studio and tourist accommodation creating a B&B in the proposed upper level. Council on 2 March 2022 approved the effluent and wastewater system designed by RFE Consulting.

The EWMP was prepared on the basis of a more intensive land use activity and designed for a total of 22 persons consisting of 6 equivalent persons in the residential dwelling, 4 persons allowance for tourist accommodation and 12 persons using the cellar door and allowing for wine tasting.

The amended proposal for tourist accommodation does not include a cellar door and therefore will accommodate significantly less people on site at any one time. Accordingly, the approved wastewater system designed by RFE Consulting will easily accommodate the smaller number of people associated with the dwelling and the two B&B areas.

Therefore there are unlikely to be any environmental risks or issues associated with this tourist accommodation proposal.

CFS compliance

Several of the representors has raised concern with Country Fire Service (CFS) access to the site. The matter was referred to the CFS who assessed the proposal and sought additional information and changes to the site plan. The CFS by notice dated 23 June 2022 advised that it supports the proposal on the basis of amended Site Plan dated 2 April 2022 for the proposed tourist accommodation

Retaining walls

Concern has been raised regarding the retaining walls. The work of the retaining walls is outside of the scope of this application. The owner has engaged contractors to rebuild the collapsed retaining walls with work commencing in early December this year.

Conclusion

We appreciate the opportunity to provide a response to the issues outlined in the letters of representation.

Having reviewed the relevant provisions of the Planning and Design Code we are of the view that the proposed development, as amended, is not seriously at variance and displays sufficient merit to warrant the granting of Planning Consent.

Would you please advise us of the date and time of the meeting when this matter will be considered, so that we or a representative can be in attendance to respond to any representations made to the Council Assessment Panel in person.

Please feel free to call me on 0413 743 405 or by email at lou@loufantasiaplanning.com.au should you have any questions or require any further information

Yours faithfully

A stylized, cursive signature of 'Lou Fantasia' in black ink.

Lou Fantasia RPIA KCHS

FINAL WORKING DRAWINGS

CFS ACCESS REQUIREMENTS - NEW ROADS

ACCESS (TO DEDICATED WATER SUPPLY) (BCA 2.3.4.1)
-A DEDICATED AND ACCESSIBLE WATER SUPPLY TO BE MADE AVAILABLE AT ALL TIMES FOR FIRE FIGHTING
-EXISTING WATER SUPPLY MAY BE UTILISED FOR THE PROVISION OF FIRE FIGHTING WATER SUPPLY.
-PROVISION SHALL BE MADE ADJACENT TO THE DEDICATED FIRE FIGHTING WATER SUPPLY OUTLET FOR A HARDSTAND AREA CAPABLE OF SUPPORTING FIRE FIGHTING VEHICLES WITH A GROSS VEHICLE MASS (GVM) OF 21 TONNES THAT IS A DISTANCE EQUAL TO OR LESS THAN 6M FROM THE WATER SUPPLY OUTLET.
NOTE: THE DEDICATED FIRE FIGHTING WATER SUPPLY OUTLET MAY BE LOCATED REMOTE FROM THE WATER STORAGE FACILITY TO ENABLE APPROPRIATE ACCESSIBILITY FOR FIRE FIGHTING SERVICES.

CFS VEGETATION LANDSCAPING

BUSHFIRE PROTECTION FEATURES TO LANDSCAPING (BCA PART 2.3.5)
-GARDEN SHALL BE MAINTAINED TO BE FREE OF ACCUMULATED DEAD VEGETATION DURING THE FIRE DANGER SEASON.
-ALL BRANCHES OVERHANGING BUILDINGS SHALL BE REMOVED OR TRIMMED CLEAR OF THE LINE OF GUTTERS
-GRASSES WITHIN 20M OF THE DWELLING, OR TO THE PROPERTY BOUNDARIES, WHICHEVER COMES FIRST SHALL BE REDUCED TO A HEIGHT OF 10CM DURING THE FIRE DANGER SEASON.

CFS WATER SUPPLY

WATER SUPPLY (FOR FIRE FIGHTING)
MINISTERS SPECIFICATION SA78 PRESCRIBES THE DEDICATED WATER SUPPLY FOR BUSHFIRE FIGHTING IN THE BUSHFIRE ZONE. THE DEDICATED BUSHFIRE FIGHTING WATER SUPPLY SHALL ALSO INCORPORATE THE INSTALLATION OF A PUMPING SYSTEM, PIPEWORK AND FIRE FIGHTING HOSE(S) IN ACCORDANCE WITH MINISTERS SPECIFICATION SA78.
-A MINIMUM SUPPLY OF 22000 LITRES OF WATER SHALL BE AVAILABLE AT ALL TIMES FOR BUSHFIRE FIGHTING PURPOSES.
-THE DEDICATED FIRE FIGHTING WATER SUPPLY SHALL BE CLEARLY IDENTIFIED AND SHALL BE ACCESSIBLE TO FIRE FIGHTING VEHICLES AT ALL TIMES.
-THE WATER STORAGE FACILITY (AND ANY SUPPORT STRUCTURE) SHALL BE CONSTRUCTED FROM NON-COMBUSTIBLE MATERIAL.
-THE DEDICATED FIRE FIGHTING WATER SUPPLY SHALL BE PRESSURISED BY A PUMP THAT HAS:
-A MINIMUM INLET DIAMETER OF 38MM, AND
-IS POWERED BY A PETROL OR DIESEL ENGINE WITH A POWER RATING OF AT LEAST 3.7KW (5HP), OR
-A PUMPING SYSTEM THAT OPERATES INDEPENDENTLY OF MAINS ELECTRICITY AND IS CAPABLE OF PRESSURISING THE WATER FOR FIRE FIGHTING PURPOSES.
-THE DEDICATED FIRE FIGHTING WATER SUPPLY PUMP SHALL BE LOCATED AT OR ADJACENT TO THE DWELLING TO ENSURE OCCUPANTS SAFETY WHE OPERATING THE PUMP DURING A BUSHFIRE. AN "OPERATIONS INSTRUCTION PROCEDURE" SHALL BE LOCATED WITH THE PUMP CONTROL PANEL.
-THE FIRE FIGHTING PUMP AND ANY FLEXIBLE CONNECTIONS TO THE WATER SUPPLY SHALL BE PROTECTED BY A NON-COMBUSTIBLE COVER THAT ALLOWS ADEQUATE AIR VENTILATION FOR EFFICIENT PUMP OPERATION.
-ALL BUSHFIRE FIGHTING WATER PIPES AND CONNECTIONS BETWEEN THE WATER STORAGE FACILITY AND A PUMP SHALL BE NO SMALLER THAN THE DIAMETER OF THE PUMP INLET.
-ALL NON METAL WATER SUPPLY PIPES FOR BUSHFIRE FIGHTING PURPOSES (OTHER THAN FLEXIBLE CONNECTIONS AND HOSES FOR FIRE FIGHTING) SHALL BE BURIED BELOW GROUND TO A MINIMUM DEPTH OF 300MM WITH NO NON-METAL PARTS ABOVE GROUND LEVEL.
-A FIRE FIGHTING HOSE (OR HOSES) SHALL BE LOCATED SO THAT ALL PARTS OF THE BUILDING ARE WITHIN REACH OF THE NOZZLE END OF THE HOSE AND IF MORE THAN ONE HOSE IS REQUIRED THEY SHOULD BE POSITIONED TO PROVIDE MAXIMUM COVERAGE OF THE BUILDING AND SURROUNDS (I.E. AT OPPOSITE ENDS OF THE DWELLING).
-ALL FIRE FIGHTING HOSES SHALL BE CAPABLE OF WITHSTANDING THE PRESSURES OF SUPPLIED WATER.
-ALL FIRE FIGHTING HOSES SHALL BE OF REINFORCED CONSTRUCTION MANUFACTURED IN ACCORDANCE WITH AS 2620 OR AS 1221.
-ALL FIRE FIGHTING HOSES SHALL HAVE A MINIMUM NOMINAL INTERNAL DIAMETER OF 18MM AND A MAXIMUM LENGTH OF 36 METERS.
-ALL FIRE FIGHTING HOSES SHALL HAVE AN ADJUSTABLE METAL NOZZLE, OR AN ADJUSTABLE PVC NOZZLE MANUFACTURED IN ACCORDANCE WITH AS 1221.
-ALL FIRE FIGHTING HOSES SHALL BE READILY AVAILABLE AT ALL TIMES.

CLIENT:
MICHAEL VELONAKIS
PROPOSED:
STUDIO BUILDING
SITE ADDRESS:
6 CRESCENT DRIVE, NORTON SUMMIT
DATE: NOVEMBER 2022
DRAWN: PR
SHEET: 1 OF 4

DIAMANT
DESIGN & PROJECT MANAGEMENT
18 ROSE STREET
GLENELG SA 5045

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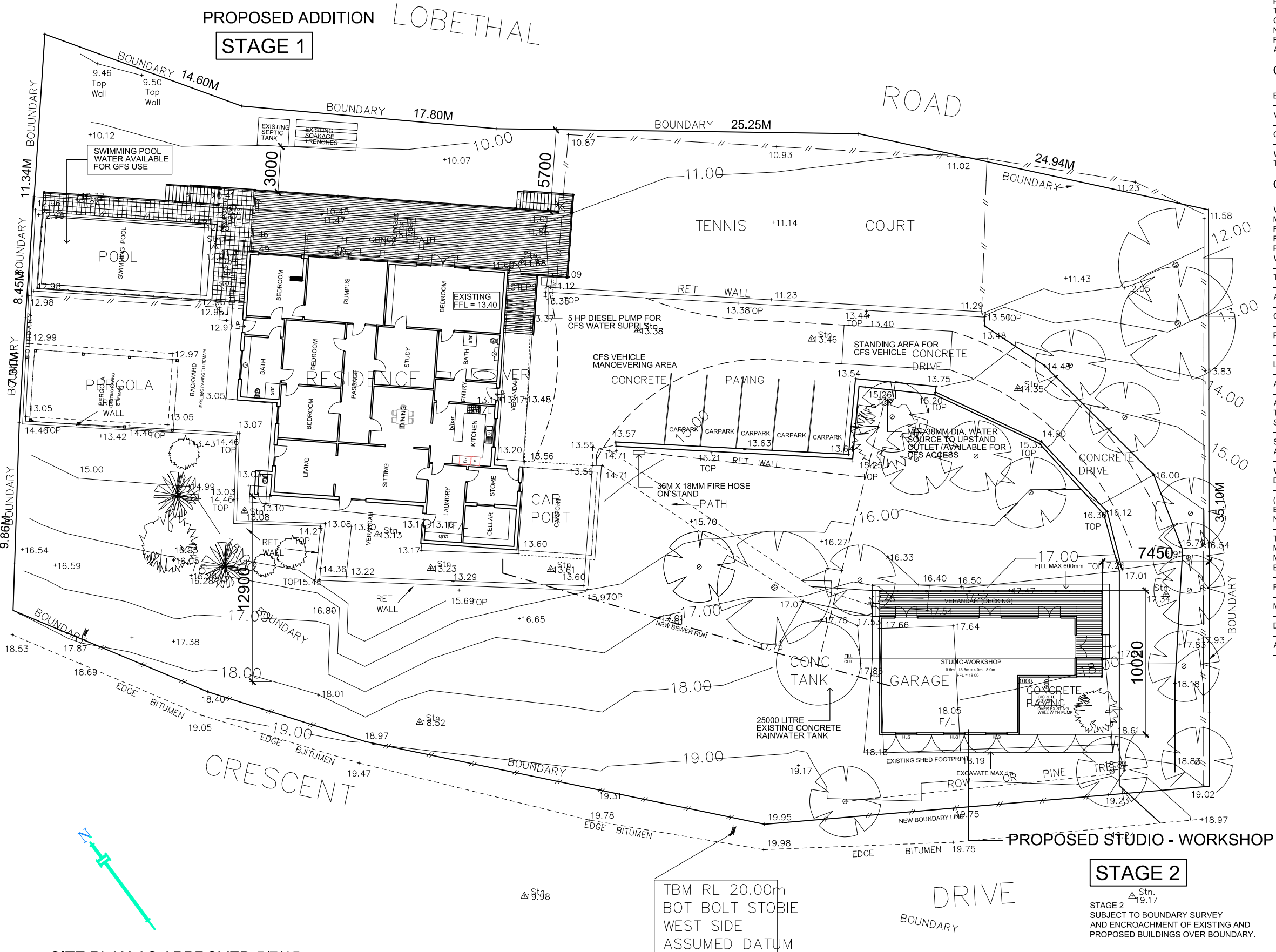
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"D" AMENDMENT 06/05/2016

- ADD CFS REQUIREMENTS NOTES
- ADD VARIOUS BUILDING NOTES

"A" AMENDMENT 15/09/2015

- ENVIROCYCLE DELETED, NEW WC BASIN
CONNECTED TO EXISTING SEWER RUN



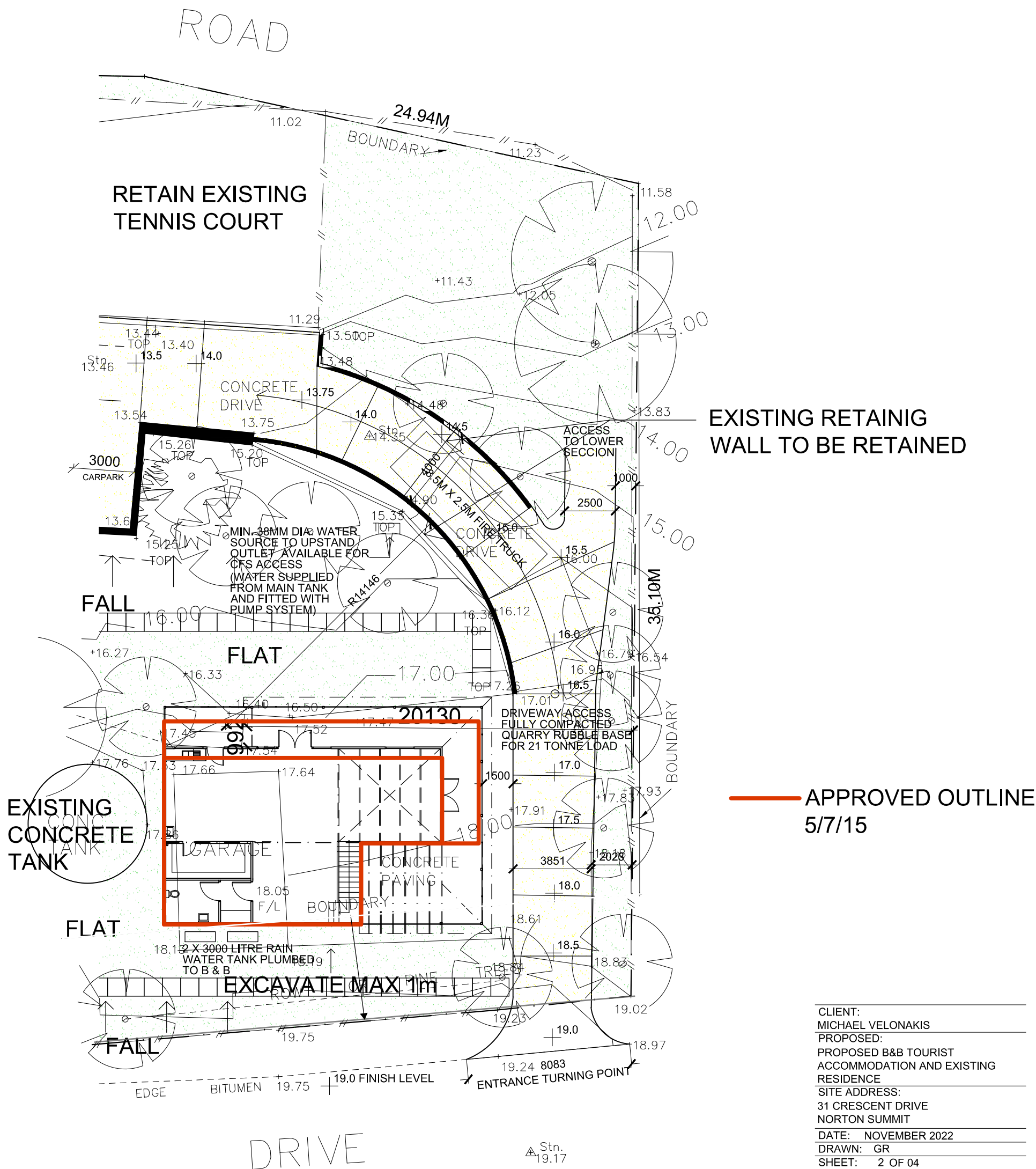
SITE PLAN AS APPROVED 5/7/15

SCALE 1:200 @ A2

TBM RL 20.00m
BOT BOLT STOBIE
WEST SIDE
ASSUMED DATUM

STAGE 2

STAGE 2
SUBJECT TO BOUNDARY SURVEY
AND ENCROACHMENT OF EXISTING AND
PROPOSED BUILDINGS OVER BOUNDARY.



BUILDING DESIGN & PROJECT MANAGEMENT
PO Box 551, Marlestone SA, 5033

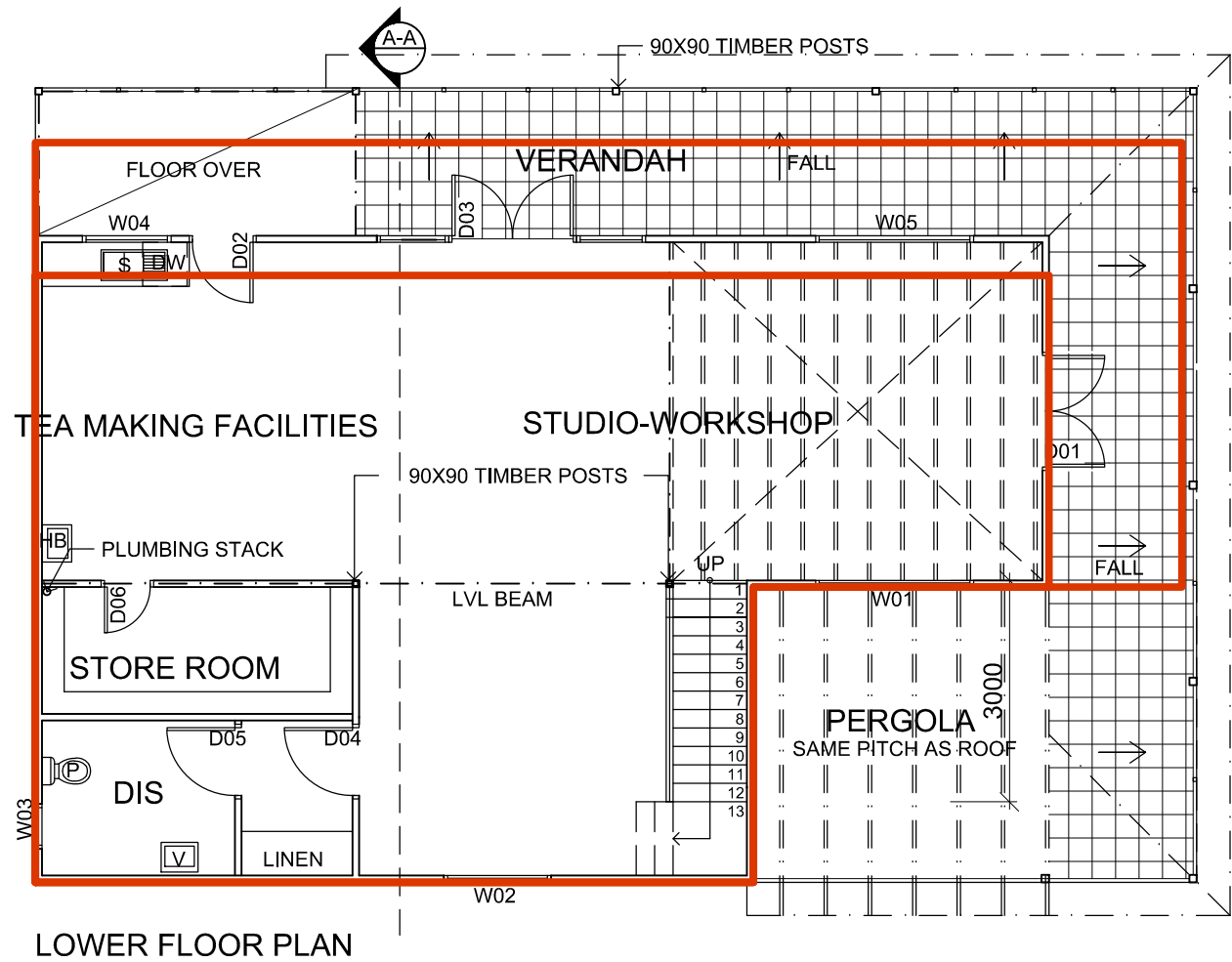
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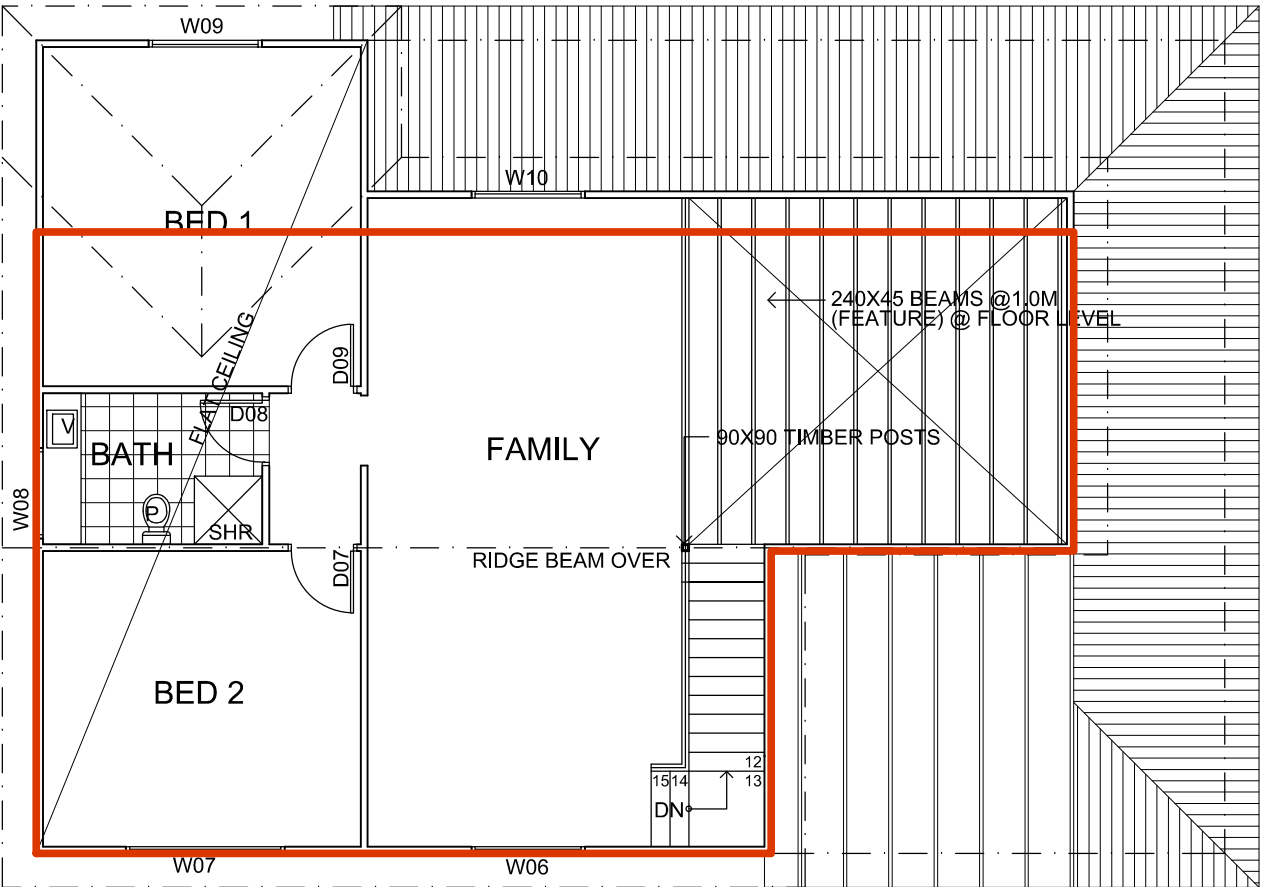
CURRENT SITE PLAN AS PER CURRENT SUBMISSION

SCALE 1:100 @ A3



LOWER FLOOR PLAN

APPROVED OUTLINE
5/7/15



MEZZANINE FLOOR PLAN

CLIENT: MICHAEL VELONAKIS
PROPOSED: PROPOSED B&B TOURIST ACCOMMODATION AND EXISTING RESIDENCE
SITE ADDRESS: 31 CRESCENT DRIVE NORTON SUMMIT
DATE: NOVEMBER 2022
DRAWN: GR
SHEET: 3 OF 04
JOB No.



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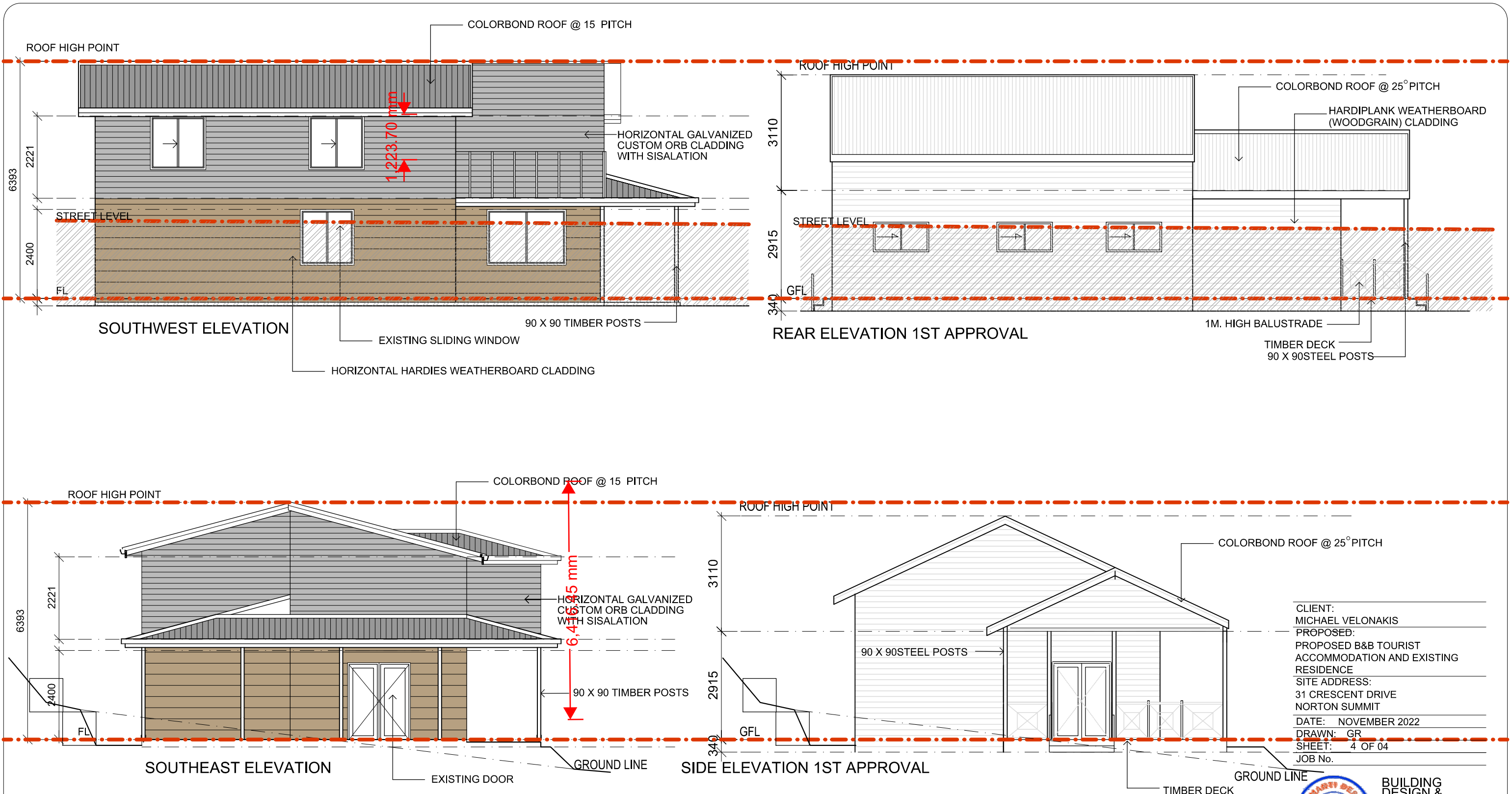
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BUILDING AND COLOUR SCHEDULE		
ITEM	CONSTRUCTION	COLOUR
FLOOR	CONCRETE RAFT SLAB & FOOTING TO ENGINEERS DESIGN	---
WALLS	TIMBER FRAME, WITH GALVANIZED HORIZONTAL CUSTOM ORB	NATURAL COLLOR
	HARDIES WEATHERBOARD CLADDING	DOESKIN
ROOF	COLOURBOND	IRONSTONE
WINDOWS	ALUMINIUM	CREAM

FLOOR PLANS AS PER CURRENT SUBMISSION

SCALE 1:100 @ A3



CLIENT: MICHAEL VELONAKIS
PROPOSED: PROPOSED B&B TOURIST ACCOMMODATION AND EXISTING RESIDENCE
SITE ADDRESS: 31 CRESCENT DRIVE NORTON SUMMIT
DATE: NOVEMBER 2022
DRAWN: GR
SHEET: 4 OF 04
JOB No.

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FIGURED DIMENSIONS SHALL TAKE PREFERENCE OVER SCALED DRAWINGS. VERIFY DIMENSIONS AND LEVELS BEFORE COMMENCING ANY BUILDING WORK. ANY DISCREPANCY TO BE REPORTED TO THE DESIGNERS IMMEDIATELY.

CURRENT AND APPROVED ELEVATIONS

SCALE 1:100 @ A3



DEVELOPMENT ASSESSMENT SERVICE

Date:	23/06/2022
Your reference:	21027228
Our reference:	Adelaide Hills DA 20220623-03ap

BUSHFIRE HAZARD PROTECTION RESPONSE

Application	Development Application Schedule 9 Referral Body Response
Development	Tourist accommodation
Location	31 Crescent Drive, NORTON SUMMIT
Applicant	Michael Velonakis
Owner	Mani White

LEGISLATIVE FRAMEWORK

Instrument	The 'Planning and Design Code' under the <i>Planning, Development and Infrastructure Act 2016</i>
Overlay	The Hazards (Bushfire – High Risk)

DECISION

The SA Country Fire Service has no objection to the proposed development with conditions.

'The Planning and Design Code' details various requirements as part of the assessment of each development application, and where applicable, these are reinforced through conditions of consent, which are hereby directed to apply to any consent issued in respect of this development application, as detailed below:

CONDITIONS OF CONSENT

SITING

'The Planning and Design Code' Hazards (Bushfire – High Risk) Overlay (Performance Outcomes 2.1, 4.1, & 4.2) details the mandatory requirements for buildings and structures to be located away from areas that pose an unacceptable bushfire risk, in order to provide sufficient defensible space for occupants and fire fighters; ensure radiant heat levels at the buildings are minimised in line with the assessed bushfire attack level & construction level; whilst maintaining reduced fuel loads and ensuring it can be maintained in perpetuity by the occupants.

- Outbuildings and other ancillary structures shall be sited no closer than 6m from the habitable building, unless built to relevant building standards for associated structures in Bushfire Prone Areas.

ACCESS TO HABITABLE BUILDING

'The Planning and Design Code' Hazards (Bushfire – High Risk) Overlay (Performance Outcome 6.2) details the mandatory requirements for 'Private' roads and driveways to facilitate safe and effective use, operation and evacuation for firefighting and emergency personnel and evacuation of residents, occupants and visitors where required. These requirements apply when the furthest point of the building is more than 60m from the nearest public road.

SA CFS has no objection to utilising the driveway and the public road for a 'T' shaped turnaround with the following conditions:

- The driveway shall be connected to a formed, all-weather public road with the transition area between the road and driveway having a gradient of not more than 7 degrees (1-in-8).
- Access to the building site shall be of all-weather construction, with a minimum formed road surface width of 3 metres.
- Private access shall have minimum internal radii of 9.5 metres on all bends.
- Private access shall provide overhead clearances of not less than 4.0m horizontally and vertically between the driveway surface and overhanging branches or other obstructions, including buildings and/or structures.
- The gradient of the access road shall not exceed 16 degrees (1-in-3.5) at any point along the driveway. In steep terrain exceeding 10 degrees (1-in-5.5) the surface should be sealed.
- The all weather road is to be constructed such that it is protected from water erosion of the traffic surface. The road surface shall be profiled to manage storm water run off to appropriate drains, at one or both sides of the traffic surface.
The accumulated volumes of water shall be directed via:
 1. open drains; or
 2. culverts and pipes under the traffic surface, and / or away from same, without causing further soil erosion, silting of adjacent areas or water courses or instability of any embankment or cutting.

WATER SUPPLY & ACCESS (to dedicated water supply)

Ministerial Building Standard MBS008 "Designated bushfire prone areas – additional requirements" July 2020, as published under the *Planning, Development and Infrastructure Act 2016*, provides the technical details of the dedicated water supply for bushfire fighting for the bushfire zone. The dedicated bushfire fighting water supply shall also incorporate the installation of a pumping system, pipe-work and fire-fighting hose(s) in accordance with MBS008.

'The Planning and Design Code' Hazards (Bushfire – High Risk) Overlay (Performance Outcome 4.3) details the mandatory requirements for the site to provide a dedicated hardstand area in a location that allows fire fighting vehicles to safely access the dedicated water supply.

SA CFS has no objection to the location of the existing dedicated water supply as detailed on drawing named PROPOSED SITE PLAN dated at last revision 02/04/2022, providing the outlet is positioned remotely to comply with the following conditions:

- The water supply outlet shall be easily accessible and clearly identifiable from the access way.
- The dedicated water supply and its location should be identified with suitable signage (i.e. blue sign with white lettering "FIRE WATER").

- Access to the dedicated water supply shall be of all-weather construction, with a minimum formed road surface width of 3 metres.
- Provision shall be made adjacent the water supply for a nominally level hardstand area (capable of supporting fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes) that is a distance equal to or less than 6 metres from the water supply outlet.
- SA CFS appliance's inlet is rear mounted; therefore the outlet/water storage shall be positioned so that the SA CFS appliance can easily connect to it rear facing.
- A gravity fed water supply outlet may be remotely located from the above ground tank to provide adequate access.
- All non-metal water supply pipes for bushfire fighting purposes (other than flexible connections and hoses for fire-fighting) shall be buried below ground to a minimum depth of 300mm with no non-metal parts above ground level.
- All water supply pipes for draughting purposes shall be capable of withstanding the required pressure for draughting.
- Ideally a remote water supply outlet should be gravity fed, where this is not possible the following dimensions shall be considered as the maximum capability in any hydraulic design for draughting purposes:
 1. The dedicated water supply outlet for draughting purposes shall not exceed 5 metres maximum vertical lift (calculated on the height of the hardstand surface to the lowest point of the storage) and no greater than 6 metres horizontal distance.
 2. The suction outlet pipework from the water supply shall be fitted with an inline non return valve of nominal internal diameter not less than that of the suction pipe and be located from the lowest point of extract from the tank. All fittings shall be installed to allow for easy maintenance.
- The minimum water supply required may be combined with domestic use, providing the outlet for domestic use is located above the dedicated fire water supply (in order for it to remain as a dedicated supply).

MAINTAIN AN ASSET PROTECTION ZONE (APZ) - VEGETATION MANAGEMENT

'The Planning and Design Code' Hazards (Bushfire – High Risk) Overlay (Performance Outcome 4.2) details the mandatory requirements to establish and maintain an asset protection zone. As such, landscaping shall include bushfire protection features that will prevent or inhibit the spread of bushfires and minimise the risk to life and/or damage to buildings and property and maintain a fuel reduced zone for safe movement of occupants and fire fighters.

The extent of an existing or proposed asset protection zone has not been detailed on supplied drawings.

- Vegetation management shall be established and maintained within 20 metres of the habitable building (or to the property boundaries – whichever comes first) as follows:
 1. The number of trees and understorey plants existing and to be established within the VMZ shall be reduced and maintained such that when considered overall a maximum coverage of 30% is attained, and so that the leaf area of shrubs is not continuous. Careful selection of the vegetation will permit the 'clumping' of shrubs where desirable, for diversity, and privacy and yet achieve the 'overall maximum coverage of 30%'. **SA CFS notes that coverage of vegetation within 20m, to the north west of the proposed habitable (studio) building as well as along the "front" boundary (adjacent Lobethal Road) is currently greater than 30%.**

2. Reduction of vegetation shall be in accordance with SA Native Vegetation Act 1991 and SA Native Vegetation Regulations 2017.
3. Trees and shrubs shall not be planted closer to the building(s) than the distance equivalent to their mature height.
4. Trees and shrubs must not overhang the roofline of the building, touch walls, windows or other elements of the building.
5. Shrubs must not be planted under trees and must be separated by at least 1.5 times their mature height from the trees' lowest branches.
6. Grasses within the zone shall be reduced to a maximum height of 10cm during the Fire Danger Season.
7. No understorey vegetation shall be established within 2 metres of the habitable building (understorey is defined as plants and bushes up to 2 metres in height).
8. Flammable objects such as plants, mulches and fences must not be located adjacent to vulnerable parts of the building such as windows, decks and eaves
9. The VMZ shall be maintained to be free of accumulated dead vegetation.


TOURIST ACCOMMODATION - BUSHFIRE SURVIVAL PLAN

CFS further recommends:

- The applicants to prepare and display a BUSHFIRE SURVIVAL PLAN (BSP) designed specifically for the purpose of any guests that may be in residence during a bushfire event, especially during the Fire Danger Season. The BSP:
 1. should provide clear directions to persons that may be unfamiliar with the area/locality and unfamiliar with what protective actions they may need to take to protect their lives during a bushfire event, including when to take such protective actions; and
 2. should address the possibility that the owners may not be present at the time of the bushfire event; and
 3. should not expect guests to be involved in fire-fighting operations.

The SA CFS 'Bushfire Safety Guide for Business' document (refer to CFS website) should be utilised as a basis for information and the drafting of the (GUEST) BSP.

The applicant should consider reducing operating hours and restrictions on days of heightened bushfire danger and/or bushfire events and consider including any alterations to bookings and services offered due to actual or predicted conditions during the Fire Danger Season in any booking/refund policy.

Assessing Officer: ANNIE POMEROY <i>BUSHFIRE SAFETY OFFICER</i> DEVELOPMENT ASSESSMENT SERVICE	Signature: 	Date: 23/06/2022
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DEVELOPMENT ASSESSMENT SERVICE

Date:	23/06/2022
Your reference:	21027228
Our reference:	Adelaide Hills DA 20220623-03ap

BUILDING ADVISORY & BUSHFIRE ATTACK LEVEL (BAL) ASSESSMENT

Application	Development Application Schedule 9 Referral Body Response
Development	Tourist accommodation
Location	31 Crescent Drive, NORTON SUMMIT
Applicant	Michael Velonakis
Owner	Mani White

LEGISLATIVE FRAMEWORK

Instrument	Ministerial Building Standard MBS 008, Designated Bushfire prone areas – additional requirements July 2020 as published under the <i>Planning, Development and Infrastructure Act 2016</i>
Overlay	The Hazards (Bushfire – High Risk)

DECISION


Asset:	Category of Bushfire Attack Level (BAL)
Existing dwelling – part conversion to Tourist Accommodation	BAL 19 This BAL rating is conditional upon the establishment and maintenance of a 20 metre Asset Protection Zone, (or to the property boundaries, whichever comes first) in accordance with the Asset Protection Zone – Vegetation Management condition of consent placed on the planning consent with the same application reference.
Self-contained studio conversion to Tourist Accommodation	BAL 29 This BAL rating is conditional upon the establishment and maintenance of a 20 metre Asset Protection Zone, (or to the property boundaries, whichever comes first) in accordance with the Asset Protection Zone – Vegetation Management condition of consent placed on the planning consent with the same application reference.

This report is considered relevant at the date of assessment with respect to the elevations and proposed site plan dated at last revision 02/04/2022 and shall not be considered as SA CFS endorsement of any subsequent development.

This report is prepared in accordance with National Construction Code of Australia (NCC) and Australian Standard™ 3959:2018 (AS3959) “Construction of Buildings in Bushfire Prone Areas”.

Please refer to the NCC, relevant standards and state provisions for construction requirements and performance provisions.

Compliance with the fire protection requirements is not a guarantee the dwelling will not burn, but its intent is to provide a *'measure of protection'* from the approach, impact and passing of a bushfire.

Assessing Officer: ANNIE POMEROY <i>BUSHFIRE SAFETY OFFICER</i> DEVELOPMENT ASSESSMENT SERVICE	Signature: 	Date: 23/06/2022
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31 CRESCENT DR NORTON SUMMIT SA 5136

Address:

Click to view a detailed interactive [SAILIS](#) in [SAILIS](#)

To view a detailed interactive property map in SAPPA click on the map below



Property Zoning Details

Overlay

- Environment and Food Production Area
- Hazards (Bushfire - High Risk)
- Heritage Adjacency
- Hazards (Flooding - Evidence Required)
- Limited Land Division
- Native Vegetation
- Prescribed Wells Area
- Traffic Generating Development
- Urban Transport Routes

Zone

- Productive Rural Landscape

Selected Development(s)

Tourist accommodation

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards.

If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Tourist accommodation - Code Assessed - Performance Assessed

Part 2 - Zones and Sub Zones

Productive Rural Landscape Zone

Assessment Provisions (AP)

Desired Outcome	
DO 1	A diverse range of land uses at an appropriate scale and intensity that capitalise on the region's proximity to the metropolitan area and the tourist and lifestyle opportunities this presents while also conserving the natural and rural character, identity, biodiversity and sensitive environmental areas and scenic qualities of the landscape.
DO 2	A zone that promotes agriculture, horticulture, value adding opportunities, farm gate businesses, the sale and consumption of agricultural based products, tourist development and accommodation that expands the economic base and promotes its regional identity.
DO 3	Create local conditions that support new and continuing investment while seeking to promote co-existence with adjoining activities and mitigate land use conflicts.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 The productive value of rural land for a range of primary production and horticultural activities and associated value adding of primary produce (such as beverage production), retailing and tourism is supported, protected and maintained. The proliferation of land uses that may be sensitive to those activities is avoided.	DTS/DPF 1.1 Development comprises one or more of the following: <ul style="list-style-type: none"> (a) Advertisement (b) Agricultural building (c) Brewery (d) Carport (e) Cidery (f) Distillery (g) Dwelling (h) Dwelling addition (i) Farming (j) Function centre (k) Horse keeping (l) Horticulture (m) Industry (n) Low intensity animal husbandry (o) Outbuilding (p) Shop (q) Small-scale ground mounted solar power facility (r) Tourist accommodation (s) Transport distribution (t) Verandah (u) Warehouse (v) Winery (w) Workers' accommodation
Siting and Design	
PO 2.1	DTS/DPF 2.1

Development is provided with suitable vehicle access.	Development is serviced by an all-weather trafficable public road.
<p>PO 2.2</p> <p>Buildings are generally located on flat land to minimise cut and fill and the associated visual impacts.</p>	<p>DTS/DPF 2.2</p> <p>Buildings:</p> <ul style="list-style-type: none"> (a) are located on a site with a slope not greater than 10% (1-in-10) (b) do not result excavation and/or filling of land that is greater than 1.5m from natural ground level.
Shops, Tourism and Function Centres	
<p>PO 6.3</p> <p>Tourist accommodation is associated with the primary use of the land for primary production or primary production related value adding industry to enhance and provide authentic visitor experiences.</p>	<p>DTS/DPF 6.3</p> <p>Tourist accommodation, other than where located in The Cedars Subzone:</p> <ul style="list-style-type: none"> (a) is ancillary to and located on the same allotment or an adjoining allotment used for primary production or primary production related value adding industry (b) in relation to the area used for accommodation: <ul style="list-style-type: none"> (i) where in a new building, does not exceed a total floor area of 100m² (ii) where in an existing building, does not exceed 150m² (c) does not result in more than one facility being located on the same allotment.
<p>PO 6.4</p> <p>Tourist accommodation proposed in a new building or buildings are sited, designed and of a scale that maintains a pleasant rural character and amenity.</p>	<p>DTS/DPF 6.4</p> <p>Tourist accommodation in new buildings:</p> <ul style="list-style-type: none"> (a) is setback from all property boundaries by at least 40m (b) has a building height that does not exceed 7m above natural ground level.
Adaptive Reuse of Existing Buildings	
<p>PO 8.1</p> <p>Adaptive reuse of existing buildings for small-scale shops, offices, tourist accommodation or ancillary rural activities.</p>	<p>DTS/DPF 8.1</p> <p>Development within an existing building is for any of the following:</p> <ul style="list-style-type: none"> (a) a shop (b) office (c) tourist accommodation.
Built Form and Character	
<p>PO 11.1</p> <p>Large buildings designed and sited to reduce impacts on scenic and rural vistas by:</p> <ul style="list-style-type: none"> (a) having substantial setbacks from boundaries and adjacent public roads (b) using low reflective materials and finishes that blend with the surrounding landscape (c) being located below ridgelines. 	<p>DTS/DPF 11.1</p> <p>None are applicable.</p>

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

A class of development listed in Column A is excluded from notification provided that it does not fall within a corresponding exclusion prescribed in Column B. In instances where development falls within multiple classes within Column A, each clause is to be read independently such that if a development is excluded from notification by any clause, it is, for the purposes of notification excluded irrespective of any other clause.

Class of Development (Column A)	Exceptions (Column B)
1. A kind of development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. Any development involving any of the following (or of any combination of any of the following): (a) advertisement (b) agricultural building (c) air handling unit, air conditioning system or exhaust fan (d) ancillary accommodation (e) building work on railway land (f) carport (g) demolition (h) dwelling (i) dwelling addition (j) farming (k) horse keeping (l) internal building work (m) land division (n) outbuilding (o) private bushfire shelter (p) protective tree netting structure (q) replacement building (r) retaining wall (s) solar photovoltaic panels (roof mounted) (t) shade sail (u) swimming pool or spa pool (v) temporary accommodation in an area affected by bushfire (w) tree damaging activity (x) verandah (y) water tank.	None specified.
3. Any development involving any of the following (or of any combination of any of the following):	Except development that does not satisfy any of the following:

<ul style="list-style-type: none"> (a) industry (b) store (c) warehouse. 	<ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 4.1 2. Productive Rural Landscape Zone DTS/DPF 4.3.
4. Demolition.	<p>Except any of the following:</p> <ul style="list-style-type: none"> 1. the demolition of a State or Local Heritage Place 2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
5. Function centre within The Cedars Subzone.	None specified.
6. Function centre.	Except function centre that does not satisfy Productive Rural Landscape Zone DTS/DPF 6.6.
7. Horticulture.	<p>Except horticulture that does not satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 3.1(d) 2. Productive Rural Landscape Zone DTS/DPF 3.1(e).
8. Shop within The Cedars Subzone.	None specified.
9. Shop.	<p>Except shop that does not satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 6.1 2. Productive Rural Landscape Zone DTS/DPF 6.2.
10. Tourist accommodation within The Cedars Subzone.	None specified.
11. Tourist accommodation.	<p>Except tourist accommodation that does not to satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 6.3 2. Productive Rural Landscape Zone DTS/DPF 6.4.

Placement of Notices - Exemptions for Performance Assessed Development

None specified.

Placement of Notices - Exemptions for Restricted Development

None specified.

Part 3 - Overlays

Hazards (Bushfire - High Risk) Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development, including land division is sited and designed to minimise the threat and impact of bushfires on life and property with regard to the following risks:</p> <ul style="list-style-type: none"> (a) potential for uncontrolled bushfire events taking into account the increased frequency and intensity of bushfires as a result of climate change (b) high levels and exposure to ember attack (c) impact from burning debris (d) radiant heat (e) likelihood and direct exposure to flames from a fire front.
DO 2	Activities that increase the number of people living and working in the area or where evacuation would be difficult is sited away from areas of unacceptable bushfire risk.
DO 3	To facilitate access for emergency service vehicles to aid the protection of lives and assets from bushfire danger.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use	
PO 1.1 Development that significantly increases the potential for fire outbreak as a result of the spontaneous combustion of materials, spark generation or through the magnification and reflection of light is not located in areas of unacceptable bushfire risk.	DTS/DPF 1.1 None are applicable.
PO 1.2 Pre-schools, educational establishments, hospitals, retirement and supported accommodation are sited away from areas of unacceptable bushfire risk and locations that: <ul style="list-style-type: none"> (a) are remote from or require extended periods of travel to reach safer locations (b) don't have a safe path of travel to safer locations. 	DTS/DPF 1.2 None are applicable.
Siting	
PO 2.1 Buildings and structures are located away from areas that pose an unacceptable bushfire risk as a result of vegetation cover and type, and terrain.	DTS/DPF 2.1 None are applicable.
Built Form	
PO 3.1 Buildings and structures are designed and configured to reduce the impact of bushfire through using designs that reduce the potential for trapping burning debris against or underneath the	DTS/DPF 3.1 None are applicable.

building or structure, or between the ground and building floor level in the case of transportable buildings and buildings on stilts.	
<p>PO 3.2</p> <p>Extensions to buildings, outbuildings and other ancillary structures are sited and constructed using materials to minimise the threat of fire spread to residential and tourist accommodation (including boarding houses, hostels, dormitory style accommodation, student accommodation and Workers' accommodation) in the event of bushfire.</p>	<p>DTS/DPF 3.2</p> <p>Outbuildings and other ancillary structures are sited no closer than 6m from the habitable building.</p>
Habitable Buildings	
<p>PO 4.1</p> <p>To minimise the threat, impact and potential exposure to bushfires on life and property, residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) is sited on the flatter portion of allotments away from steep slopes.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) is sited away from vegetated areas that pose an unacceptable bushfire risk.</p>	<p>DTS/DPF 4.2</p> <p>Residential and tourist accommodation and habitable buildings for vulnerable communities are provided with asset protection zone(s) in accordance with (a) and (b):</p> <ul style="list-style-type: none"> (a) the asset protection zone has a minimum width of at least: <ul style="list-style-type: none"> (i) 50 metres to unmanaged grasslands (ii) 100 metres to hazardous bushland vegetation (b) the asset protection zone is contained wholly within the allotment of the development.
<p>PO 4.3</p> <p>Residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) has a dedicated area available that:</p> <ul style="list-style-type: none"> (a) is capable of accommodating a bushfire protection system comprising firefighting equipment and water supply in accordance with <i>Ministerial Building Standard MBS 008 - Designated bushfire prone areas - additional requirements</i> (b) includes the provision of an all-weather hardstand area in a location that: <ul style="list-style-type: none"> (i) allows fire-fighting vehicles to safely access the dedicated water supply and exit the site in a forward direction (ii) is no further than 6 metres from the dedicated water supply outlet(s) where required. 	<p>DTS/DPF 4.3</p> <p>None are applicable.</p>
Vehicle Access – Roads, Driveways and Fire Tracks	
<p>PO 6.1</p>	<p>DTS/DPF 6.1</p>

<p>Roads are designed and constructed to facilitate the safe and effective:</p> <ul style="list-style-type: none"> (a) access, operation and evacuation of fire-fighting vehicles and emergency personnel (b) evacuation of residents, occupants and visitors. 	<p>Roads:</p> <ul style="list-style-type: none"> (a) are constructed with a formed, all-weather surface (b) have a gradient of not more than 16 degrees (1-in-3.5) at any point along the road (c) have a cross fall of not more than 6 degrees (1-in-9.5) at any point along the road (d) have a minimum formed road width of 6m (e) provide overhead clearance of not less than 4.0m between the road surface and overhanging branches or other obstructions including buildings and/or structures (Figure 1) (f) allow fire-fighting services (personnel and vehicles) to travel in a continuous forward movement around road curves by constructing the curves with a minimum external radius of 12.5m (Figure 2) (g) incorporating cul-de-sac endings or dead end roads are provided within an alternative evacuation route and do not exceed 200m in length and the end of the road has either: <ul style="list-style-type: none"> (i) a turning area with a minimum formed surface radius of 12.5m (Figure 3) or (ii) a 'T' or 'Y' shaped turning area with a minimum formed surface length of 11m and minimum internal radii of 9.5m (Figure 4) (h) incorporate solid, all-weather crossings over any watercourse that support fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes.
<p>PO 6.2</p> <p>Access to habitable buildings is designed and constructed to facilitate the safe and effective:</p> <ul style="list-style-type: none"> (a) use, operation and evacuation of fire-fighting and emergency personnel (b) evacuation of residents, occupants and visitors. 	<p>DTS/DPF 6.2</p> <p>Access is in accordance with (a) or (b):</p> <ul style="list-style-type: none"> (a) a clear and unobstructed vehicle or pedestrian pathway of not greater than 60 metres in length is available between the most distant part of the habitable building and the nearest part of a formed public access road (b) driveways: <ul style="list-style-type: none"> (i) do not exceed 600m in length (ii) are constructed with a formed, all-weather surface (iii) are connected to a formed, all-weather public road with the transition area between the road and driveway having a gradient of not more than 7 degrees (1-in-8) (iv) have a gradient of not more than 16 degrees (1-in-3.5) at any point along the driveway (v) have a crossfall of not more than 6 degrees (1-in-9.5) at any point along the driveway (vi) have a minimum formed width of 3m (4m where the gradient of the driveway is steeper than 12 degrees (1-in-4.5)) plus 0.5 metres clearance either side of the driveway from overhanging branches or other obstructions, including buildings and/or structures (Figure 1) (vii) incorporate passing bays with a minimum width of 6m and length of 17m every 200m (Figure 5) (viii) provide overhead clearance of not less than 4.0m between the driveway surface and overhanging branches or other obstructions,

	<p>including buildings and/or structures (Figure 1)</p> <p>(ix) allow fire-fighting services (personnel and vehicles) to travel in a continuous forward movement around driveway curves by constructing the curves with a minimum external radius of 12.5m (Figure 2)</p> <p>(x) allow fire-fighting vehicles to safely enter and exit an allotment in a forward direction by using a 'U' shaped drive through design or by incorporating at the end of the driveway either:</p> <p>A. a loop road around the building or</p> <p>B. a turning area with a minimum radius of 12.5m (Figure 3) or</p> <p>C. a 'T' or 'Y' shaped turning area with a minimum formed length of 11m and minimum internal radii of 9.5m (Figure 4)</p> <p>(xi) incorporate solid, all-weather crossings over any watercourse that support fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes.</p>
<p>PO 6.3</p> <p>Development does not rely on fire tracks as means of evacuation or access for fire-fighting purposes unless there are no safe alternatives available.</p>	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

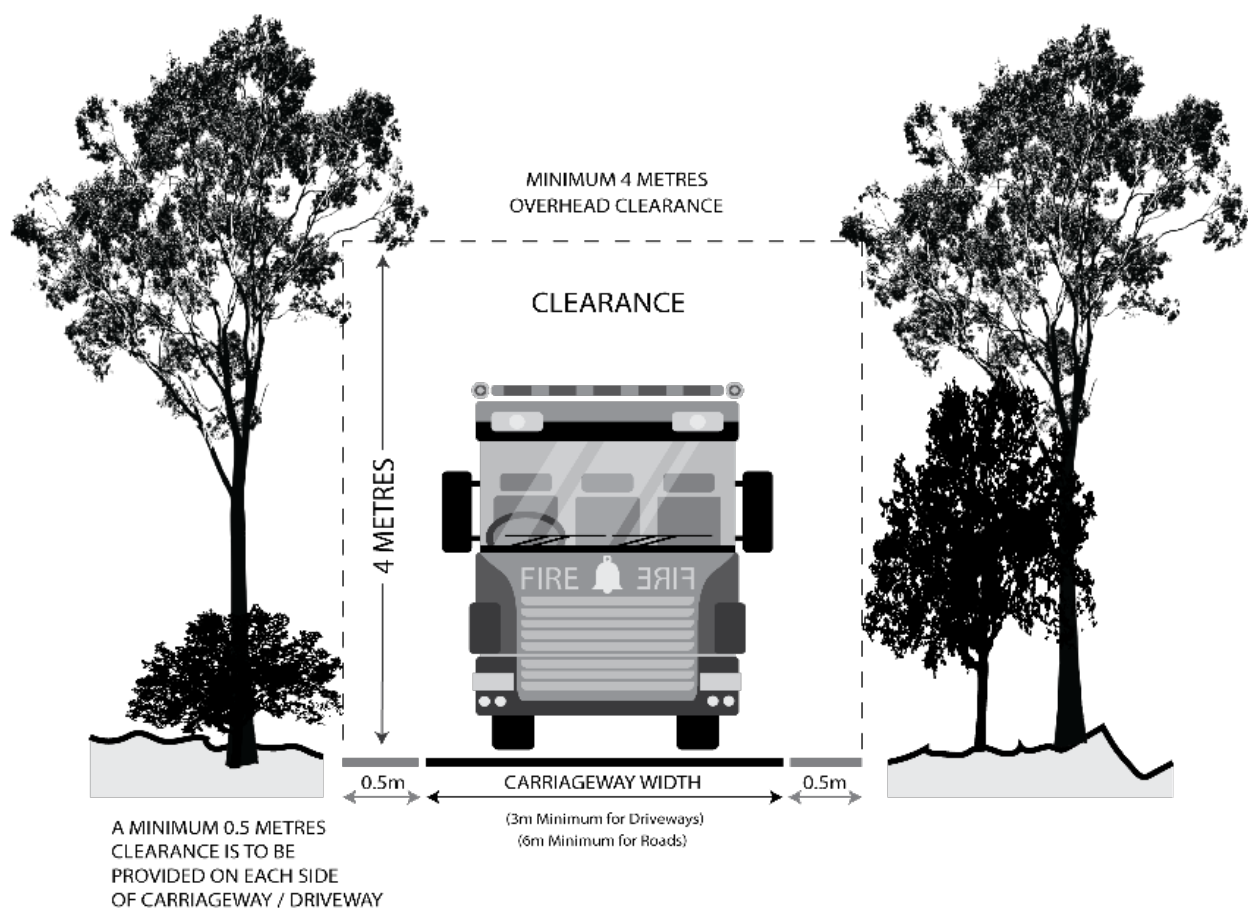
Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except if a relevant certificate accompanies the application for planning consent in respect of the development, any of the following classes of development (including alterations and additions which increase the floor area of such buildings by 10% or more):</p> <p>(a) land division creating one or more additional allotments</p> <p>(b) dwelling</p> <p>(c) ancillary accommodation</p> <p>(d) residential flat building</p> <p>(e) tourist accommodation</p> <p>(f) boarding home</p> <p>(g) dormitory style accommodation</p> <p>(h) workers' accommodation</p> <p>(i) student accommodation</p> <p>(j) pre-school</p>	South Australian Country Fire Service.	To provide expert assessment and direction to the relevant authority on the potential impacts of bushfire on the development.	Development of a class to which Schedule 9 clause 3 item 2 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

(k)	educational establishment			
(l)	retirement village			
(m)	supported accommodation			
(n)	residential park			
(o)	hospital			
(p)	camp ground.			

Figures and Diagrams

Fire Appliance Clearances

Figure 1 - Overhead and Side Clearances



Roads and Driveway Design

Figure 2 - Road and Driveway Curves

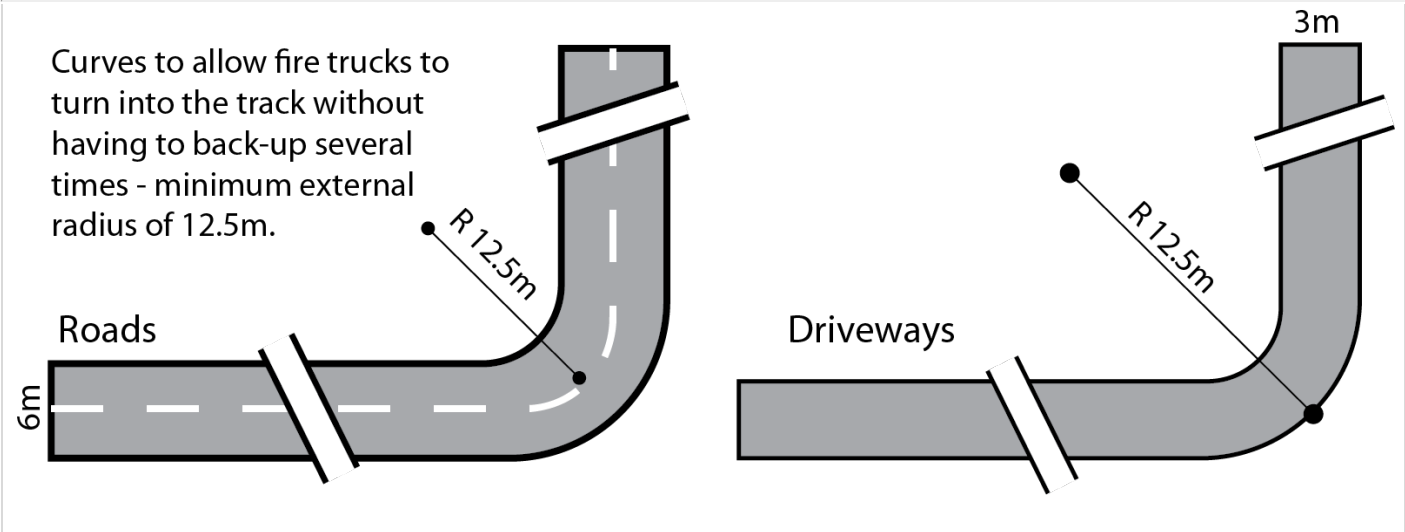


Figure 3 - Full Circle Turning Area

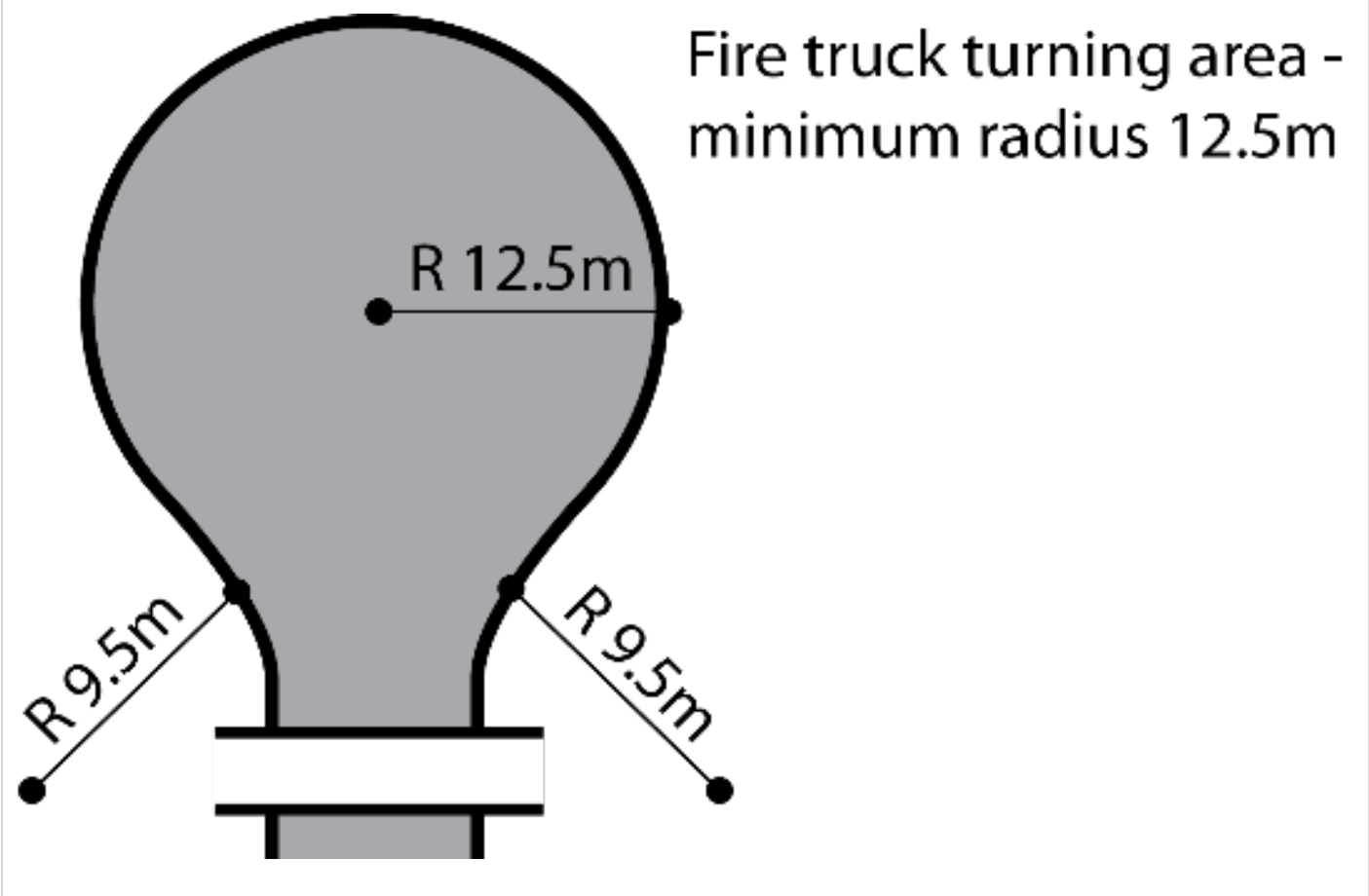


Figure 4 - 'T' or 'Y' Shaped Turning Head

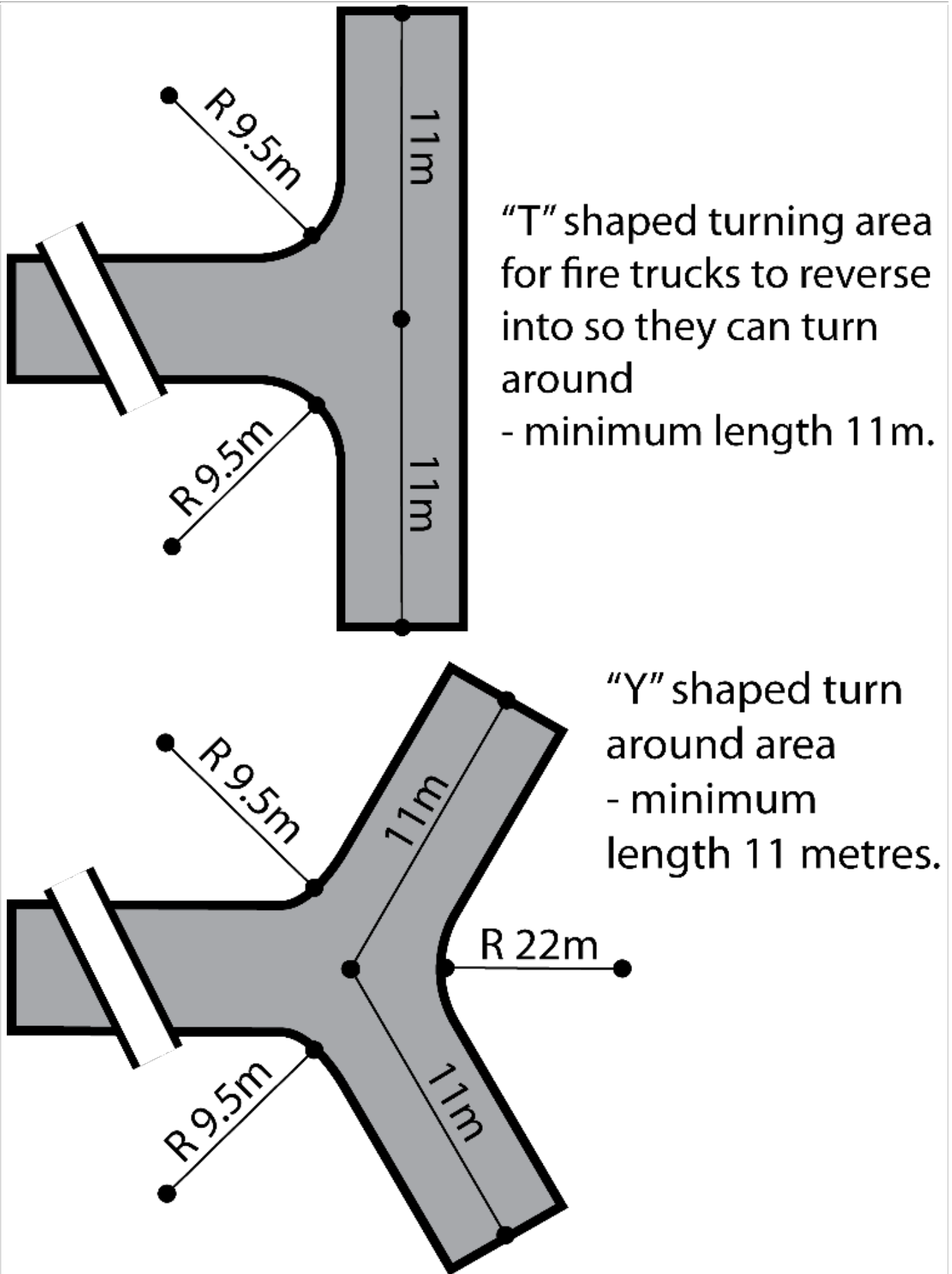
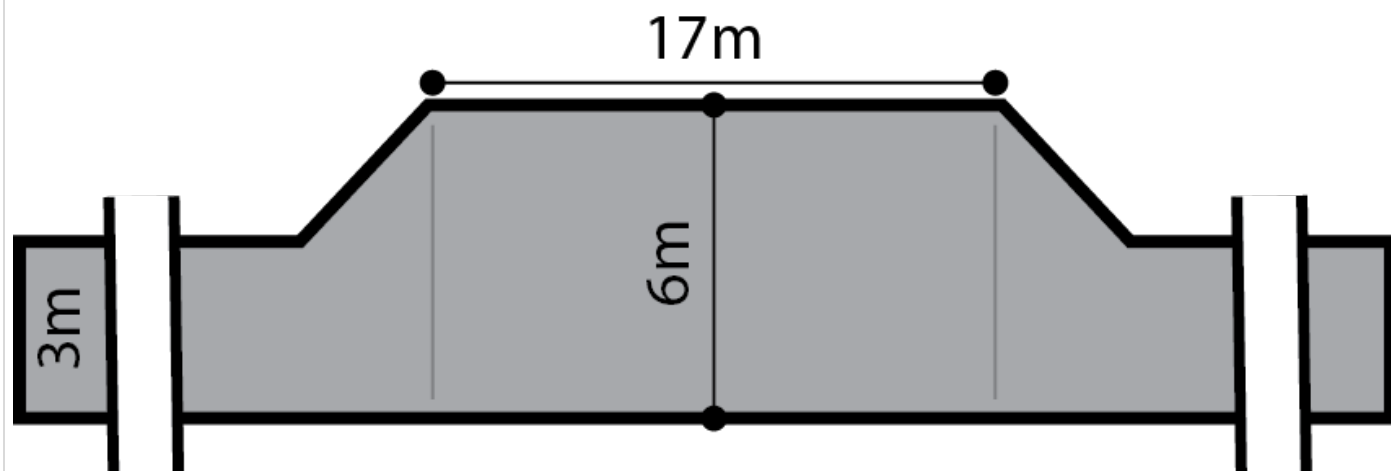


Figure 5 - Driveway Passing Bays

Passing bay for fire trucks - minimum width 6 metres, minimum length 17 metres.



Hazards (Flooding - Evidence Required) Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development adopts a precautionary approach to mitigate potential impacts on people, property, infrastructure and the environment from potential flood risk through the appropriate siting and design of development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Flood Resilience	
PO 1.1 Development is sited, designed and constructed to minimise the risk of entry of potential floodwaters where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.	DTS/DPF 1.1 Habitable buildings, commercial and industrial buildings, and buildings used for animal keeping incorporate a finished floor level at least 300mm above: <ul style="list-style-type: none"> (a) the highest point of top of kerb of the primary street or (b) the highest point of natural ground level at the primary street boundary where there is no kerb

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
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None	None	None	None
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Heritage Adjacency Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Development adjacent to a State or Local Heritage Place does not dominate, encroach on or unduly impact on the setting of the Place.	DTS/DPF 1.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development that may materially affect the context of a State Heritage Place.	Minister responsible for the administration of the <i>Heritage Places Act 1993</i> .	To provide expert assessment and direction to the relevant authority on the potential impacts of development adjacent State Heritage Places.	Development of a class to which Schedule 9 clause 3 item 17 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Native Vegetation Overlay

Assessment Provisions (AP)

Desired Outcome

DO 1	Areas of native vegetation are protected, retained and restored in order to sustain biodiversity, threatened species and vegetation communities, fauna habitat, ecosystem services, carbon storage and amenity values.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Environmental Protection	
<p>PO 1.1</p> <p>Development avoids, or where it cannot be practically avoided, minimises the clearance of native vegetation taking into account the siting of buildings, access points, bushfire protection measures and building maintenance.</p>	<p>DTS/DPF 1.1</p> <p>An application is accompanied by:</p> <ul style="list-style-type: none"> (a) a declaration stating that the proposal will not, or would not, involve clearance of native vegetation under the Native Vegetation Act 1991, including any clearance that may occur: <ul style="list-style-type: none"> (i) in connection with a relevant access point and / or driveway (ii) within 10m of a building (other than a residential building or tourist accommodation) (iii) within 20m of a dwelling or addition to an existing dwelling for fire prevention and control (iv) within 50m of residential or tourist accommodation in connection with a requirement under a relevant overlay to establish an asset protection zone in a bushfire prone area or (b) a report prepared in accordance with Regulation 18(2) (a) of the Native Vegetation Regulations 2017 that establishes that the clearance is categorised as 'Level 1 clearance'.
<p>PO 1.2</p> <p>Native vegetation clearance in association with development avoids the following:</p> <ul style="list-style-type: none"> (a) significant wildlife habitat and movement corridors (b) rare, vulnerable or endangered plants species (c) native vegetation that is significant because it is located in an area which has been extensively cleared (d) native vegetation that is growing in, or in association with, a wetland environment. 	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Intensive animal husbandry and agricultural activities are sited, set back and designed to minimise impacts on native vegetation, including impacts on native vegetation in an adjacent State Significant Native Vegetation Area, from:</p> <ul style="list-style-type: none"> (a) the spread of pest plants and phytophthora (b) the spread of non-indigenous plants species (c) excessive nutrient loading of the soil or loading arising 	<p>DTS/DPF 1.3</p> <p>Development within 500 metres of a boundary of a State Significant Native Vegetation Area does not involve any of the following:</p> <ul style="list-style-type: none"> (a) horticulture (b) intensive animal husbandry (c) dairy (d) commercial forestry

from surface water runoff (d) soil compaction (e) chemical spray drift.	(e) aquaculture.
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Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development that is the subject of a report prepared in accordance with Regulation 18(2)(a) of the <i>Native Vegetation Regulations 2017</i> that categorises the clearance, or potential clearance, as 'Level 3 clearance' or 'Level 4 clearance'.	Native Vegetation Council	To provide expert assessment and direction to the relevant authority on the potential impacts of development on native vegetation.	Development of a class to which Schedule 9 clause 3 item 11 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Urban Transport Routes Overlay

Assessment Provisions (AP)

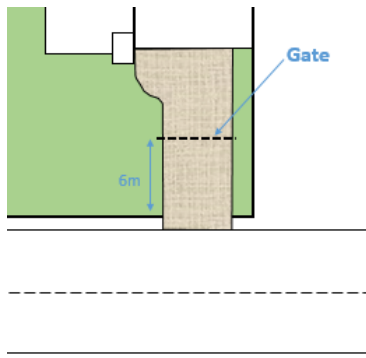
Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

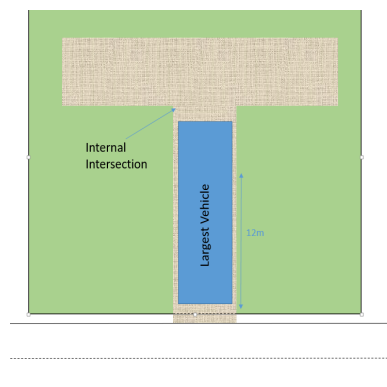
Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
PO 1.1 Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.	DTS/DPF 1.1 An access point satisfies (a), (b) or (c): <ul style="list-style-type: none"> (a) where servicing a single (1) dwelling / residential allotment: <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road

	<ul style="list-style-type: none"> (v) it will have a width of between 3m and 4m (measured at the site boundary) <p>(b) where the development will result in 2 and up to 6 dwellings:</p> <ul style="list-style-type: none"> (i) (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site) <p>(c) where the development will result in 7 or more dwellings, or is a non-residential land use:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the road and B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.
Access - On-Site Queuing	
<p>PO 2.1</p> <p>Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional</p>	<p>DTS/DPF 2.1</p> <p>An access point in accordance with one of the following:</p> <ul style="list-style-type: none"> (a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram:

performance of the road and maintain safe vehicle movements.



- (b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
- (i) is expected to be serviced by vehicles with a length no greater than 6.4m
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
- (c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
- (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
 - (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop
 - (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram:



Access - (Location Spacing) - Existing Access Point

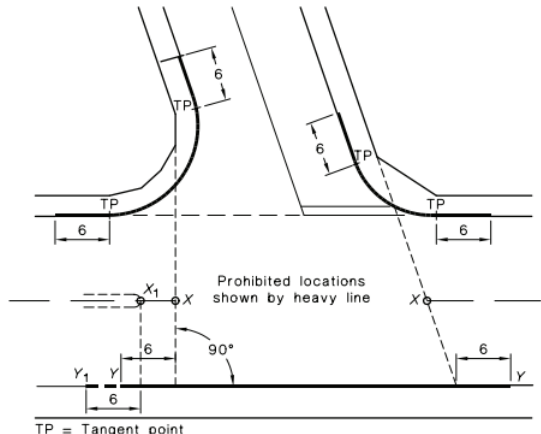
PO 3.1

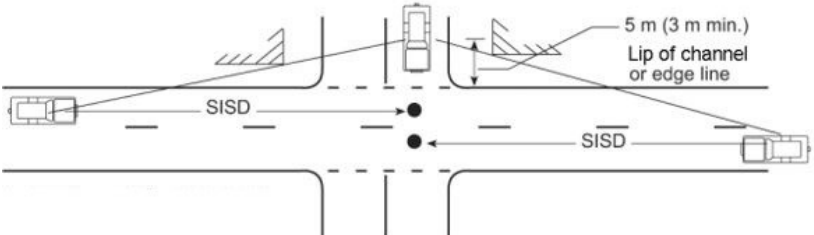
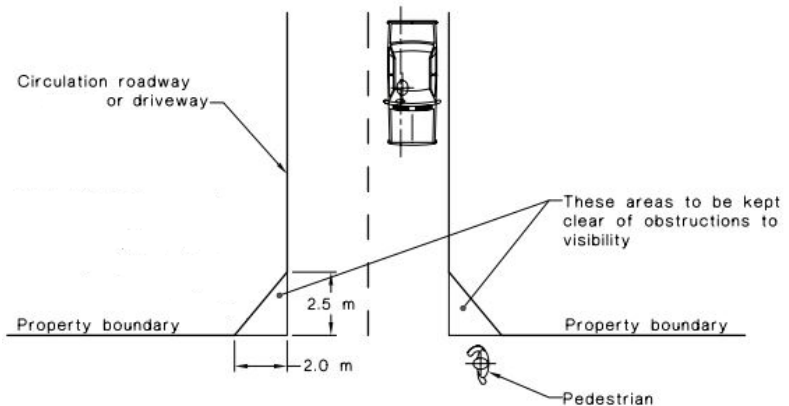
Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.

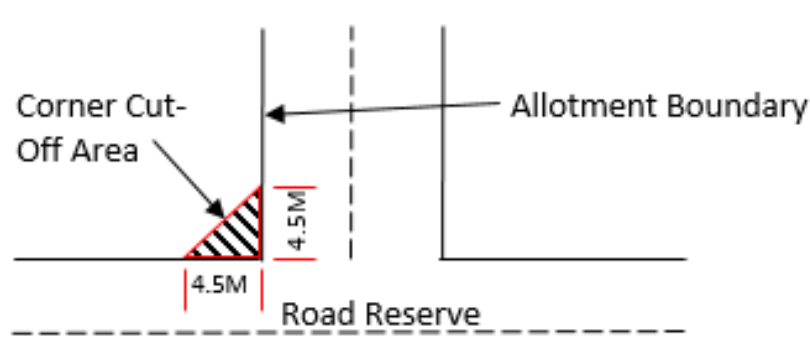
DTS/DPF 3.1

An existing access point satisfies (a), (b) or (c):

- (a) it will not service, or is not intended to service, more than 6 dwellings
- (b) it is not located on a Controlled Access Road and will not service development that will result in (b) a larger class of vehicle expected to access the site using the existing access
- (c) is not located on a Controlled Access Road and development constitutes:
 - (i) a change of use between an office <500m² gross leasable floor area

	<p>and a consulting room <500m² gross leasable floor area or vice versa</p> <ul style="list-style-type: none">(ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment(iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area(iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area(v) an office or consulting room with a <500m² gross leasable floor area.									
Access – Location (Spacing) – New Access Points										
PO 4.1	<p>DTS/DPF 4.1</p> <p>A new access point satisfies (a), (b) or (c):</p> <p>(a) where a development site is intended to serve between 1 and 6 dwellings and has frontage to a local road (not being a Controlled Access Road) with a speed environment of 60km/h or less, the new access point is provided on the local road and located a minimum of 6.0m from the tangent point as shown in the following diagram:</p> <div><p>TP = Tangent point</p><p>NOTE: The points marked X_1 and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point Y_1.</p></div> <p>(b) where the development site is intended to serve between 1 and 6 dwellings and access from a local road (being a road that is not a State Maintained Road) is not available, the new access:</p> <ul style="list-style-type: none">(i) is not located on a Controlled Access Road(ii) is not located on a section of road affected by double barrier lines(iii) will be on a road with a speed environment of 70km/h or less(iv) is located outside of the bold lines on the diagram shown in the diagram following part (a)(v) located minimum of 6m from a median opening or pedestrian crossing <p>(c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:</p> <table><tr><th>Speed Limit</th><th>Separation between access points</th><th>Separation from public road junctions and merging/terminating lanes</th></tr><tr><td>50 km/h or less</td><td>No spacing requirement</td><td>20m</td></tr><tr><td>60 km/h</td><td>30m</td><td>73m</td></tr></table>	Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes	50 km/h or less	No spacing requirement	20m	60 km/h	30m	73m
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Access - Location (Sight Lines)																													
PO 5.1	DTS/DPF 5.1	<p>Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.</p> <p>An access point satisfies (a) or (b):</p> <p>(a) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):</p> <table> <tr> <th>Speed Limit</th><th>Access point serving 1-6 dwellings</th><th>Access point serving all other development</th></tr> <tr> <td>40 km/h or less</td><td>40m</td><td>73m</td></tr> <tr> <td>50 km/h</td><td>55m</td><td>97m</td></tr> <tr> <td>60 km/h</td><td>73m</td><td>123m</td></tr> <tr> <td>70 km/h</td><td>92m</td><td>151m</td></tr> <tr> <td>80 km/h</td><td>114m</td><td>181m</td></tr> <tr> <td>90 km/h</td><td>139m</td><td>214m</td></tr> <tr> <td>100 km/h</td><td>165m</td><td>248m</td></tr> <tr> <td>110km/h</td><td>193m</td><td>285m</td></tr> </table>  <p>(b) pedestrian sightlines in accordance with the following diagram:</p> 	Speed Limit	Access point serving 1-6 dwellings	Access point serving all other development	40 km/h or less	40m	73m	50 km/h	55m	97m	60 km/h	73m	123m	70 km/h	92m	151m	80 km/h	114m	181m	90 km/h	139m	214m	100 km/h	165m	248m	110km/h	193m	285m
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Access – Mud and Debris																													
PO 6.1	DTS/DPF 6.1																												

Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.	Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).
Access - Stormwater	
PO 7.1 Access points are designed to minimise negative impact on roadside drainage of water.	DTS/DPF 7.1 Development does not: (a) decrease the capacity of an existing drainage point (b) restrict or prevent the flow of stormwater through an existing drainage point and system.
Building on Road Reserve	
PO 8.1 Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe movements by all road users.	DTS/DPF 8.1 Buildings or structures are not located on, above or below the road reserve.
Public Road Junctions	
PO 9.1 New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.	DTS/DPF 9.1 Development does not comprise any of the following: (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
Corner Cut-Offs	
PO 10.1 Development is located and designed to maintain sightlines for drivers turning into and out of public road junctions to contribute to driver safety.	DTS/DPF 10.1 Development does not involve building work, or building work is located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram: 

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

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Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Part 4 - General Development Policies

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.</p>	<p>DTS/DPF 1.1</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i> (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is:

	<ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributes to the character of the immediate area (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access, and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.
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Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All development	
External Appearance	
<p>PO 1.4</p> <p>Plant, exhaust and intake vents and other technical equipment is integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by:</p> <ul style="list-style-type: none"> (a) positioning plant and equipment in unobtrusive locations viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses. 	<p>DTS/DPF 1.4</p> <p>Development does not incorporate any structures that protrude beyond the roofline.</p>
On-site Waste Treatment Systems	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
Carparking Appearance	
<p>PO 7.4</p> <p>Street level vehicle parking areas incorporate tree planting to provide shade and reduce solar heat absorption and reflection.</p>	<p>DTS/DPF 7.4</p> <p>None are applicable.</p>
<p>PO 7.5</p> <p>Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.</p>	<p>DTS/DPF 7.5</p> <p>None are applicable.</p>

Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.

Infrastructure and Renewable Energy Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Water Supply	
<p>PO 11.1</p> <p>Development is connected to an appropriate water supply to meet the ongoing requirements of the intended use.</p>	<p>DTS/DPF 11.1</p> <p>Development is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the on-going requirements of the development.</p>
Wastewater Services	
<p>PO 12.1</p> <p>Development is connected to an approved common wastewater disposal service with the capacity to meet the requirements of the intended use. Where this is not available an appropriate on-site service is provided to meet the ongoing requirements of the intended use in accordance with the following:</p> <ul style="list-style-type: none"> (a) it is wholly located and contained within the allotment of the development it will service (b) in areas where there is a high risk of contamination of surface, ground, or marine water resources from on-site disposal of liquid wastes, disposal systems are included to minimise the risk of pollution to those water resources (c) septic tank effluent drainage fields and other wastewater disposal areas are located away from watercourses and flood prone, sloping, saline or poorly drained land to minimise environmental harm. 	<p>DTS/DPF 12.1</p> <p>Development is connected, or will be connected, to an approved common wastewater disposal service with the capacity to meet the requirements of the development. Where this is not available it is instead capable of being serviced by an on-site waste water treatment system in accordance with the following:</p> <ul style="list-style-type: none"> (a) the system is wholly located and contained within the allotment of development it will service; and (b) the system will comply with the requirements of the South Australian Public Health Act 2011.

PO 12.2	DTS/DPF 12.2
Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Interface with Rural Activities	
PO 9.3 Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing land-based aquaculture activities and do not prejudice the continued operation of these activities.	DTS/DPF 9.3 Sensitive receivers are located at least 200m from the boundary of a site used for land-based aquaculture and associated components in other ownership.
PO 9.4 Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing dairies including associated wastewater lagoons and liquid/solid waste storage and disposal facilities and do not prejudice the continued operation of these activities.	DTS/DPF 9.4 Sensitive receivers are sited at least 500m from the boundary of a site used for a dairy and associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities in other ownership.
PO 9.5 Sensitive receivers are located and designed to mitigate the potential impacts from lawfully existing facilities used for the handling, transportation and storage of bulk commodities (recognising the potential for extended hours of operation) and do not prejudice the continued operation of these activities.	DTS/DPF 9.5 Sensitive receivers are located away from the boundary of a site used for the handling, transportation and/or storage of bulk commodities in other ownership in accordance with the following: <ul style="list-style-type: none"> (a) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility (b) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals) where the handling of these materials into or from vessels does not exceed 100 tonnes per day (c) 500m or more, where it involves the storage of bulk petroleum in individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1000 cubic metres (d) 500m or more, where it involves the handling of coal

	<p>with a capacity up to 1 tonne per day or a storage capacity up to 50 tonnes</p> <p>(e) 1000m or more, where it involves the handling of coal with a capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes.</p>
Interface with Mines and Quarries (Rural and Remote Areas)	
<p>PO 10.1</p> <p>Sensitive receivers are separated from existing mines to minimise the adverse impacts from noise, dust and vibration.</p>	<p>DTS/DPF 10.1</p> <p>Sensitive receivers are located no closer than 500m from the boundary of a Mining Production Tenement under the <i>Mining Act 1971</i>.</p>

Site Contamination

Assessment Provisions (AP)

Desired Outcome	
DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site contamination.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Ensure land is suitable for use when land use changes to a more sensitive use.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies (a), (b), (c) or (d):</p> <ul style="list-style-type: none"> (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a more sensitive use (c) involves a change in the use of land to a more sensitive use on land at which site contamination is unlikely to exist (as demonstrated in a site contamination declaration form) (d) involves a change in the use of land to a more sensitive use on land at which site contamination exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> (i) a site contamination audit report has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that- <ul style="list-style-type: none"> A. site contamination does not exist (or no longer exists) at the land or B. the land is suitable for the proposed use or range of uses (without the need for any further remediation) or C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the

	<p>remediation works will be implemented in association with the development)</p> <p>and</p> <p>(ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).</p>
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Tourism Development

Assessment Provisions (AP)

Desired Outcome	
DO 1	Tourism development is built in locations that cater to the needs of visitors and positively contributes to South Australia's visitor economy.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Tourism development complements and contributes to local, natural, cultural or historical context where: <ul style="list-style-type: none"> (a) it supports immersive natural experiences (b) it showcases South Australia's landscapes and produce (c) its events and functions are connected to local food, wine and nature. 	DTS/DPF 1.1 None are applicable.
PO 1.2 Tourism development comprising multiple accommodation units (including any facilities and activities for use by guests and visitors) is clustered to minimise environmental and contextual impact.	DTS/DPF 1.2 None are applicable.
Caravan and Tourist Parks	
PO 2.1 Potential conflicts between long-term residents and short-term tourists are minimised through suitable siting and design measures.	DTS/DPF 2.1 None are applicable.
PO 2.2 Occupants are provided privacy and amenity through landscaping and fencing.	DTS/DPF 2.2 None are applicable.
PO 2.3	DTS/DPF 2.3

Communal open space and centrally located recreation facilities are provided for guests and visitors.	12.5% or more of a caravan park comprises clearly defined communal open space, landscaped areas and areas for recreation.
PO 2.4 Perimeter landscaping is used to enhance the amenity of the locality.	DTS/DPF 2.4 None are applicable.
PO 2.5 Amenity blocks (showers, toilets, laundry and kitchen facilities) are sufficient to serve the full occupancy of the development.	DTS/DPF 2.5 None are applicable.
PO 2.6 Long-term occupation does not displace tourist accommodation, particularly in important tourist destinations such as coastal and riverine locations.	DTS/DPF 2.6 None are applicable.
Tourist accommodation in areas constituted under the National Parks and Wildlife Act 1972	
PO 3.1 Tourist accommodation avoids delicate or environmentally sensitive areas such as sand dunes, cliff tops, estuaries, wetlands or substantially intact strata of native vegetation (including regenerated areas of native vegetation lost through bushfire).	DTS/DPF 3.1 None are applicable.
PO 3.2 Tourist accommodation is sited and designed in a manner that is subservient to the natural environment and where adverse impacts on natural features, landscapes, habitats and cultural assets are avoided.	DTS/DPF 3.2 None are applicable.
PO 3.3 Tourist accommodation and recreational facilities, including associated access ways and ancillary structures, are located on cleared (other than where cleared as a result of bushfire) or degraded areas or where environmental improvements can be achieved.	DTS/DPF 3.3 None are applicable.
PO 3.4 Tourist accommodation is designed to prevent conversion to private dwellings through: (a) comprising a minimum of 10 accommodation units (b) clustering separated individual accommodation units (c) being of a size unsuitable for a private dwelling (d) ensuring functional areas that are generally associated with a private dwelling such as kitchens and laundries are excluded from, or physically separated from individual accommodation units, or are of a size unsuitable for a private dwelling.	DTS/DPF 3.4 None are applicable.

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
PO 1.4 Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	DTS/DPF 1.4 All vehicle manoeuvring occurs onsite.
Vehicle Access	
PO 3.1 Safe and convenient access minimises impact or interruption on the operation of public roads.	DTS/DPF 3.1 The access is: (a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land or (b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.
PO 3.5 Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.	DTS/DPF 3.5 Vehicle access to designated car parking spaces satisfy (a) or (b): (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
Access for People with Disabilities	
PO 4.1 Development is sited and designed to provide safe, dignified and	DTS/DPF 4.1 None are applicable.

convenient access for people with a disability.	
Vehicle Parking Rates	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements (b) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
Vehicle Parking Areas	
<p>PO 6.1</p> <p>Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.</p>	<p>DTS/DPF 6.1</p> <p>Movement between vehicle parking areas within the site can occur without the need to use a public road.</p>
<p>PO 6.2</p> <p>Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>
<p>PO 6.6</p> <p>Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.</p>	<p>DTS/DPF 6.6</p> <p>Loading areas and designated parking spaces are wholly located within the site.</p>

Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)
<p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>	
Residential Development	
Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Group Dwelling	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.

	<p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Residential Flat Building	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Row Dwelling where vehicle access is from the primary street	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Semi-Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Aged / Supported Accommodation	
Retirement village	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Supported accommodation	0.3 spaces per bed.
Residential Development (Other)	
Ancillary accommodation	No additional requirements beyond those associated with the main dwelling.
Residential park	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Student accommodation	0.3 spaces per bed.
Workers' accommodation	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
Tourist	

Caravan park / tourist park	<p>Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation.</p> <p>Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation.</p> <p>A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.</p>
Tourist accommodation	1 car parking space per accommodation unit / guest room.
Commercial Uses	
Auction room/ depot	1 space per 100m ² of building floor area plus an additional 2 spaces.
Automotive collision repair	3 spaces per service bay.
Call centre	8 spaces per 100m ² of gross leasable floor area.
Motor repair station	3 spaces per service bay.
Office	4 spaces per 100m ² of gross leasable floor area.
Retail fuel outlet	3 spaces per 100m ² gross leasable floor area.
Service trade premises	<p>2.5 spaces per 100m² of gross leasable floor area</p> <p>1 space per 100m² of outdoor area used for display purposes.</p>
Shop (no commercial kitchen)	<p>5.5 spaces per 100m² of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.</p> <p>5 spaces per 100m² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.</p>
Shop (in the form of a bulky goods outlet)	2.5 spaces per 100m ² of gross leasable floor area.
Shop (in the form of a restaurant or involving a commercial kitchen)	<p>Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat.</p> <p>Premises with take-away service but with no seats - 12 spaces per 100m² of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point.</p> <p>Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.</p>

Community and Civic Uses	
Childcare centre	0.25 spaces per child
Library	4 spaces per 100m ² of total floor area.
Community facility	10 spaces per 100m ² of total floor area.
Hall / meeting hall	0.2 spaces per seat.
Place of worship	1 space for every 3 visitor seats.
Pre-school	1 per employee plus 0.25 per child (drop off/pick up bays)
Educational establishment	<p>For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any time.</p>
Health Related Uses	
Hospital	<p>4.5 spaces per bed for a public hospital.</p> <p>1.5 spaces per bed for a private hospital.</p>
Consulting room	4 spaces per consulting room excluding ancillary facilities.
Recreational and Entertainment Uses	
Cinema complex	0.2 spaces per seat.
Concert hall / theatre	0.2 spaces per seat.
Hotel	1 space for every 2m ² of total floor area in a public bar plus 1 space for every 6m ² of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
Indoor recreation facility	<p>6.5 spaces per 100m² of total floor area for a Fitness Centre</p> <p>4.5 spaces per 100m² of total floor area for all other Indoor recreation facilities.</p>

Industry/Employment Uses	
Fuel depot	1.5 spaces per 100m ² total floor area 1 spaces per 100m ² of outdoor area used for fuel depot activity purposes.
Industry	1.5 spaces per 100m ² of total floor area.
Store	0.5 spaces per 100m ² of total floor area.
Timber yard	1.5 spaces per 100m ² of total floor area 1 space per 100m ² of outdoor area used for display purposes.
Warehouse	0.5 spaces per 100m ² total floor area.
Other Uses	
Funeral Parlour	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the parlour.
Radio or Television Station	5 spaces per 100m ² of total building floor area.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column subject to the following:

- (a) the location of the development is unable to satisfy the requirements of Table 2 – Criteria (other than where a location is exempted from the application of those criteria)
- or
- (b) the development satisfies Table 2 – Criteria (or is exempt from those criteria) and is located in an area where a lawfully established carparking fund operates, in which case the number of spaces are reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate		Designated Areas
	Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.		
	Minimum number of spaces	Maximum number of spaces	
Development generally			
All classes of development	No minimum.	No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:	Capital City Zone
		1 space for each dwelling with	City Main Street Zone City Riverbank Zone Adelaide Park Lands Zone

		<p>a total floor area less than 75 square metres</p> <p>2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres</p> <p>3 spaces for each dwelling with a total floor area greater than 150 square metres.</p> <p>Residential flat building or Residential component of a multi-storey building: 1 visitor space for each 6 dwellings.</p>	<p>Business Neighbourhood Zone (within the City of Adelaide)</p> <p>The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone</p>
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	5 spaces per 100m ² of gross leasable floor area.	<p>City Living Zone</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p> <p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone</p>
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	6 spaces per 100m ² of gross leasable floor area.	<p>Strategic Innovation Zone</p> <p>Suburban Activity Centre Zone</p> <p>Suburban Business Zone</p> <p>Business Neighbourhood Zone</p> <p>Suburban Main Street Zone</p> <p>Urban Activity Centre Zone</p>
Tourist accommodation	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	<p>City Living Zone</p> <p>Urban Activity Centre Zone</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p> <p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone</p>
Residential development			
Residential component of a multi-storey	Dwelling with no separate bedroom -0.25 spaces per	None specified.	City Living Zone

building	dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.		Strategic Innovation Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Residential flat building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone

Table 2 - Criteria:

The following criteria are used in conjunction with Table 2. The 'Exception' column identifies locations where the criteria do not apply and the car parking rates in Table 2 are applicable.

Criteria	Exceptions
<p>The designated area is wholly located within Metropolitan Adelaide and any part of the development site satisfies one or more of the following:</p> <p>(a) is within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service⁽²⁾</p> <p>(b) is within 400 metres of a bus interchange⁽¹⁾</p> <p>(c) is within 400 metres of an O-Bahn interchange⁽¹⁾</p> <p>(d) is within 400 metres of a passenger rail station⁽¹⁾</p> <p>(e) is within 400 metres of a passenger tram station⁽¹⁾</p> <p>(f) is within 400 metres of the Adelaide Parklands.</p>	<p>(a) All zones in the City of Adelaide</p> <p>(b) Strategic Innovation Zone in the following locations:</p> <p>(i) City of Burnside</p> <p>(ii) City of Marion</p> <p>(iii) City of Mitcham</p> <p>(c) Urban Corridor (Boulevard) Zone</p> <p>(d) Urban Corridor (Business) Zone</p> <p>(e) Urban Corridor (Living) Zone</p> <p>(f) Urban Corridor (Main Street) Zone</p> <p>(g) Urban Neighbourhood Zone</p>

[NOTE(S): (1) Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose

waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles. (2) A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.]

31 CRESCENT DR NORTON SUMMIT SA 5136**Address:**Click to view a detailed interactive [SAILIS](#) in [SAILIS](#)

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**Property Zoning Details****Overlay**

Environment and Food Production Area
 Hazards (Bushfire - High Risk)
 Heritage Adjacency
 Hazards (Flooding - Evidence Required)
 Limited Land Division
 Native Vegetation
 Prescribed Wells Area
 Traffic Generating Development
 Urban Transport Routes

Zone

Productive Rural Landscape

Development Pathways

- Productive Rural Landscape

1. Accepted Development

Means that the development type does not require planning consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

- None

2. Code Assessed - Deemed to Satisfy

Means that the development type requires consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

- Outbuilding

3. Code Assessed - Performance Assessed

Performance Assessed development types listed below are those for which the Code identifies relevant policies. Additional development types that are not listed as Accepted, Deemed to Satisfy or Restricted default to a Performance assessed Pathway. Please contact your local council for more information.

- None

4. Impact Assessed - Restricted

Means that the development type requires approval. Classes of development that are classified as Restricted are listed in Table 4 of the relevant Zones.

Property Policy Information for above selection

Part 2 - Zones and Sub Zones

Productive Rural Landscape Zone

Assessment Provisions (AP)

Desired Outcome	
DO 1	A diverse range of land uses at an appropriate scale and intensity that capitalise on the region's proximity to the metropolitan area and the tourist and lifestyle opportunities this presents while also conserving the natural and rural character, identity, biodiversity and sensitive environmental areas and scenic qualities of the landscape.
DO 2	A zone that promotes agriculture, horticulture, value adding opportunities, farm gate businesses, the sale and consumption of agricultural based products, tourist development and accommodation that expands the economic base and promotes its regional identity.
DO 3	Create local conditions that support new and continuing investment while seeking to promote co-existence with adjoining activities and mitigate land use conflicts.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 The productive value of rural land for a range of primary production and horticultural activities and associated value adding of primary produce (such as beverage production), retailing and tourism is supported, protected and maintained. The proliferation of land uses that may be sensitive to those activities is avoided.	DTS/DPF 1.1 Development comprises one or more of the following: (a) Advertisement (b) Agricultural building (c) Brewery (d) Carport (e) Cidery (f) Distillery (g) Dwelling (h) Dwelling addition (i) Farming (j) Function centre

	<ul style="list-style-type: none"> (k) Horse keeping (l) Horticulture (m) Industry (n) Low intensity animal husbandry (o) Outbuilding (p) Shop (q) Small-scale ground mounted solar power facility (r) Tourist accommodation (s) Transport distribution (t) Verandah (u) Warehouse (v) Winery (w) Workers' accommodation
Siting and Design	
PO 2.1 Development is provided with suitable vehicle access.	DTS/DPF 2.1 Development is serviced by an all-weather trafficable public road.
PO 2.2 Buildings are generally located on flat land to minimise cut and fill and the associated visual impacts.	DTS/DPF 2.2 Buildings: <ul style="list-style-type: none"> (a) are located on a site with a slope not greater than 10% (1-in-10) (b) do not result excavation and/or filling of land that is greater than 1.5m from natural ground level.
Horticulture	
PO 3.1 Horticulture is located and conducted on land that has the physical capability of supporting the activity and in a manner that: <ul style="list-style-type: none"> (a) enhances the productivity of the land for the growing of food and produce in a sustainable manner (b) avoids adverse interface conflicts with other land uses (c) utilises sound environmental practices to mitigate negative impacts on natural resources and water quality (d) is sympathetic to surrounding rural landscape character and amenity, where horticulture is proposed to be carried out in an enclosed building such as such as a greenhouse. 	DTS/DPF 3.1 Horticultural activities: <ul style="list-style-type: none"> (a) are conducted on an allotment with an area of at least 1ha (b) are sited on land with a slope not greater than 10% (1-in-10) (c) are not conducted within 50m of a watercourse or native vegetation (d) are not conducted within 100m of a sensitive receiver in other ownership (e) provide for a headland area between plantings and property boundaries of at least 10m in width (f) where carried out in an enclosed building such as a greenhouse, the building has a total floor area not greater than 250m² (g) in the form of olive growing, is not located within 500m of a conservation or national park.
Rural Industry	
PO 4.1 Small-scale industry (including beverage production and washing, processing, bottling and packaging activities), storage, warehousing, produce grading and packing, transport distribution or similar activities provide opportunities for diversification and value adding to locally sourced primary production activities.	DTS/DPF 4.1 Industries, storage, warehousing, produce grading and packing and transport distribution activities and similar activities (or any combination thereof): <ul style="list-style-type: none"> (a) are directly related and ancillary to a primary production use on the same or adjoining allotment

	<p>(b) are located on an allotment not less than 2ha in area</p> <p>(c) have a total floor area not exceeding 350m².</p>
<p>PO 4.2</p> <p>Expansion of established small-scale or new large scale industry (including beverage production and washing, processing, bottling and packaging activities), storage, warehousing, produce grading and packing, transport distribution or similar activities:</p> <p>(a) are commensurate with the allotment on which it is situated to mitigate adverse impacts on the amenity of land in other ownership and the character of locality</p> <p>(b) realise efficiencies in primary production related storage, sorting, packaging, manufacturing and the like</p> <p>(c) primarily involve primary production commodities sourced from the same allotment and/or surrounding rural areas.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Industry, storage, warehousing, transport distribution or similar activities are sited, designed and of a scale that maintains rural function and character in a manner that respects landscape amenity.</p>	<p>DTS/DPF 4.3</p> <p>Buildings and associated activities:</p> <p>(a) are setback at least 50m from all road and allotment boundaries</p> <p>(b) are not sited within 100m of a sensitive receiver in other ownership</p> <p>(c) have a building height not greater than 10m above natural ground level</p> <p>(d) incorporate the loading and unloading of vehicles within the confines of the allotment.</p>
Dwellings	
<p>PO 5.1</p> <p>Dwellings provide a convenient base for landowners to conduct and manage commercial scale primary production and related value adding activities without compromising the use of the allotment, adjacent land or long term purpose of the zone for primary production or related tourism values due to a proliferation of dwellings.</p>	<p>DTS/DPF 5.1</p> <p>Dwellings:</p> <p>(a) are located on an allotment with an area not less than:</p> <p>(b) are located on an allotment used for and is ancillary to primary production and/or primary production related value-adding activities</p> <p>(c) will not result in more than one dwelling on an allotment.</p> <p>In relation to DTS/DPF 5.1, in instances where:</p> <p>(d) more than one value is returned, refer to the <i>Minimum Dwelling Allotment Size Technical and Numeric Variation</i> layer in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p>(e) no value is returned for DTS/DPF 5.1(a) (ie there is a blank field), then there is no minimum dwelling allotment size applicable and DTS/DPF 5.1(a) is met.</p>
<p>PO 5.2</p> <p>Dwelling are sited, designed and of a scale that maintains a pleasant natural and rural character and amenity.</p>	<p>DTS/DPF 5.2</p> <p>Dwellings:</p> <p>(a) are setback from all allotment boundaries by at least 40m</p>

	<ul style="list-style-type: none"> (b) do not exceed 2 building levels and 9m measured from the top of the footings (c) have a wall height no greater than 6m.
<p>PO 5.3</p> <p>Development resulting in more than one dwelling on an allotment supports ageing in place for the owner of the allotment or multi-generational management of farms in a manner that minimises the potential loss of land available for primary production.</p>	<p>DTS/DPF 5.3</p> <p>Dwelling that will result in more than one dwelling on an allotment where all the following are satisfied:</p> <ul style="list-style-type: none"> (a) it is located within 20m of an existing dwelling (b) share the same utilities of the existing dwelling (c) will use the same access point from a public road as the existing dwelling (d) it is located on an allotment not less than 40ha in area (e) will not result in more than two dwellings on an allotment.
<p>PO 5.4</p> <p>Dwelling additions are sited, designed and of a scale that maintains a pleasant rural character and amenity.</p>	<p>DTS/DPF 5.4</p> <p>Additions or alterations to an existing dwelling:</p> <ul style="list-style-type: none"> (a) are setback behind the main façade of the existing dwelling (b) do not exceed 2 building levels and 9m measured from the top of the footings (c) have a wall height that is no greater than 6m from the top of the footings.
Shops, Tourism and Function Centres	
<p>PO 6.1</p> <p>Shops are associated with an existing primary production or primary production related value adding industry to support diversification of employment, provide services to visitors and showcase local and regional products.</p>	<p>DTS/DPF 6.1</p> <p>Shops, other than where located in The Cedars Subzone:</p> <ul style="list-style-type: none"> (a) are ancillary to and located on the same allotment or adjoining allotment used for primary production or primary production related value adding industries (b) offer for sale or consumption produce or goods that are primarily sourced, produced or manufactured on the same allotment or adjoining allotments (c) have a gross leasable floor area not exceeding 100m² or 250m² in the case of a cellar door (d) have an area for the display of produce or goods external to a building not exceeding 25m² (e) do not result in more than 75 seats for customer dining purposes in a restaurant.
<p>PO 6.2</p> <p>Shops that are proposed in new buildings are sited, designed and of a scale that maintains a pleasant rural character and amenity.</p>	<p>DTS/DPF 6.2</p> <p>Shops in new buildings:</p> <ul style="list-style-type: none"> (a) are setback from all property boundaries by at least 20m (b) are not sited within 100m of a sensitive receiver in other ownership (c) have a building height that does not exceed 9m above natural ground level.

PO 6.3	DTS/DPF 6.3
Tourist accommodation is associated with the primary use of the land for primary production or primary production related value adding industry to enhance and provide authentic visitor experiences.	<p>Tourist accommodation, other than where located in The Cedars Subzone:</p> <ul style="list-style-type: none"> (a) is ancillary to and located on the same allotment or an adjoining allotment used for primary production or primary production related value adding industry (b) in relation to the area used for accommodation: <ul style="list-style-type: none"> (i) where in a new building, does not exceed a total floor area of 100m² (ii) where in an existing building, does not exceed 150m² (c) does not result in more than one facility being located on the same allotment.
PO 6.4	DTS/DPF 6.4
Tourist accommodation proposed in a new building or buildings are sited, designed and of a scale that maintains a pleasant rural character and amenity.	<p>Tourist accommodation in new buildings:</p> <ul style="list-style-type: none"> (a) is setback from all property boundaries by at least 40m (b) has a building height that does not exceed 7m above natural ground level.
PO 6.5	DTS/DPF 6.5
Function centres are associated with the primary use of the land for primary production or primary production related value adding industry.	<p>Function centres, other than where located in The Cedars Subzone:</p> <ul style="list-style-type: none"> (a) are ancillary to and located on the same allotment or an adjoining allotment used for primary production or primary production related value adding industry (b) do not exceed a capacity of 75 persons for customer dining purposes.
PO 6.6	DTS/DPF 6.6
Function centres are sited, designed and of a scale that maintains a pleasant natural and rural character and amenity.	<p>Function centres:</p> <ul style="list-style-type: none"> (a) are located on an allotment having an area of at least 5ha (b) are setback from all property boundaries by at least 40m (c) are not sited within 100m of a sensitive receiver in other ownership (d) have a building height that does not exceed 9m above natural ground level.
Offices	
PO 7.1	DTS/DPF 7.1
Offices are directly related to and associated with the primary use of the land for primary production or primary production related value adding industry.	<p>Offices, other than where located in The Cedars Subzone:</p> <ul style="list-style-type: none"> (a) are ancillary to and located on the same allotment or an adjoining allotment used for primary production or primary production related value adding industry (b) have a gross leasable floor area not exceeding 100m².
Adaptive Reuse of Existing Buildings	

PO 8.1 Adaptive reuse of existing buildings for small-scale shops, offices, tourist accommodation or ancillary rural activities.	DTS/DPF 8.1 Development within an existing building is for any of the following: (a) a shop (b) office (c) tourist accommodation.
Workers' accommodation	
PO 9.1 Workers' accommodation provides short-term accommodation for persons temporarily engaged in the production, management or processing of primary produce.	DTS/DPF 9.1 Workers' accommodation: (a) is developed on a site at least 2ha in area (b) has a total floor area not exceeding 250m ² (c) is in the form of a single building or part of a cluster of buildings that are physically connected (d) amenities accommodate not more than 20 persons at any one time (e) is setback at least 50m from a road boundary (f) is setback at least 40m from a side or rear allotment boundary (g) is located within 20m of an existing dwelling on the same allotment (h) does not result in more than one facility being located on the same allotment.
Renewable Energy Facilities	
PO 10.1 Renewable energy facilities and ancillary development minimises significant fragmentation or displacement of existing primary production.	DTS/DPF 10.1 None are applicable.
PO 10.2 Small-scale ground mounted solar power facilities support rural production or value-adding industries.	DTS/DPF 10.2 None are applicable.
Built Form and Character	
PO 11.1 Large buildings designed and sited to reduce impacts on scenic and rural vistas by: (a) having substantial setbacks from boundaries and adjacent public roads (b) using low reflective materials and finishes that blend with the surrounding landscape (c) being located below ridgelines.	DTS/DPF 11.1 None are applicable.
Land Division	
PO 12.1 Land division creating additional allotments is not supported other than where located in The Cedars Subzone to support tourist development.	DTS/DPF 12.1 Except where the land division is proposed in The Cedars Subzone, no additional allotments are created.

PO 12.2	DTS/DPF 12.2
<p>Allotment boundaries, including by realignment, are positioned to incorporate sufficient space around existing residential, tourist accommodation and other habitable buildings (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) to:</p> <ul style="list-style-type: none"> (a) maintain a pleasant rural character and amenity for occupants (b) manage vegetation within the same allotment to mitigate bushfire hazard. 	<p>Allotment boundaries are located no closer to an existing residential, tourist accommodation or other habitable building than the greater of the following:</p> <ul style="list-style-type: none"> (a) 40m (b) the distance required to accommodate an asset protection zone wholly within the relevant allotment.
Agricultural Buildings	
PO 13.1	DTS/DPF 13.1
<p>Agricultural buildings and associated activities are sited, designed and of a scale that maintains a pleasant rural character and function.</p>	<p>Agricultural buildings:</p> <ul style="list-style-type: none"> (a) are located on an allotment having an area of at least 2ha (b) are setback at least 40m from an allotment boundary (c) have a building height not exceeding 10m above natural ground level (d) do not exceed 350m² in total floor area (e) incorporate the loading and unloading of vehicles within the confines of the allotment.
Outbuildings, Carports and Verandahs	
PO 14.1	DTS/DPF 14.1
<p>Outbuildings are sited, designed and of a scale that maintain a pleasant natural and rural character and amenity.</p>	<p>Outbuildings:</p> <ul style="list-style-type: none"> (a) have a primary street setback that is at least as far back as the building to which it is ancillary (b) have a combined total floor area that does not exceed 100m² (c) have walls that do not exceed 5m in height measured from natural ground level not including a gable end (d) have a total roof height that does not exceed 6m measured from natural ground level (e) if clad in sheet metal, it is pre-colour treated or painted in a non-reflective colour (f) will not result in more than 2 outbuildings on the same allotment.
PO 14.2	DTS/DPF 14.2
<p>Carports and verandahs are sited, designed and of a scale to maintain a pleasant natural and rural character and amenity.</p>	<p>Carports and verandahs:</p> <ul style="list-style-type: none"> (a) are set back from the primary street at least as far back as the building to which it is ancillary (b) have a total floor area that does not exceed 80m² (c) have a post height that does not exceed 3m measured from natural ground level (not including a gable end) (d) have a total roof height that does not exceed 5m measured from natural ground level (e) if clad in sheet metal, the cladding is pre-colour treated or painted in a non-reflective colour.
Concept Plans	

<p>PO 15.1</p> <p>Development is compatible with the outcomes sought by any relevant Concept Plan contained within Part 12 - Concept Plans of the Planning and Design Code to support the orderly development of land through staging of development and provision of infrastructure.</p>	<p>DTS/DPF 15.1</p> <p>The site of the development is wholly located outside any relevant Concept Plan boundary. The following Concept Plans are relevant:</p> <p>In relation to DTS/DPF 15.1, in instances where:</p> <ul style="list-style-type: none"> (a) one or more Concept Plan is returned, refer to Part 12 - Concept Plans in the Planning and Design Code to determine if a Concept Plan is relevant to the site of the proposed development. Note: multiple concept plans may be relevant. (b) in instances where 'no value' is returned, there is no relevant concept plan and DTS/DPF 15.1 is met.
Advertisements	
<p>PO 16.1</p> <p>Freestanding advertisements that identify the associated business without creating a visually dominant element within the locality.</p>	<p>DTS/DPF 16.1</p> <p>Freestanding advertisements:</p> <ul style="list-style-type: none"> (a) do not exceed 2m in height (b) do not have a sign face that exceeds 2m² per side.

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

A class of development listed in Column A is excluded from notification provided that it does not fall within a corresponding exclusion prescribed in Column B. In instances where development falls within multiple classes within Column A, each clause is to be read independently such that if a development is excluded from notification by any clause, it is, for the purposes of notification excluded irrespective of any other clause.

Class of Development (Column A)	Exceptions (Column B)
1. A kind of development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> (a) advertisement (b) agricultural building (c) air handling unit, air conditioning system or exhaust fan (d) ancillary accommodation (e) building work on railway land (f) carport (g) demolition (h) dwelling 	None specified.

<ul style="list-style-type: none"> (i) dwelling addition (j) farming (k) horse keeping (l) internal building work (m) land division (n) outbuilding (o) private bushfire shelter (p) protective tree netting structure (q) replacement building (r) retaining wall (s) solar photovoltaic panels (roof mounted) (t) shade sail (u) swimming pool or spa pool (v) temporary accommodation in an area affected by bushfire (w) tree damaging activity (x) verandah (y) water tank. 	
<p>3. Any development involving any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> (a) industry (b) store (c) warehouse. 	<p>Except development that does not satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 4.1 2. Productive Rural Landscape Zone DTS/DPF 4.3.
<p>4. Demolition.</p>	<p>Except any of the following:</p> <ul style="list-style-type: none"> 1. the demolition of a State or Local Heritage Place 2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
<p>5. Function centre within The Cedars Subzone.</p>	<p>None specified.</p>
<p>6. Function centre.</p>	<p>Except function centre that does not satisfy Productive Rural Landscape Zone DTS/DPF 6.6.</p>
<p>7. Horticulture.</p>	<p>Except horticulture that does not satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 3.1(d) 2. Productive Rural Landscape Zone DTS/DPF 3.1(e).
<p>8. Shop within The Cedars Subzone.</p>	<p>None specified.</p>
<p>9. Shop.</p>	<p>Except shop that does not satisfy any of the following:</p> <ul style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 6.1 2. Productive Rural Landscape Zone DTS/DPF 6.2.
<p>10. Tourist accommodation within The Cedars Subzone.</p>	<p>None specified.</p>

11. Tourist accommodation.	<p>Except tourist accommodation that does not to satisfy any of the following:</p> <ol style="list-style-type: none"> 1. Productive Rural Landscape Zone DTS/DPF 6.3 2. Productive Rural Landscape Zone DTS/DPF 6.4.
Placement of Notices - Exemptions for Performance Assessed Development	
None specified.	
Placement of Notices - Exemptions for Restricted Development	
None specified.	

Part 3 - Overlays

Environment and Food Production Areas Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of valuable rural, landscape, environmental and food production areas from urban encroachment.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Land division undertaken in accordance with Section 7 of the <i>Planning, Development and Infrastructure Act 2016</i> .	DTS/DPF 1.1 None are applicable.

Procedural Matters (PM)

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Hazards (Bushfire - High Risk) Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development, including land division is sited and designed to minimise the threat and impact of bushfires on life and property with regard to the following risks:</p> <ul style="list-style-type: none"> (a) potential for uncontrolled bushfire events taking into account the increased frequency and intensity of bushfires as a result of climate change (b) high levels and exposure to ember attack (c) impact from burning debris (d) radiant heat (e) likelihood and direct exposure to flames from a fire front.
DO 2	Activities that increase the number of people living and working in the area or where evacuation would be difficult is sited away from areas of unacceptable bushfire risk.
DO 3	To facilitate access for emergency service vehicles to aid the protection of lives and assets from bushfire danger.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use	
PO 1.1 Development that significantly increases the potential for fire outbreak as a result of the spontaneous combustion of materials, spark generation or through the magnification and reflection of light is not located in areas of unacceptable bushfire risk.	DTS/DPF 1.1 None are applicable.
PO 1.2 Pre-schools, educational establishments, hospitals, retirement and supported accommodation are sited away from areas of unacceptable bushfire risk and locations that: <ul style="list-style-type: none"> (a) are remote from or require extended periods of travel to reach safer locations (b) don't have a safe path of travel to safer locations. 	DTS/DPF 1.2 None are applicable.
Siting	
PO 2.1 Buildings and structures are located away from areas that pose an unacceptable bushfire risk as a result of vegetation cover and type, and terrain.	DTS/DPF 2.1 None are applicable.

Built Form	
PO 3.1 Buildings and structures are designed and configured to reduce the impact of bushfire through using designs that reduce the potential for trapping burning debris against or underneath the building or structure, or between the ground and building floor level in the case of transportable buildings and buildings on stilts.	DTS/DPF 3.1 None are applicable.
PO 3.2 Extensions to buildings, outbuildings and other ancillary structures are sited and constructed using materials to minimise the threat of fire spread to residential and tourist accommodation (including boarding houses, hostels, dormitory style accommodation, student accommodation and Workers' accommodation) in the event of bushfire.	DTS/DPF 3.2 Outbuildings and other ancillary structures are sited no closer than 6m from the habitable building.
Habitable Buildings	
PO 4.1 To minimise the threat, impact and potential exposure to bushfires on life and property, residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) is sited on the flatter portion of allotments away from steep slopes.	DTS/DPF 4.1 None are applicable.
PO 4.2 Residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) is sited away from vegetated areas that pose an unacceptable bushfire risk.	DTS/DPF 4.2 Residential and tourist accommodation and habitable buildings for vulnerable communities are provided with asset protection zone(s) in accordance with (a) and (b): (a) the asset protection zone has a minimum width of at least: (i) 50 metres to unmanaged grasslands (ii) 100 metres to hazardous bushland vegetation (b) the asset protection zone is contained wholly within the allotment of the development.
PO 4.3 Residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) has a dedicated area available that: (a) is capable of accommodating a bushfire protection system comprising firefighting equipment and water supply in accordance with <i>Ministerial Building Standard MBS 008 - Designated bushfire prone areas - additional requirements</i> (b) includes the provision of an all-weather hardstand area in a location that: (i) allows fire-fighting vehicles to safely access the dedicated water supply and exit the site in a forward direction	DTS/DPF 4.3 None are applicable.

(ii) is no further than 6 metres from the dedicated water supply outlet(s) where required.	
Land Division	
<p>PO 5.1</p> <p>Land division for residential and tourist accommodation and habitable buildings for vulnerable communities (including boarding houses, hostels, dormitory style accommodation, student accommodation and workers' accommodation) is limited to those areas specifically set aside for these uses.</p>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
<p>PO 5.2</p> <p>Land division is designed and incorporates measures to minimise the danger of fire hazard to residents and occupants of buildings, and to protect buildings and property from physical damage in the event of a bushfire.</p>	<p>DTS/DPF 5.2</p> <p>None are applicable.</p>
<p>PO 5.3</p> <p>Land division is designed to provide a continuous street pattern (avoiding the use of dead end roads/cul-de-sac road design) to facilitate the safe movement and evacuation of emergency vehicles, residents, occupants and visitors. Where cul-de-sac / dead end roads are proposed, an alternative emergency evacuation route is provided.</p>	<p>DTS/DPF 5.3</p> <p>None are applicable.</p>
<p>PO 5.4</p> <p>Where 10 or more new allotments are proposed, land division includes at least two separate and safe exit points to enable multiple avenues of evacuation in the event of a bushfire.</p>	<p>DTS/DPF 5.4</p> <p>None are applicable.</p>
<p>PO 5.5</p> <p>Land division provides sufficient space for future asset protection zones and incorporates perimeter roads of adequate design in conjunction with bushfire buffer zones to achieve adequate separation between residential allotments and areas of unacceptable bushfire risk and to support safe access for the purposes of fire-fighting.</p>	<p>DTS/DPF 5.5</p> <p>None are applicable.</p>
Vehicle Access – Roads, Driveways and Fire Tracks	
<p>PO 6.1</p> <p>Roads are designed and constructed to facilitate the safe and effective:</p> <ul style="list-style-type: none"> (a) access, operation and evacuation of fire-fighting vehicles and emergency personnel (b) evacuation of residents, occupants and visitors. 	<p>DTS/DPF 6.1</p> <p>Roads:</p> <ul style="list-style-type: none"> (a) are constructed with a formed, all-weather surface (b) have a gradient of not more than 16 degrees (1-in-3.5) at any point along the road (c) have a cross fall of not more than 6 degrees (1-in-9.5) at any point along the road (d) have a minimum formed road width of 6m (e) provide overhead clearance of not less than 4.0m between the road surface and overhanging branches or other obstructions including buildings and/or structures (Figure 1) (f) allow fire-fighting services (personnel and vehicles) to travel in a continuous forward movement around road

	<p>curves by constructing the curves with a minimum external radius of 12.5m (Figure 2)</p> <p>(g) incorporating cul-de-sac endings or dead end roads are provided within an alternative evacuation route and do not exceed 200m in length and the end of the road has either:</p> <ul style="list-style-type: none"> (i) a turning area with a minimum formed surface radius of 12.5m (Figure 3) or (ii) a 'T' or 'Y' shaped turning area with a minimum formed surface length of 11m and minimum internal radii of 9.5m (Figure 4) <p>(h) incorporate solid, all-weather crossings over any watercourse that support fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes.</p>
<p>PO 6.2</p> <p>Access to habitable buildings is designed and constructed to facilitate the safe and effective:</p> <ul style="list-style-type: none"> (a) use, operation and evacuation of fire-fighting and emergency personnel (b) evacuation of residents, occupants and visitors. 	<p>DTS/DPF 6.2</p> <p>Access is in accordance with (a) or (b):</p> <ul style="list-style-type: none"> (a) a clear and unobstructed vehicle or pedestrian pathway of not greater than 60 metres in length is available between the most distant part of the habitable building and the nearest part of a formed public access road (b) driveways: <ul style="list-style-type: none"> (i) do not exceed 600m in length (ii) are constructed with a formed, all-weather surface (iii) are connected to a formed, all-weather public road with the transition area between the road and driveway having a gradient of not more than 7 degrees (1-in-8) (iv) have a gradient of not more than 16 degrees (1-in-3.5) at any point along the driveway (v) have a crossfall of not more than 6 degrees (1-in-9.5) at any point along the driveway (vi) have a minimum formed width of 3m (4m where the gradient of the driveway is steeper than 12 degrees (1-in-4.5)) plus 0.5 metres clearance either side of the driveway from overhanging branches or other obstructions, including buildings and/or structures (Figure 1) (vii) incorporate passing bays with a minimum width of 6m and length of 17m every 200m (Figure 5) (viii) provide overhead clearance of not less than 4.0m between the driveway surface and overhanging branches or other obstructions, including buildings and/or structures (Figure 1) (ix) allow fire-fighting services (personnel and vehicles) to travel in a continuous forward movement around driveway curves by constructing the curves with a minimum external radius of 12.5m (Figure 2) (x) allow fire-fighting vehicles to safely enter and exit an allotment in a forward direction by using a 'U' shaped drive through design or by incorporating at the end of the driveway either: <ul style="list-style-type: none"> A. a loop road around the building or B. a turning area with a minimum radius of 12.5m (Figure 3)

	<p>or</p> <p>C. a 'T' or 'Y' shaped turning area with a minimum formed length of 11m and minimum internal radii of 9.5m (Figure 4)</p> <p>(xi) incorporate solid, all-weather crossings over any watercourse that support fire-fighting vehicles with a gross vehicle mass (GVM) of 21 tonnes.</p>
<p>PO 6.3</p> <p>Development does not rely on fire tracks as means of evacuation or access for fire-fighting purposes unless there are no safe alternatives available.</p>	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>

Procedural Matters (PM) - Referrals

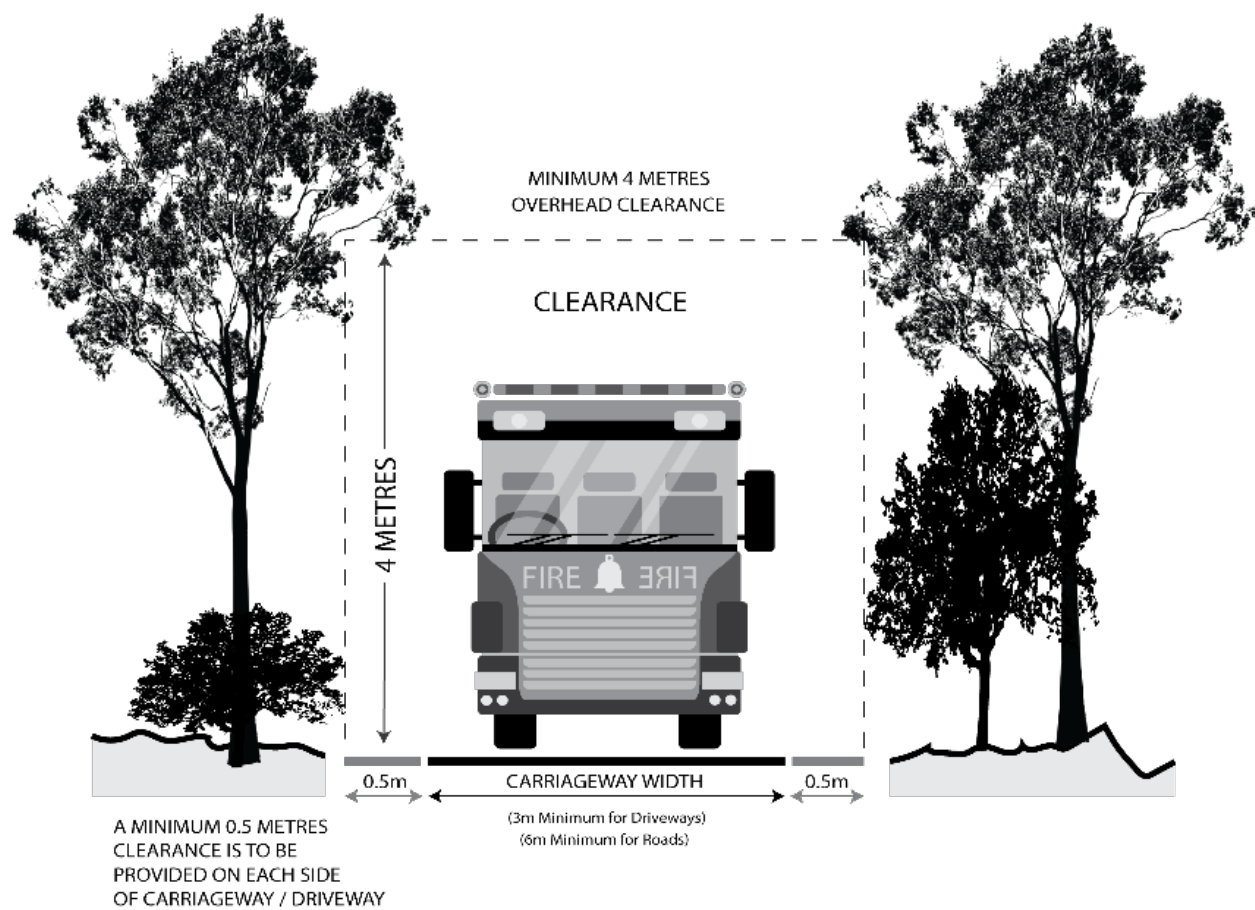
The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except if a relevant certificate accompanies the application for planning consent in respect of the development, any of the following classes of development (including alterations and additions which increase the floor area of such buildings by 10% or more):</p> <ul style="list-style-type: none"> (a) land division creating one or more additional allotments (b) dwelling (c) ancillary accommodation (d) residential flat building (e) tourist accommodation (f) boarding home (g) dormitory style accommodation (h) workers' accommodation (i) student accommodation (j) pre-school (k) educational establishment (l) retirement village (m) supported accommodation (n) residential park (o) hospital (p) camp ground. 	South Australian Country Fire Service.	To provide expert assessment and direction to the relevant authority on the potential impacts of bushfire on the development.	Development of a class to which Schedule 9 clause 3 item 2 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Figures and Diagrams

Fire Appliance Clearances

Figure 1 - Overhead and Side Clearances



Roads and Driveway Design

Figure 2 - Road and Driveway Curves

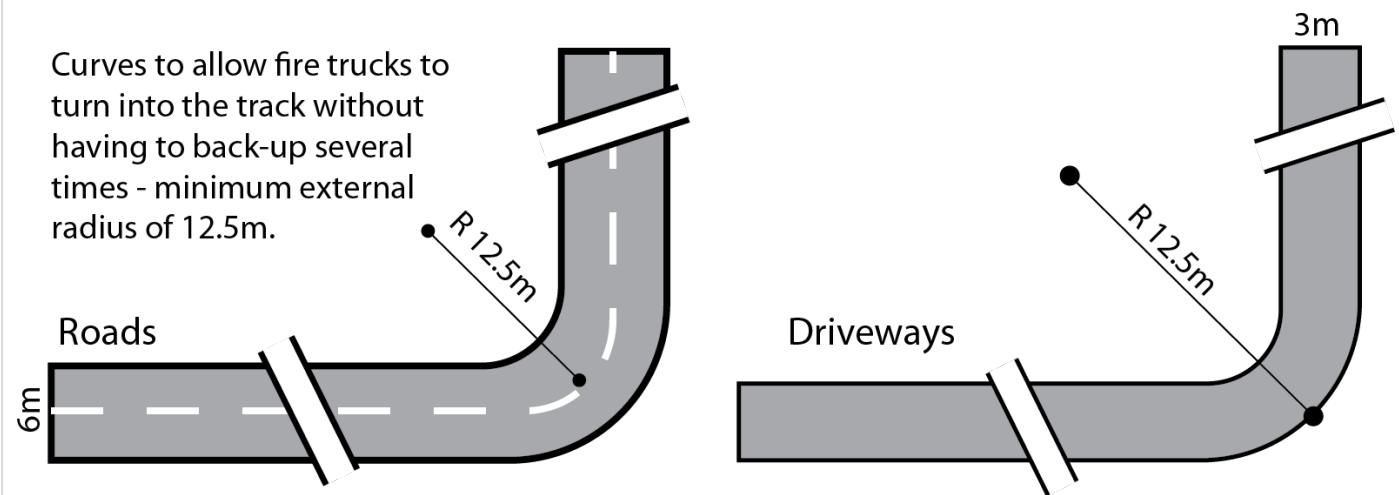


Figure 3 - Full Circle Turning Area

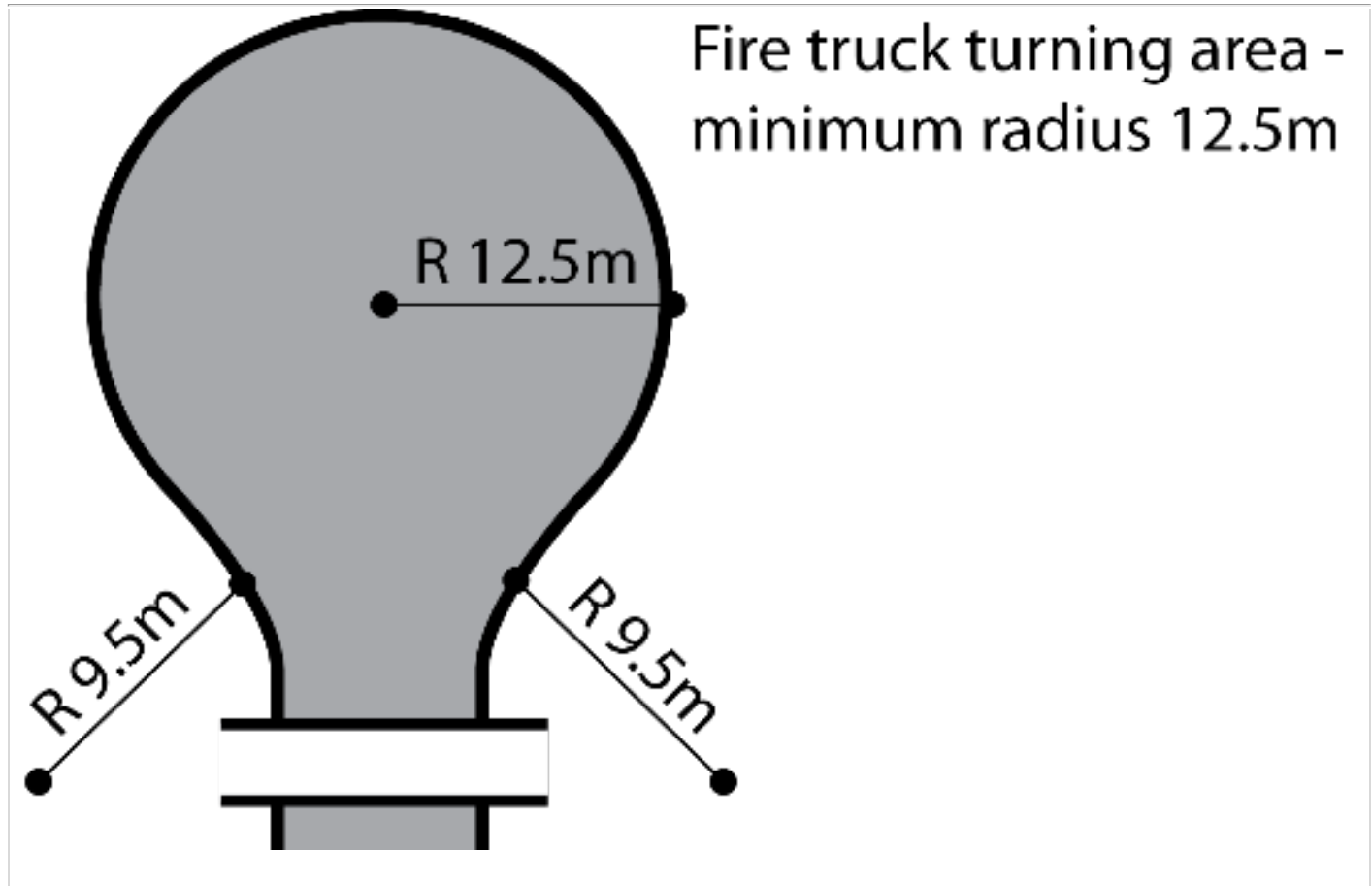
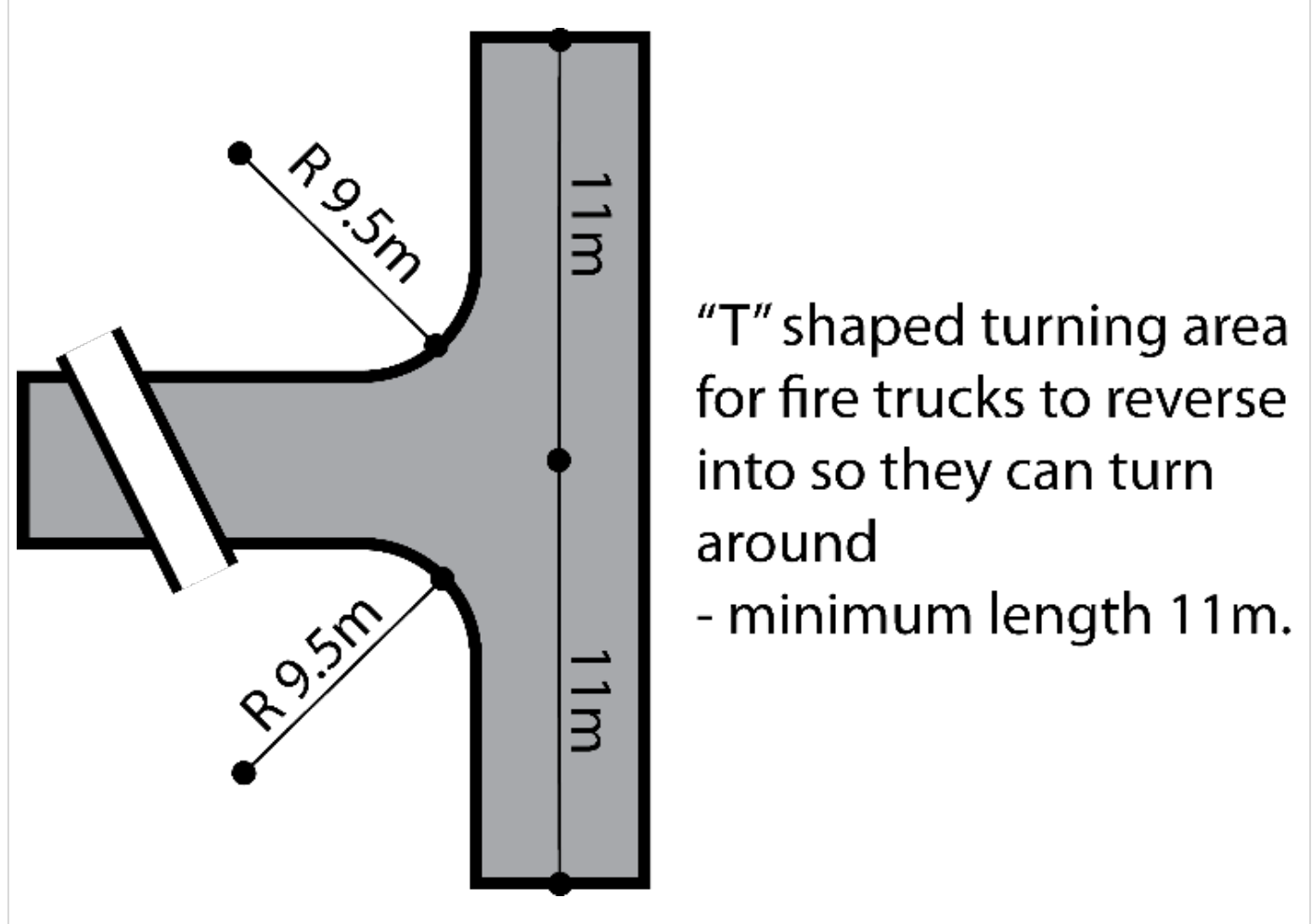


Figure 4 - 'T' or 'Y' Shaped Turning Head



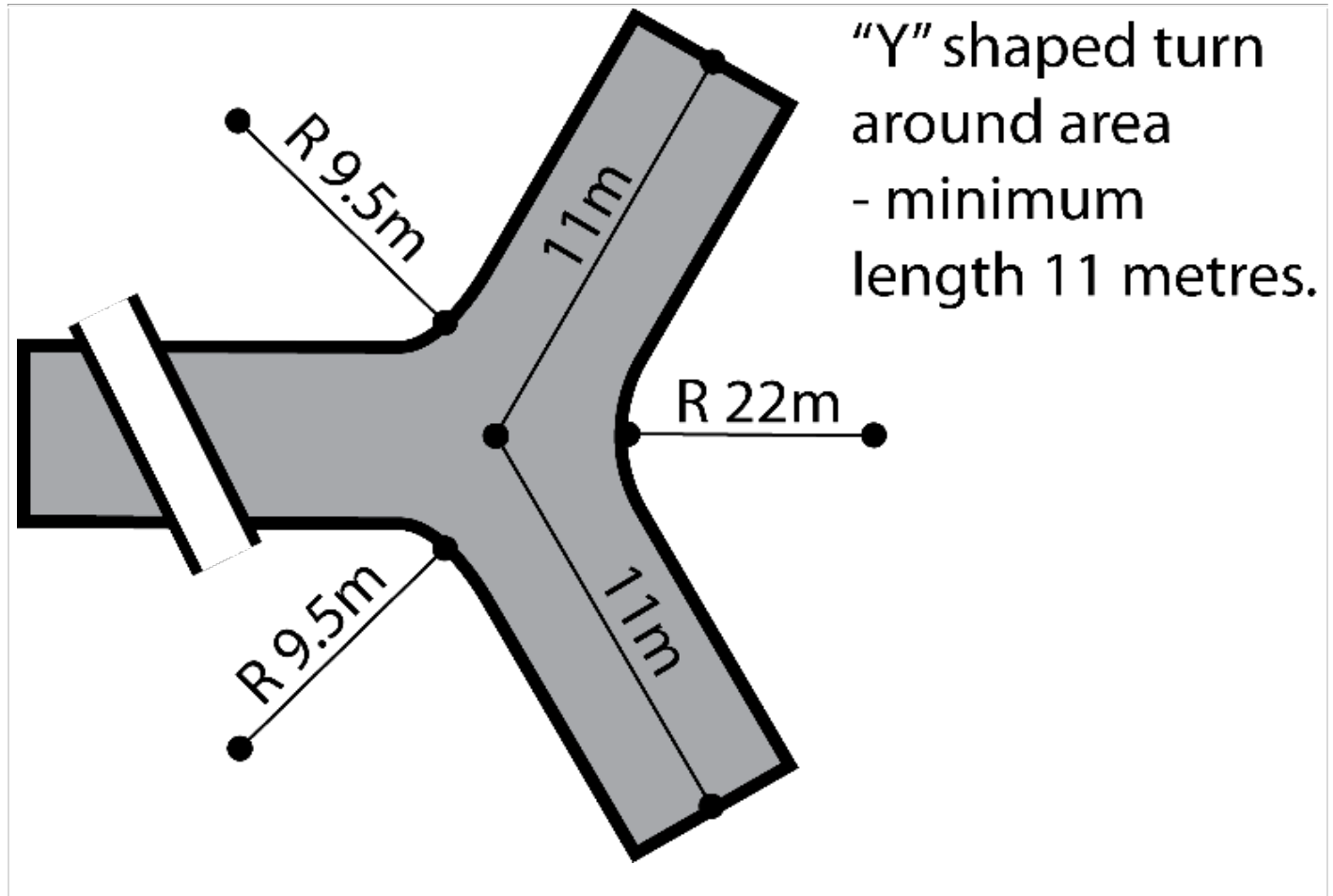
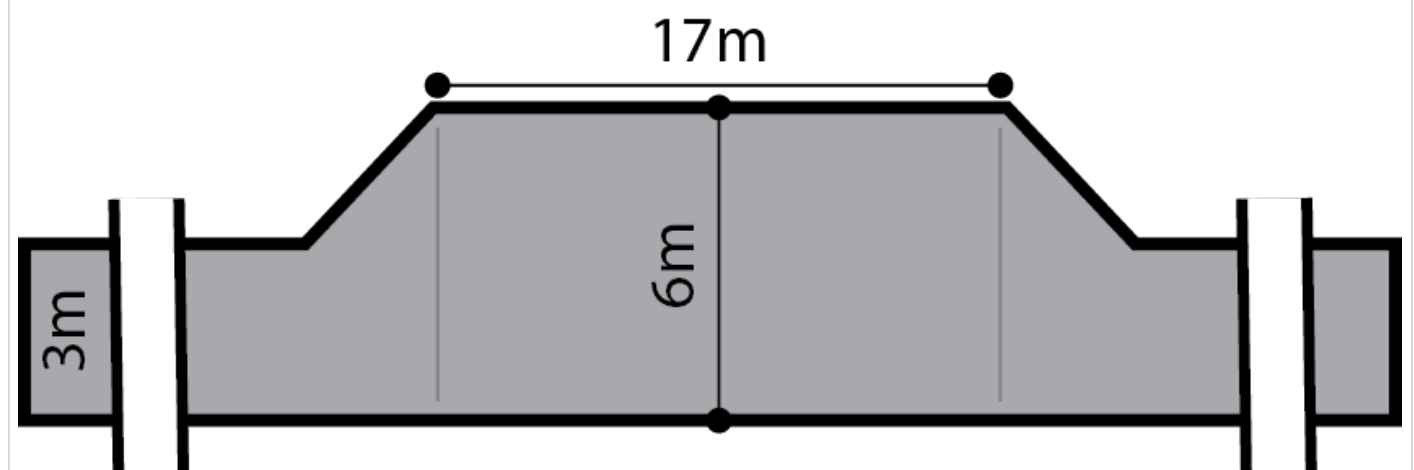


Figure 5 - Driveway Passing Bays

Passing bay for fire trucks - minimum width 6 metres, minimum length 17 metres.



Hazards (Flooding - Evidence Required) Overlay

Assessment Provisions (AP)

Desired Outcome

DO 1	Development adopts a precautionary approach to mitigate potential impacts on people, property, infrastructure and the environment from potential flood risk through the appropriate siting and design of development.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Flood Resilience	
PO 1.1 Development is sited, designed and constructed to minimise the risk of entry of potential floodwaters where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.	DTS/DPF 1.1 Habitable buildings, commercial and industrial buildings, and buildings used for animal keeping incorporate a finished floor level at least 300mm above: (a) the highest point of top of kerb of the primary street or (b) the highest point of natural ground level at the primary street boundary where there is no kerb
Environmental Protection	
PO 2.1 Buildings and structures used either partly or wholly to contain or store hazardous materials are designed to prevent spills or leaks leaving the confines of the building.	DTS/DPF 2.1 Development does not involve the storage of hazardous materials.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Heritage Adjacency Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Development adjacent to a State or Local Heritage Place does not dominate, encroach on or unduly impact on the setting of the Place.	DTS/DPF 1.1 None are applicable.
Land Division	
PO 2.1 Land division adjacent to a State or Local Heritage Place creates allotments that are of a size and dimension that enables the siting and setbacks of new buildings from allotment boundaries so that they do not dominate, encroach or unduly impact on the setting of the Place.	DTS/DPF 2.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development that may materially affect the context of a State Heritage Place.	Minister responsible for the administration of the <i>Heritage Places Act 1993</i> .	To provide expert assessment and direction to the relevant authority on the potential impacts of development adjacent State Heritage Places.	Development of a class to which Schedule 9 clause 3 item 17 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Limited Land Division Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	The long term use of land for primary production is maintained by minimising fragmentation through division of land.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Land division does not result in the creation of an additional allotment.	DTS/DPF 1.1 No additional allotments are created.
PO 1.2 Land division involving boundary realignments occurs only where the number of resulting allotments with a site area less than that specified in the relevant Zone is not greater than the number that existed prior to the realignment.	DTS/DPF 1.2 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Native Vegetation Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Areas of native vegetation are protected, retained and restored in order to sustain biodiversity, threatened species and vegetation communities, fauna habitat, ecosystem services, carbon storage and amenity values.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Environmental Protection	

<p>PO 1.1</p> <p>Development avoids, or where it cannot be practically avoided, minimises the clearance of native vegetation taking into account the siting of buildings, access points, bushfire protection measures and building maintenance.</p>	<p>DTS/DPF 1.1</p> <p>An application is accompanied by:</p> <ul style="list-style-type: none"> (a) a declaration stating that the proposal will not, or would not, involve clearance of native vegetation under the Native Vegetation Act 1991, including any clearance that may occur: <ul style="list-style-type: none"> (i) in connection with a relevant access point and / or driveway (ii) within 10m of a building (other than a residential building or tourist accommodation) (iii) within 20m of a dwelling or addition to an existing dwelling for fire prevention and control (iv) within 50m of residential or tourist accommodation in connection with a requirement under a relevant overlay to establish an asset protection zone in a bushfire prone area or (b) a report prepared in accordance with Regulation 18(2) (a) of the Native Vegetation Regulations 2017 that establishes that the clearance is categorised as 'Level 1 clearance'.
<p>PO 1.2</p> <p>Native vegetation clearance in association with development avoids the following:</p> <ul style="list-style-type: none"> (a) significant wildlife habitat and movement corridors (b) rare, vulnerable or endangered plants species (c) native vegetation that is significant because it is located in an area which has been extensively cleared (d) native vegetation that is growing in, or in association with, a wetland environment. 	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Intensive animal husbandry and agricultural activities are sited, set back and designed to minimise impacts on native vegetation, including impacts on native vegetation in an adjacent State Significant Native Vegetation Area, from:</p> <ul style="list-style-type: none"> (a) the spread of pest plants and phytophthora (b) the spread of non-indigenous plants species (c) excessive nutrient loading of the soil or loading arising from surface water runoff (d) soil compaction (e) chemical spray drift. 	<p>DTS/DPF 1.3</p> <p>Development within 500 metres of a boundary of a State Significant Native Vegetation Area does not involve any of the following:</p> <ul style="list-style-type: none"> (a) horticulture (b) intensive animal husbandry (c) dairy (d) commercial forestry (e) aquaculture.
<p>PO 1.4</p> <p>Development restores and enhances biodiversity and habitat values through revegetation using locally indigenous plant species.</p>	<p>DTS/DPF 1.4</p> <p>None are applicable.</p>
Land division	
<p>PO 2.1</p> <p>Land division does not result in the fragmentation of land</p>	<p>DTS/DPF 2.1</p> <p>Land division where:</p>

containing native vegetation, or necessitate the clearance of native vegetation, unless such clearance is considered minor, taking into account the location of allotment boundaries, access ways, fire breaks, boundary fencing and potential building siting or the like.	<p>(a) an application is accompanied by one of the following:</p> <ul style="list-style-type: none"> (i) a declaration stating that none of the allotments in the proposed plan of division contain native vegetation under the <i>Native Vegetation Act 1991</i> (ii) a declaration stating that no native vegetation clearance under the <i>Native Vegetation Act 1991</i> will be required as a result of the division of land (iii) a report prepared in accordance with Regulation 18(2)(a) of the Native Vegetation Regulations 2017 that establishes that the vegetation to be cleared is categorised as 'Level 1 clearance' <p>or</p> <p>(b) an application for land division which is being considered concurrently with a proposal to develop each allotment which will satisfy, or would satisfy, the requirements of DTS/DPF 1.1, including any clearance that may occur</p> <p>or</p> <p>(c) the division is to support a Heritage Agreement under the Native Vegetation Act 1991 or the <i>Heritage Places Act 1993</i>.</p>
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Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development that is the subject of a report prepared in accordance with Regulation 18(2)(a) of the <i>Native Vegetation Regulations 2017</i> that categorises the clearance, or potential clearance, as 'Level 3 clearance' or 'Level 4 clearance'.	Native Vegetation Council	To provide expert assessment and direction to the relevant authority on the potential impacts of development on native vegetation.	Development of a class to which Schedule 9 clause 3 item 11 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Prescribed Wells Area Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Sustainable water use in prescribed wells areas.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>All development, but in particular involving any of the following:</p> <ul style="list-style-type: none"> (a) horticulture (b) activities requiring irrigation (c) aquaculture (d) industry (e) intensive animal husbandry (f) commercial forestry <p>has a lawful, sustainable and reliable water supply that does not place undue strain on water resources in prescribed wells areas.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies either of the following:</p> <ul style="list-style-type: none"> (a) the applicant has a current water licence in which sufficient spare capacity exists to accommodate the water needs of the proposed use or (b) the proposal does not involve the taking of water for which a licence would be required under the <i>Landscape South Australia Act 2019</i>.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Any of the following classes of development that require or may require water to be taken in addition to any allocation that has already been granted under the <i>Landscape South Australia Act 2019</i>:</p> <ul style="list-style-type: none"> (a) horticulture (b) activities requiring irrigation (c) aquaculture (d) industry (e) intensive animal husbandry (f) commercial forestry. 	<p>The Chief Executive of the Department of the Minister responsible for the administration of the <i>Landscape South Australia Act 2019</i>.</p>	<p>To provide expert technical assessment and direction to the relevant authority on the taking of water to ensure development is undertaken sustainably.</p>	<p>Development of a class to which Schedule 9 clause 3 item 13 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>
<p>Commercial forestry that requires a forest water licence under Part 8 Division 6 of the <i>Landscape South Australia Act 2019</i>.</p>			

Traffic Generating Development Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes and Major Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from urban transport routes and major urban transport routes.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Traffic Generating Development	
<p>PO 1.1</p> <p>Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.</p>	<p>DTS/DPF 1.1</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more.
<p>PO 1.2</p> <p>Access points sited and designed to accommodate the type and volume of traffic likely to be generated by development.</p>	<p>DTS/DPF 1.2</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more.
<p>PO 1.3</p> <p>Sufficient accessible on-site queuing provided to meet the needs of the development so that queues do not impact on the State Maintained Road network.</p>	<p>DTS/DPF 1.3</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more

	<ul style="list-style-type: none"> (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more.
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Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, any of the following classes of development that are proposed within 250m of a State Maintained Road:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Urban Transport Routes Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance	Deemed-to-Satisfy Criteria / Designated
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Outcome	Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
<p>PO 1.1</p> <p>Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.</p>	<p>DTS/DPF 1.1</p> <p>An access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where servicing a single (1) dwelling / residential allotment: <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 3m and 4m (measured at the site boundary) (b) where the development will result in 2 and up to 6 dwellings: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site) (c) where the development will result in 7 or more dwellings, or is a non-residential land use: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the road and B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.

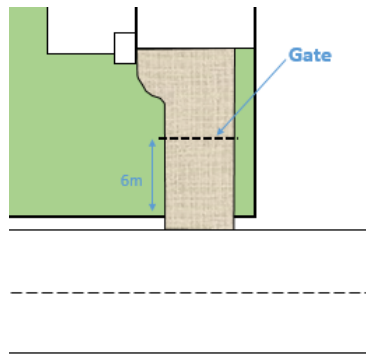
PO 2.1

Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional performance of the road and maintain safe vehicle movements.

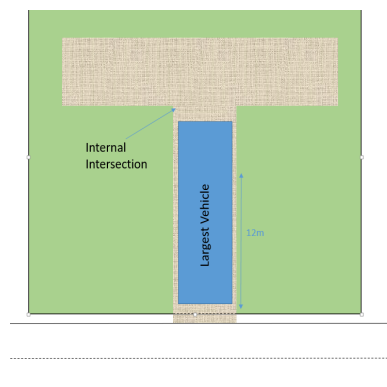
DTS/DPF 2.1

An access point in accordance with one of the following:

- (a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram:



- (b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
- (i) is expected to be serviced by vehicles with a length no greater than 6.4m
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
- (c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
- (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
 - (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop
 - (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram:



Access - (Location Spacing) - Existing Access Point

<p>PO 3.1</p> <p>Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.</p>	<p>DTS/DPF 3.1</p> <p>An existing access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) it will not service, or is not intended to service, more than 6 dwellings (b) it is not located on a Controlled Access Road and will not service development that will result in (b) a larger class of vehicle expected to access the site using the existing access (c) is not located on a Controlled Access Road and development constitutes: <ul style="list-style-type: none"> (i) a change of use between an office <500m² gross leasable floor area and a consulting room <500m² gross leasable floor area or vice versa (ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment (iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area (iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area (v) an office or consulting room with a <500m² gross leasable floor area.
Access – Location (Spacing) – New Access Points	
<p>PO 4.1</p> <p>New access points are spaced apart from any existing access point or public road junction to manage impediments to traffic flow and maintain safe and efficient operating conditions on the road.</p>	<p>DTS/DPF 4.1</p> <p>A new access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where a development site is intended to serve between 1 and 6 dwellings and has frontage to a local road (not being a Controlled Access Road) with a speed environment of 60km/h or less, the new access point is provided on the local road and located a minimum of 6.0m from the tangent point as shown in the following diagram: <div data-bbox="790 1153 1332 1601" data-label="Diagram"> <p style="text-align: center;">TP = Tangent point</p> </div> <p>NOTE: The points marked X_1 and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point Y_1.</p> (b) where the development site is intended to serve between 1 and 6 dwellings and access from a local road (being a road that is not a State Maintained Road) is not available, the new access: <ul style="list-style-type: none"> (i) is not located on a Controlled Access Road (ii) is not located on a section of road affected by double barrier lines (iii) will be on a road with a speed environment of 70km/h or less (iv) is located outside of the bold lines on the diagram shown in the diagram following part (a) (v) located minimum of 6m from a median opening or pedestrian crossing (c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an

alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
50 km/h or less	No spacing requirement	20m
60 km/h	30m	73m
70 km/h	40m	92m
80 km/h	50m	114m
90 km/h	65m	139m
100 km/h	80m	165m
110 km/h	100m	193m

Access - Location (Sight Lines)

PO 5.1

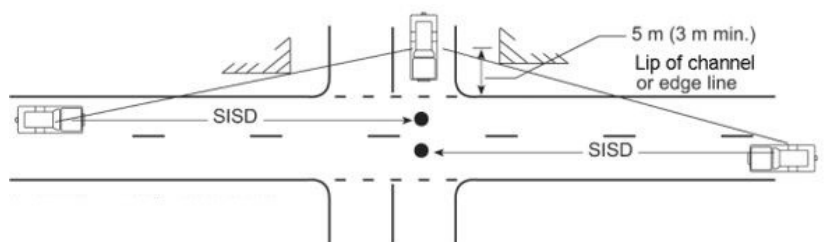
Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.

DTS/DPF 5.1

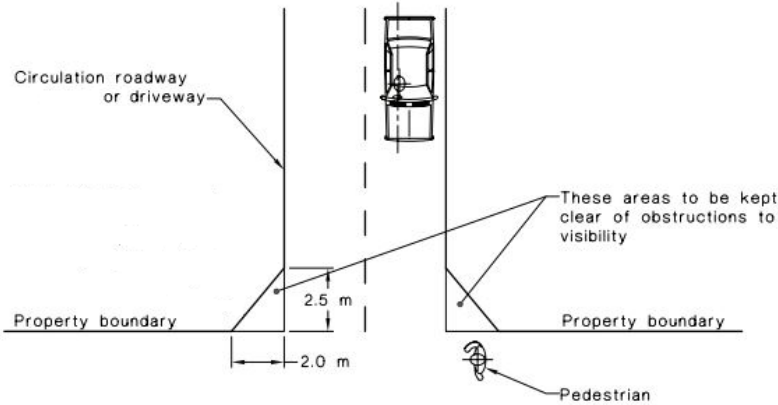
An access point satisfies (a) or (b):

- (a) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):

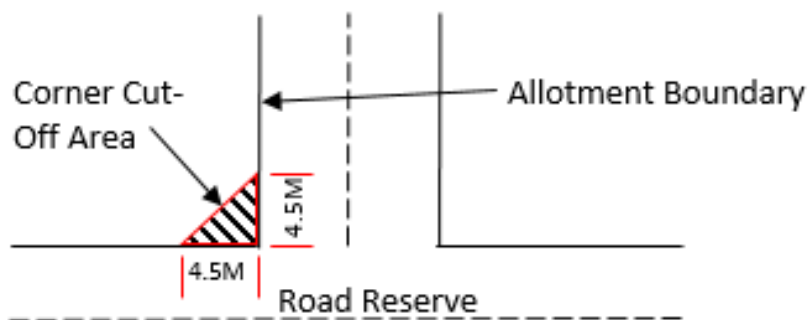
Speed Limit	Access point serving 1-6 dwellings	Access point serving all other development
40 km/h or less	40m	73m
50 km/h	55m	97m
60 km/h	73m	123m
70 km/h	92m	151m
80 km/h	114m	181m
90 km/h	139m	214m
100 km/h	165m	248m
110km/h	193m	285m



- (b) pedestrian sightlines in accordance with the following diagram:

	
Access – Mud and Debris	
<p>PO 6.1</p> <p>Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.</p>	<p>DTS/DPF 6.1</p> <p>Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).</p>
Access - Stormwater	
<p>PO 7.1</p> <p>Access points are designed to minimise negative impact on roadside drainage of water.</p>	<p>DTS/DPF 7.1</p> <p>Development does not:</p> <ul style="list-style-type: none"> (a) decrease the capacity of an existing drainage point (b) restrict or prevent the flow of stormwater through an existing drainage point and system.
Building on Road Reserve	
<p>PO 8.1</p> <p>Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe movements by all road users.</p>	<p>DTS/DPF 8.1</p> <p>Buildings or structures are not located on, above or below the road reserve.</p>
Public Road Junctions	
<p>PO 9.1</p> <p>New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.</p>	<p>DTS/DPF 9.1</p> <p>Development does not comprise any of the following:</p> <ul style="list-style-type: none"> (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
Corner Cut-Offs	
<p>PO 10.1</p> <p>Development is located and designed to maintain sightlines for drivers turning into and out of public road junctions to</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram:</p>

contribute to driver safety.



Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Part 4 - General Development Policies

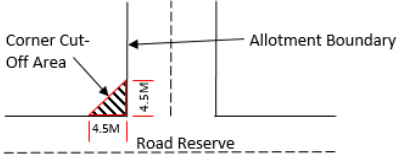
Advertisements

Assessment Provisions (AP)

Desired Outcome	
DO 1	Advertisements and advertising hoardings are appropriate to context, efficient and effective in communicating with the public, limited in number to avoid clutter, and do not create hazard.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Appearance	
<p>PO 1.1</p> <p>Advertisements are compatible and integrated with the design of the building and/or land they are located on.</p>	<p>DTS/DPF 1.1</p> <p>Advertisements attached to a building satisfy all of the following:</p> <ul style="list-style-type: none"> (a) are not located in a Neighbourhood-type zone (b) where they are flush with a wall: <ul style="list-style-type: none"> (i) if located at canopy level, are in the form of a fascia sign (ii) if located above canopy level: <ul style="list-style-type: none"> A. do not have any part rising above parapet height B. are not attached to the roof of the building (c) where they are not flush with a wall: <ul style="list-style-type: none"> (i) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure (ii) if attached to a two-storey building: <ul style="list-style-type: none"> A. has no part located above the finished floor level of the second storey of the building B. does not protrude beyond the outer limits of any verandah structure below C. does not have a sign face that exceeds 1m² per side. (d) if located below canopy level, are flush with a wall (e) if located at canopy level, are in the form of a fascia sign (f) if located above a canopy: <ul style="list-style-type: none"> (i) are flush with a wall (ii) do not have any part rising above parapet height (iii) are not attached to the roof of the building. (g) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure (h) if attached to a two-storey building, have no part located above the finished floor level of the second storey of the building (i) where they are flush with a wall, do not, in combination with any other existing sign, cover more than 15% of the building facade to which they are attached.
<p>PO 1.2</p>	<p>DTS/DPF 1.2</p>

Advertising hoardings do not disfigure the appearance of the land upon which they are situated or the character of the locality.	Where development comprises an advertising hoarding, the supporting structure is: <ul style="list-style-type: none"> (a) concealed by the associated advertisement and decorative detailing or (b) not visible from an adjacent public street or thoroughfare, other than a support structure in the form of a single or dual post design.
PO 1.3 Advertising does not encroach on public land or the land of an adjacent allotment.	DTS/DPF 1.3 Advertisements and/or advertising hoardings are contained within the boundaries of the site.
PO 1.4 Where possible, advertisements on public land are integrated with existing structures and infrastructure.	DTS/DPF 1.4 Advertisements on public land that meet at least one of the following: <ul style="list-style-type: none"> (a) achieves Advertisements DTS/DPF 1.1 (b) are integrated with a bus shelter.
PO 1.5 Advertisements and/or advertising hoardings are of a scale and size appropriate to the character of the locality.	DTS/DPF 1.5 None are applicable.
Proliferation of Advertisements	
PO 2.1 Proliferation of advertisements is minimised to avoid visual clutter and untidiness.	DTS/DPF 2.1 No more than one freestanding advertisement is displayed per occupancy.
PO 2.2 Multiple business or activity advertisements are co-located and coordinated to avoid visual clutter and untidiness.	DTS/DPF 2.2 Advertising of a multiple business or activity complex is located on a single advertisement fixture or structure.
PO 2.3 Proliferation of advertisements attached to buildings is minimised to avoid visual clutter and untidiness.	DTS/DPF 2.3 Advertisements satisfy all of the following: <ul style="list-style-type: none"> (a) are attached to a building (b) other than in a Neighbourhood-type zone, where they are flush with a wall, cover no more than 15% of the building facade to which they are attached (c) do not result in more than one sign per occupancy that is not flush with a wall.
Advertising Content	
PO 3.1 Advertisements are limited to information relating to the lawful use of land they are located on to assist in the ready identification of the activity or activities on the land and avoid unrelated content that contributes to visual clutter and untidiness.	DTS/DPF 3.1 Advertisements contain information limited to a lawful existing or proposed activity or activities on the same site as the advertisement.
Amenity Impacts	
PO 4.1	DTS/DPF 4.1

Light spill from advertisement illumination does not unreasonably compromise the amenity of sensitive receivers.	Advertisements do not incorporate any illumination.
Safety	
PO 5.1 Advertisements and/or advertising hoardings erected on a verandah or projecting from a building wall are designed and located to allow for safe and convenient pedestrian access.	DTS/DPF 5.1 Advertisements have a minimum clearance of 2.5m between the top of the footpath and base of the underside of the sign.
PO 5.2 Advertisements and/or advertising hoardings do not distract or create a hazard to drivers through excessive illumination.	DTS/DPF 5.2 No advertisement illumination is proposed.
PO 5.3 Advertisements and/or advertising hoardings do not create a hazard to drivers by: (a) being liable to interpretation by drivers as an official traffic sign or signal (b) obscuring or impairing drivers' view of official traffic signs or signals (c) obscuring or impairing drivers' view of features of a road that are potentially hazardous (such as junctions, bends, changes in width and traffic control devices) or other road or rail vehicles at/or approaching level crossings.	DTS/DPF 5.3 Advertisements satisfy all of the following: (a) are not located in a public road or rail reserve (b) are located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram 
PO 5.4 Advertisements and/or advertising hoardings do not create a hazard by distracting drivers from the primary driving task at a location where the demands on driver concentration are high.	DTS/DPF 5.4 Advertisements and/or advertising hoardings are not located along or adjacent to a road having a speed limit of 80km/h or more.
PO 5.5 Advertisements and/or advertising hoardings provide sufficient clearance from the road carriageway to allow for safe and convenient movement by all road users.	DTS/DPF 5.5 Where the advertisement or advertising hoarding is: (a) on a kerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 0.6m from the roadside edge of the kerb (b) on an unkerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 5.5m from the edge of the seal (c) on any other kerbed or unkerbed road, the advertisement or advertising hoarding is located a minimum of the following distance from the roadside edge of the kerb or the seal: (a) 110 km/h road - 14m (b) 100 km/h road - 13m (c) 90 km/h road - 10m (d) 70 or 80 km/h road - 8.5m.
PO 5.6 Advertising near signalised intersections does not cause unreasonable distraction to road users through illumination, flashing lights, or moving or changing displays or messages.	DTS/DPF 5.6 Advertising: (a) is not illuminated (b) does not incorporate a moving or changing display or

	<p>message</p> <p>(c) does not incorporate a flashing light(s).</p>
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Animal Keeping and Horse Keeping

Assessment Provisions (AP)

Desired Outcome	
DO 1	Animals are kept at a density that is not beyond the carrying capacity of the land and in a manner that minimises their adverse effects on the environment, local amenity and surrounding development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
PO 1.1 Animal keeping, horse keeping and associated activities do not create adverse impacts on the environment or the amenity of the locality.	DTS/DPF 1.1 None are applicable.
PO 1.2 Animal keeping and horse keeping is located and managed to minimise the potential transmission of disease to other operations where animals are kept.	DTS/DPF 1.2 None are applicable.
Horse Keeping	
PO 2.1 Water from stable wash-down areas is directed to appropriate absorption areas and/or drainage pits to minimise pollution of land and water.	DTS/DPF 2.1 None are applicable.
PO 2.2 Stables, horse shelters or associated yards are sited appropriate distances away from sensitive receivers and/or allotments in other ownership to avoid adverse impacts from dust, erosion and odour.	DTS/DPF 2.2 Stables, horse shelters and associated yards are sited in accordance with all of the following: <ul style="list-style-type: none"> (a) 30m or more from any sensitive receivers (existing or approved) on land in other ownership (b) where an adjacent allotment is vacant and in other ownership, 30m or more from the boundary of that allotment.
PO 2.3	DTS/DPF 2.3

All areas accessible to horses are separated from septic tank effluent disposal areas to protect the integrity of that system. Stable flooring is constructed with an impervious material to facilitate regular cleaning.	Septic tank effluent disposal areas are enclosed with a horse-proof barrier such as a fence to exclude horses from this area.
PO 2.4 To minimise environmental harm and adverse impacts on water resources, stables, horse shelters and associated yards are appropriately set back from a watercourse.	DTS/DPF 2.4 Stables, horse shelters and associated yards are set back 50m or more from a watercourse.
PO 2.5 Stables, horse shelters and associated yards are located on slopes that are stable to minimise the risk of soil erosion and water runoff.	DTS/DPF 2.5 Stables, horse shelters and associated yards are not located on land with a slope greater than 10% (1-in-10).
Kennels	
PO 3.1 Kennel flooring is constructed with an impervious material to facilitate regular cleaning.	DTS/DPF 3.1 The floors of kennels satisfy all of the following: (a) are constructed of impervious concrete (b) are designed to be self-draining when washed down.
PO 3.2 Kennels and exercise yards are designed and sited to minimise noise nuisance to neighbours through measures such as: (a) adopting appropriate separation distances (b) orientating openings away from sensitive receivers.	DTS/DPF 3.2 Kennels are sited 500m or more from the nearest sensitive receiver on land in other ownership.
PO 3.3 Dogs are regularly observed and managed to minimise nuisance impact on adjoining sensitive receivers from animal behaviour.	DTS/DPF 3.3 Kennels are sited in association with a permanent dwelling on the land.
Wastes	
PO 4.1 Storage of manure, used litter and other wastes (other than wastewater lagoons) is designed, constructed and managed to minimise attracting and harbouring vermin.	DTS/DPF 4.1 None are applicable.
PO 4.2 Facilities for the storage of manure, used litter and other wastes (other than wastewater lagoons) are located to minimise the potential for polluting water resources.	DTS/DPF 4.2 Waste storage facilities (other than wastewater lagoons) are located outside the 1% AEP flood event areas.

Aquaculture

Assessment Provisions (AP)

Desired Outcome

DO 1	Aquaculture facilities are developed in an ecologically, economically and socially sustainable manner to support an equitable sharing of marine, coastal and inland resources and mitigate conflict with other water-based and land-based uses.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land-based Aquaculture	
<p>PO 1.1</p> <p>Land-based aquaculture and associated components are sited and designed to mitigate adverse impacts on nearby sensitive receivers.</p>	<p>DTS/DPF 1.1</p> <p>Land-based aquaculture and associated components are located to satisfy all of the following:</p> <ul style="list-style-type: none"> (a) 200m or more from a sensitive receiver in other ownership (b) 500m or more from the boundary of a zone primarily intended to accommodate sensitive receivers.
<p>PO 1.2</p> <p>Land-based aquaculture and associated components are sited and designed to prevent surface flows from entering ponds in a 1% AEP sea flood level event.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Land-based aquaculture and associated components are sited and designed to prevent pond leakage that would pollute groundwater.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Land-based aquaculture and associated components are sited and designed to prevent farmed species escaping and entering into any waters.</p>	<p>DTS/DPF 1.4</p> <p>None are applicable.</p>
<p>PO 1.5</p> <p>Land-based aquaculture and associated components, including intake and discharge pipes, are designed to minimise the need to traverse sensitive areas to minimise impact on the natural environment.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
<p>PO 1.6</p> <p>Pipe inlets and outlets associated with land-based aquaculture are sited and designed to minimise the risk of disease transmission.</p>	<p>DTS/DPF 1.6</p> <p>None are applicable.</p>
<p>PO 1.7</p> <p>Storage areas associated with aquaculture activity are integrated with the use of the land and sited and designed to minimise their visual impact on the surrounding environment.</p>	<p>DTS/DPF 1.7</p> <p>None are applicable.</p>

Marine Based Aquaculture	
<p>PO 2.1</p> <p>Marine aquaculture is sited and designed to minimise its adverse impacts on sensitive ecological areas including:</p> <ul style="list-style-type: none"> (a) creeks and estuaries (b) wetlands (c) significant seagrass and mangrove communities (d) marine habitats and ecosystems. 	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Marine aquaculture is sited in areas with adequate water current to disperse sediments and dissolve particulate wastes to prevent the build-up of waste that may cause environmental harm.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Marine aquaculture is designed to not involve discharge of human waste on the site, on any adjacent land or into nearby waters.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Marine aquaculture (other than inter-tidal aquaculture) is located an appropriate distance seaward of the high water mark.</p>	<p>DTS/DPF 2.4</p> <p>Marine aquaculture development is located 100m or more seaward of the high water mark.</p>
<p>PO 2.5</p> <p>Marine aquaculture is sited and designed to not obstruct or interfere with:</p> <ul style="list-style-type: none"> (a) areas of high public use (b) areas, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports (c) areas of outstanding visual or environmental value (d) areas of high tourism value (e) areas of important regional or state economic activity, including commercial ports, wharfs and jetties (f) the operation of infrastructure facilities including inlet and outlet pipes associated with the desalination of sea water. 	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
<p>PO 2.6</p> <p>Marine aquaculture is sited and designed to minimise interference and obstruction to the natural processes of the coastal and marine environment.</p>	<p>DTS/DPF 2.6</p> <p>None are applicable.</p>
<p>PO 2.7</p> <p>Marine aquaculture is designed to be as unobtrusive as practicable by incorporating measures such as:</p> <ul style="list-style-type: none"> (a) using feed hoppers painted in subdued colours and suspending them as close as possible to the surface of the water 	<p>DTS/DPF 2.7</p> <p>None are applicable.</p>

<p>(b) positioning structures to protrude the minimum distance practicable above the surface of the water</p> <p>(c) avoiding the use of shelters and structures above cages and platforms unless necessary to exclude predators and protected species from interacting with the farming structures and/or stock inside the cages, or for safety reasons</p> <p>(d) positioning racks, floats and other farm structures in unobtrusive locations landward from the shoreline.</p>	
<p>PO 2.8</p> <p>Access, launching and maintenance facilities utilise existing established roads, tracks, ramps and paths to or from the sea where possible to minimise environmental and amenity impacts.</p>	<p>DTS/DPF 2.8</p> <p>None are applicable.</p>
<p>PO 2.9</p> <p>Access, launching and maintenance facilities are developed as common user facilities and are co-located where practicable to mitigate adverse impacts on coastal areas.</p>	<p>DTS/DPF 2.9</p> <p>None are applicable.</p>
<p>PO 2.10</p> <p>Marine aquaculture is sited to minimise potential impacts on, and to protect the integrity of, reserves under the <i>National Parks and Wildlife Act 1972</i>.</p>	<p>DTS/DPF 2.10</p> <p>Marine aquaculture is located 1000m or more seaward of the boundary of any reserve under the <i>National Parks and Wildlife Act 1972</i>.</p>
<p>PO 2.11</p> <p>Onshore storage, cooling and processing facilities do not impair the coastline and its visual amenity by:</p> <p>(a) being sited, designed, landscaped and of a scale to reduce the overall bulk and appearance of buildings and complement the coastal landscape</p> <p>(b) making provision for appropriately sited and designed vehicular access arrangements, including using existing vehicular access arrangements as far as practicable</p> <p>(c) incorporating appropriate waste treatment and disposal.</p>	<p>DTS/DPF 2.11</p> <p>None are applicable.</p>
Navigation and Safety	
<p>PO 3.1</p> <p>Marine aquaculture sites are suitably marked to maintain navigational safety.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Marine aquaculture is sited to provide adequate separation between farms for safe navigation.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
Environmental Management	
<p>PO 4.1</p> <p>Marine aquaculture is maintained to prevent hazards to people and wildlife, including breeding grounds and habitats of native marine mammals and terrestrial fauna, especially migratory species.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p>	<p>DTS/DPF 4.2</p>

Marine aquaculture is designed to facilitate the relocation or removal of structures in the case of emergency such as oil spills, algal blooms and altered water flows.	None are applicable.
PO 4.3 Marine aquaculture provides for progressive or future reclamation of disturbed areas ahead of, or upon, decommissioning.	DTS/DPF 4.3 None are applicable.
PO 4.4 Aquaculture operations incorporate measures for the removal and disposal of litter, disused material, shells, debris, detritus, dead animals and animal waste to prevent pollution of waters, wetlands, or the nearby coastline.	DTS/DPF 4.4 None are applicable.

Beverage Production in Rural Areas

Assessment Provisions (AP)

Desired Outcome	
DO 1	Mitigation of potential amenity and environmental impacts of value-adding beverage production facilities such as wineries, distilleries, cideries and breweries.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Odour and Noise	
PO 1.1 Beverage production activities are designed and sited to minimise odour impacts on rural amenity.	DTS/DPF 1.1 None are applicable.
PO 1.2 Beverage production activities are designed and sited to minimise noise impacts on sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Fermentation, distillation, manufacturing, storage, packaging and bottling activities occur within enclosed buildings to improve the visual appearance within a locality and manage noise associated with these activities.	DTS/DPF 1.3 None are applicable.

PO 1.4 Breweries are designed to minimise odours emitted during boiling and fermentation stages of production.	DTS/DPF 1.4 Brew kettles are fitted with a vapour condenser.
PO 1.5 Beverage production solid wastes are stored in a manner that minimises odour impacts on sensitive receivers in other ownership.	DTS/DPF 1.5 Solid waste from beverage production is collected and stored in sealed containers and removed from the site within 48 hours.
Water Quality	
PO 2.1 Beverage production wastewater management systems (including wastewater irrigation) are set back from watercourses to minimise adverse impacts on water resources.	DTS/DPF 2.1 Wastewater management systems are set back 50m or more from the banks of watercourses and bores.
PO 2.2 The storage or disposal of chemicals or hazardous substances is undertaken in a manner to prevent pollution of water resources.	DTS/DPF 2.2 None are applicable.
PO 2.3 Stormwater runoff from areas that may cause contamination due to beverage production activities (including vehicle movements and machinery operations) is drained to an onsite stormwater treatment system to manage potential environmental impacts.	DTS/DPF 2.3 None are applicable.
PO 2.4 Stormwater runoff from areas unlikely to cause contamination by beverage production and associated activities (such as roof catchments and clean hard-paved surfaces) is diverted away from beverage production areas and wastewater management systems.	DTS/DPF 2.4 None are applicable.
Wastewater Irrigation	
PO 3.1 Beverage production wastewater irrigation systems are designed and located to not contaminate soil and surface and ground water resources or damage crops.	DTS/DPF 3.1 None are applicable.
PO 3.2 Beverage production wastewater irrigation systems are designed and located to minimise impact on amenity and avoid spray drift onto adjoining land.	DTS/DPF 3.2 Beverage production wastewater is not irrigated within 50m of any dwelling in other ownership.
PO 3.3 Beverage production wastewater is not irrigated onto areas that pose an undue risk to the environment or amenity such as: (a) waterlogged areas (b) land within 50m of a creek, swamp or domestic or stock water bore (c) land subject to flooding (d) steeply sloping land (e) rocky or highly permeable soil overlaying an unconfined	DTS/DPF 3.3 None are applicable.

aquifer.

Bulk Handling and Storage Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Facilities for the bulk handling and storage of agricultural, mineral, petroleum, rock, ore or other similar commodities are designed to minimise adverse impacts on transport networks, the landscape and surrounding land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
PO 1.1 Bulk handling and storage facilities are sited and designed to minimise risks of adverse air quality and noise impacts on sensitive receivers.	DTS/DPF 1.1 Facilities for the handling, storage and dispatch of commodities in bulk (excluding processing) meet the following minimum separation distances from sensitive receivers: <ul style="list-style-type: none"> (a) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals), where the handling of these materials into or from vessels does not exceed 100 tonnes per day: 300m or more from residential premises not associated with the facility (b) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility: 300m or more from residential premises not associated with the facility (c) bulk petroleum storage involving individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1,000 cubic metres: 500m or more (d) coal handling with: <ul style="list-style-type: none"> a. capacity up to 1 tonne per day or a storage capacity up to 50 tonnes: 500m or more b. capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes: 1000m or more.
Buffers and Landscaping	
PO 2.1 Bulk handling and storage facilities incorporate a buffer area for the establishment of dense landscaping adjacent road frontages	DTS/DPF 2.1 None are applicable.

to enhance the appearance of land and buildings from public thoroughfares.	
PO 2.2 Bulk handling and storage facilities incorporate landscaping to assist with screening and dust filtration.	DTS/DPF 2.2 None are applicable.
Access and Parking	
PO 3.1 Roadways and vehicle parking areas associated with bulk handling and storage facilities are designed and surfaced to control dust emissions and prevent drag out of material from the site.	DTS/DPF 3.1 Roadways and vehicle parking areas are sealed with an all-weather surface.
Slipways, Wharves and Pontoons	
PO 4.1 Slipways, wharves and pontoons used for the handling of bulk materials (such as fuel, oil, catch, bait and the like) incorporate catchment devices to avoid the release of materials into adjacent waters.	DTS/DPF 4.1 None are applicable.

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.	DTS/DPF 1.1 One of the following is satisfied: (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i> (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development is:</p> <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributes to the character of the immediate area (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access, and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All development	
External Appearance	
PO 1.1 Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).	DTS/DPF 1.1 None are applicable.
PO 1.2 Where zero or minor setbacks are desirable, development provides shelter over footpaths (<u>in the form of verandahs, awnings, canopies and the like, with adequate lighting</u>) to positively contribute to the walkability, comfort and safety of the public realm.	DTS/DPF 1.2 None are applicable.
PO 1.3 Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.	DTS/DPF 1.3 None are applicable.
PO 1.4 Plant, exhaust and intake vents and other technical equipment is integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by: <ul style="list-style-type: none"> (a) positioning plant and equipment in unobtrusive locations viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses. 	DTS/DPF 1.4 Development does not incorporate any structures that protrude beyond the roofline.

PO 1.5	DTS/DPF 1.5
The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form) taking into account the form of development contemplated in the relevant zone.	None are applicable.
Safety	
PO 2.1	DTS/DPF 2.1
Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.	None are applicable.
PO 2.2	DTS/DPF 2.2
Development is designed to differentiate public, communal and private areas.	None are applicable.
PO 2.3	DTS/DPF 2.3
Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.	None are applicable.
PO 2.4	DTS/DPF 2.4
Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.	None are applicable.
PO 2.5	DTS/DPF 2.5
Common areas and entry points of buildings (such as the foyer areas of residential buildings), and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.	None are applicable.
Landscaping	
PO 3.1	DTS/DPF 3.1
Soft landscaping and tree planting is incorporated to: <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration (d) enhance the appearance of land and streetscapes (e) contribute to biodiversity. 	None are applicable.
PO 3.2	DTS/DPF 3.2
Soft landscaping and tree planting maximises the use of locally indigenous plant species, incorporates plant species best suited to current and future climate conditions and avoids pest plant and weed species.	None are applicable.
Environmental Performance	

PO 4.1	DTS/DPF 4.1
Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.	None are applicable.
PO 4.2	DTS/DPF 4.2
Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.	None are applicable.
PO 4.3	DTS/DPF 4.3
Buildings incorporate climate-responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.	None are applicable.
Water Sensitive Design	
PO 5.1	DTS/DPF 5.1
Development is sited and designed to maintain natural hydrological systems without negatively impacting:	None are applicable.
<ul style="list-style-type: none"> (a) the quantity and quality of surface water and groundwater (b) the depth and directional flow of surface water and groundwater (c) the quality and function of natural springs. 	
On-site Waste Treatment Systems	
PO 6.1	DTS/DPF 6.1
Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.	<p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
Carparking Appearance	
PO 7.1	DTS/DPF 7.1
Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on the streetscapes through techniques such as:	None are applicable.
<ul style="list-style-type: none"> (a) limiting protrusion above finished ground level (b) screening through appropriate planting, fencing and mounding (c) limiting the width of openings and integrating them into the building structure. 	
PO 7.2	DTS/DPF 7.2

Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.	None are applicable.
PO 7.3 Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.	DTS/DPF 7.3 None are applicable.
PO 7.4 Street level vehicle parking areas incorporate tree planting to provide shade and reduce solar heat absorption and reflection.	DTS/DPF 7.4 None are applicable.
PO 7.5 Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.	DTS/DPF 7.5 None are applicable.
PO 7.6 Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.	DTS/DPF 7.6 None are applicable.
PO 7.7 Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.	DTS/DPF 7.7 None are applicable.
Earthworks and sloping land	
PO 8.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 8.1 Development does not involve any of the following: (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
PO 8.2 Driveways and access tracks are designed and constructed to allow safe and convenient access on sloping land (with a gradient exceeding 1 in 8).	DTS/DPF 8.2 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b): (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
PO 8.3 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8): (a) do not contribute to the instability of embankments and cuttings	DTS/DPF 8.3 None are applicable.

<p>(b) provide level transition areas for the safe movement of people and goods to and from the development</p> <p>(c) are designed to integrate with the natural topography of the land.</p>	
<p>PO 8.4</p> <p>Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on-site drainage systems to minimise erosion.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>
<p>PO 8.5</p> <p>Development does not occur on land at risk of landslip nor increases the potential for landslip or land surface instability.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>
Fences and Walls	
<p>PO 9.1</p> <p>Fences, walls and retaining walls are of sufficient height to maintain privacy and security without unreasonably impacting the visual amenity and adjoining land's access to sunlight or the amenity of public places.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Landscaping incorporated on the low side of retaining walls is visible from public roads and public open space to minimise visual impacts.</p>	<p>DTS/DPF 9.2</p> <p>A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.</p>
Overlooking / Visual Privacy (in building 3 storeys or less)	
<p>PO 10.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.</p>	<p>DTS/DPF 10.1</p> <p>Upper level windows facing side or rear boundaries shared with a residential allotment/site satisfy one of the following:</p> <ul style="list-style-type: none"> (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.
<p>PO 10.2</p> <p>Development mitigates direct overlooking from balconies, terraces and decks to habitable rooms and private open space of adjoining residential uses.</p>	<p>DTS/DPF 10.2</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> (i) 1.5m above finished floor level where the

	<p>balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</p> <p>or</p> <p>(ii) 1.7m above finished floor level in all other cases</p>
All Residential development	
Front elevations and passive surveillance	
<p>PO 11.1</p> <p>Dwellings incorporate windows along primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 11.1</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m² facing the primary street.
<p>PO 11.2</p> <p>Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.</p>	<p>DTS/DPF 11.2</p> <p>Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.</p>
Outlook and amenity	
<p>PO 12.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 12.1</p> <p>A living room of a dwelling incorporates a window with an outlook towards the street frontage or private open space, public open space, or waterfront areas.</p>
<p>PO 12.2</p> <p>Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.</p>	<p>DTS/DPF 12.2</p> <p>None are applicable.</p>
Ancillary Development	
<p>PO 13.1</p> <p>Residential ancillary buildings and structures are sited and designed to not detract from the streetscape or appearance of buildings on the site or neighbouring properties.</p>	<p>DTS/DPF 13.1</p> <p>Ancillary buildings:</p> <ul style="list-style-type: none"> (a) are ancillary to a dwelling erected on the same site (b) have a floor area not exceeding 60m² (c) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> (i) in front of any part of the building line of the dwelling to which it is ancillary or (ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> (i) is set back at least 5.5m from the boundary of the primary street (ii) when facing a primary street or secondary street, has a total door / opening not exceeding: <ul style="list-style-type: none"> A. for dwellings of single building level - 7m in width or 50% of the site frontage,

	<p>whichever is the lesser</p> <p>B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width</p> <p>(e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:</p> <p>(i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary</p> <p>and</p> <p>(ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent</p> <p>(f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary</p> <p>(g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure</p> <p>(h) have a wall height or post height not exceeding 3m above natural ground level</p> <p>(i) have a roof height where no part of the roof is more than 5m above the natural ground level</p> <p>(j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour</p> <p>(k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:</p> <p>(i) a total area as determined by the following table:</p> <table border="1"> <thead> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th><th>Minimum percentage of site</th></tr> </thead> <tbody> <tr> <td><150</td><td>10%</td></tr> <tr> <td>150-200</td><td>15%</td></tr> <tr> <td>201-450</td><td>20%</td></tr> <tr> <td>>450</td><td>25%</td></tr> </tbody> </table> <p>(ii) the amount of existing soft landscaping prior to the development occurring.</p>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	201-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
201-450	20%										
>450	25%										
PO 13.2	DTS/DPF 13.2										

Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision or car parking requirements and do not result in over-development of the site.	Ancillary buildings and structures do not result in: <ul style="list-style-type: none"> (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
<p>PO 13.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa is positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.</p>	<p>DTS/DPF 13.3</p> <p>The pump and/or filtration system is ancillary to a dwelling erected on the same site and is:</p> <ul style="list-style-type: none"> (a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.
Garage appearance	
<p>PO 14.1</p> <p>Garaging is designed to not detract from the streetscape or appearance of a dwelling.</p>	<p>DTS/DPF 14.1</p> <p>Garages and carports facing a street:</p> <ul style="list-style-type: none"> (a) are situated so that no part of the garage or carport is in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening not exceeding 7m in width (d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.
Massing	
<p>PO 15.1</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 15.1</p> <p>None are applicable</p>
Dwelling additions	
<p>PO 16.1</p> <p>Dwelling additions are sited and designed to not detract from the streetscape or amenity of adjoining properties and do not impede on-site functional requirements.</p>	<p>DTS / DPF 16.1</p> <p>Dwelling additions:</p> <ul style="list-style-type: none"> (a) are not constructed, added to or altered so that any part is situated closer to a public street (b) do not result in: <ul style="list-style-type: none"> (i) excavation exceeding a vertical height of 1m (ii) filling exceeding a vertical height of 1m (iii) a total combined excavation and filling vertical height of 2m or more (iv) less Private Open Space than specified in Design Table 1 - Private Open Space (v) less on-site parking than specified in Transport Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas

	<p>(vi) upper level windows facing side or rear boundaries unless:</p> <ul style="list-style-type: none"> A. they are permanently obscured to a height of 1.5m above finished floor level that is fixed or not capable of being opened more than 200mm or B. have sill heights greater than or equal to 1.5m above finished floor level or C. incorporate screening to a height of 1.5m above finished floor level <p>(vii) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of:</p> <ul style="list-style-type: none"> A. 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land B. 1.7m above finished floor level in all other cases.
Private Open Space	
<p>PO 17.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 17.1</p> <p>Private open space is provided in accordance with Design Table 1 - Private Open Space.</p>
Water Sensitive Design	
<p>PO 18.1</p> <p>Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.</p>	<p>DTS/DPF 18.1</p> <p>Residential development creating a common driveway / access that services 5 or more dwellings achieves the following stormwater runoff outcomes:</p> <ul style="list-style-type: none"> (a) 80 per cent reduction in average annual total suspended solids (b) 60 per cent reduction in average annual total phosphorus (c) 45 per cent reduction in average annual total nitrogen.
<p>PO 18.2</p> <p>Residential development creating a common driveway / access includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.</p>	<p>DTS/DPF 18.2</p> <p>Development creating a common driveway / access that services 5 or more dwellings:</p> <ul style="list-style-type: none"> (a) maintains the pre-development peak flow rate from the site based upon a 0.35 runoff coefficient for the 18.1% AEP 30-minute storm and the stormwater runoff time to peak is not increased or captures and retains the difference in pre-development runoff volume (based upon a 0.35 runoff coefficient) vs post development runoff volume from the site for an 18.1% AEP 30-minute storm; and (b) manages site generated stormwater runoff up to and including the 1% AEP flood event to avoid flooding of

	buildings.
Car parking, access and manoeuvrability	
<p>PO 19.1</p> <p>Enclosed parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single width car parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m per space (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space.
<p>PO 19.2</p> <p>Uncovered parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m
<p>PO 19.3</p> <p>Driveways are located and designed to facilitate safe access and egress while maximising land available for street tree planting, landscaped street frontages, domestic waste collection and on-street parking.</p>	<p>DTS/DPF 19.3</p> <p>Driveways and access points on sites with a frontage to a public road of 10m or less have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site.</p>
<p>PO 19.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 19.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed: <ul style="list-style-type: none"> (i) is set back 6m or more from the tangent point of an intersection of 2 or more roads (ii) is set back outside of the marked lines or infrastructure dedicating a pedestrian crossing (iii) does not involve the removal, relocation or damage to of mature street trees, street furniture or utility infrastructure services.
<p>PO 19.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 19.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of

	<p>the garage or carport is not steeper than 1:4 on average</p> <p>(b) they are aligned relative to the street boundary so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the street boundary</p> <p>(c) if located to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site</p>								
<p>PO 19.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 19.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <p>(a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</p> <p>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</p> <p>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</p>								
Waste storage									
<p>PO 20.1</p> <p>Provision is made for the adequate and convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 20.1</p> <p>None are applicable.</p>								
Design of Transportable Dwellings									
<p>PO 21.1</p> <p>The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.</p>	<p>DTS/DPF 21.1</p> <p>Buildings satisfy (a) or (b):</p> <p>(a) are not transportable or</p> <p>(b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.</p>								
Group dwelling, residential flat buildings and battle-axe development									
Amenity									
<p>PO 22.1</p> <p>Dwellings are of a suitable size to accommodate a layout that is well organised and provides a high standard of amenity for occupants.</p>	<p>DTS/DPF 22.1</p> <p>Dwellings have a minimum internal floor area in accordance with the following table:</p> <table border="1"> <thead> <tr> <th>Number of bedrooms</th><th>Minimum internal floor area</th></tr> </thead> <tbody> <tr> <td>Studio</td><td>35m²</td></tr> <tr> <td>1 bedroom</td><td>50m²</td></tr> <tr> <td>2 bedroom</td><td>65m²</td></tr> </tbody> </table>	Number of bedrooms	Minimum internal floor area	Studio	35m ²	1 bedroom	50m ²	2 bedroom	65m ²
Number of bedrooms	Minimum internal floor area								
Studio	35m ²								
1 bedroom	50m ²								
2 bedroom	65m ²								

	3+ bedrooms	80m ² and any dwelling over 3 bedrooms provides an additional 15m ² for every additional bedroom
PO 22.2	DTS/DPF 22.2	
The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.	None are applicable.	
PO 22.3	DTS/DPF 22.3	
Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.	None are applicable.	
PO 22.4	DTS/DPF 22.4	
Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.	Dwelling sites/allotments are not in the form of a battle-axe arrangement.	
Communal Open Space		
PO 23.1	DTS/DPF 23.1	
Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	None are applicable.	
PO 23.2	DTS/DPF 23.2	
Communal open space is of sufficient size and dimensions to cater for group recreation.	Communal open space incorporates a minimum dimension of 5 metres.	
PO 23.3	DTS/DPF 23.3	
Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	None are applicable.	
PO 23.4	DTS/DPF 23.4	
Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	None are applicable.	
PO 23.5	DTS/DPF 23.5	
Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	None are applicable.	
Carparking, access and manoeuvrability		
PO 24.1	DTS/DPF 24.1	

Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.	Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements: <ul style="list-style-type: none"> (a) minimum 0.33 on-street car parks per proposed dwellings (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
PO 24.2 The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.	DTS/DPF 24.2 Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.
PO 24.3 Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.	DTS/DPF 24.3 Driveways that service more than 1 dwelling or a dwelling on a battle-axe site: <ul style="list-style-type: none"> (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: <ul style="list-style-type: none"> (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
PO 24.4 Residential driveways in a battle-axe configuration are designed to allow safe and convenient movement.	DTS/DPF 24.4 Where in a battle-axe configuration, a driveway servicing one dwelling has a minimum width of 3m.
PO 24.5 Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 24.5 Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.
PO 24.6 Dwellings are adequately separated from common driveways and manoeuvring areas.	DTS/DPF 24.6 Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Soft Landscaping	
PO 25.1 Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.	DTS/DPF 25.1 Other than where located directly in front of a garage or a building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.
PO 25.2 Soft landscaping is provided that improves the appearance of common driveways.	DTS/DPF 25.2 Where a common driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site

	boundary (excluding along the perimeter of a passing point).
Site Facilities / Waste Storage	
PO 26.1 Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	DTS/DPF 26.1 None are applicable.
PO 26.2 Provision is made for suitable external clothes drying facilities.	DTS/DPF 26.2 None are applicable.
PO 26.3 Provision is made for suitable household waste and recyclable material storage facilities which are: (a) located away, or screened, from public view, and (b) conveniently located in proximity to dwellings and the waste collection point.	DTS/DPF 26.3 None are applicable.
PO 26.4 Waste and recyclable material storage areas are located away from dwellings.	DTS/DPF 26.4 Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 26.5 Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.	DTS/DPF 26.5 None are applicable.
PO 26.6 Services including gas and water meters are conveniently located and screened from public view.	DTS/DPF 26.6 None are applicable.
Supported accommodation and retirement facilities	
Siting and Configuration	
PO 27.1 Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of residents is not unduly restricted by the slope of the land.	DTS/DPF 27.1 None are applicable.
Movement and Access	
PO 28.1 Development is designed to support safe and convenient access and movement for residents by providing: (a) ground-level access or lifted access to all units (b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places (c) car parks with gradients no steeper than 1-in-40 and of sufficient area to provide for wheelchair manoeuvrability	DTS/DPF 28.1 None are applicable.

(d) kerb ramps at pedestrian crossing points.	
Communal Open Space	
PO 29.1 Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.	DTS/DPF 29.1 None are applicable.
PO 29.2 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 29.2 None are applicable.
PO 29.3 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 29.3 Communal open space incorporates a minimum dimension of 5 metres.
PO 29.4 Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 29.4 None are applicable.
PO 29.5 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 29.5 None are applicable.
PO 29.6 Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	DTS/DPF 29.6 None are applicable.
Site Facilities / Waste Storage	
PO 30.1 Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric powered vehicles.	DTS/DPF 30.1 None are applicable.
PO 30.2 Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	DTS/DPF 30.2 None are applicable.
PO 30.3	DTS/DPF 28.3

Provision is made for suitable external clothes drying facilities.	None are applicable.
PO 30.4 Provision is made for suitable household waste and recyclable material storage facilities conveniently located and screened from public view.	DTS/DPF 30.4 None are applicable.
PO 30.5 Waste and recyclable material storage areas are located away from dwellings.	DTS/DPF 30.5 Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 30.6 Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.	DTS/DPF 30.6 None are applicable.
PO 30.7 Services including gas and water meters are conveniently located and screened from public view.	DTS/DPF 30.7 None are applicable.
All non-residential development	
Water Sensitive Design	
PO 31.1 Development likely to result in significant risk of export of litter, oil or grease includes stormwater management systems designed to minimise pollutants entering stormwater.	DTS/DPF 31.1 None are applicable.
PO 31.2 Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.	DTS/DPF 31.2 None are applicable.
Wash-down and Waste Loading and Unloading	
PO 32.1 Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, vessels, plant or equipment are: (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area (d) designed to drain wastewater to either: (i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or (ii) a holding tank and its subsequent removal off-site on a regular basis.	DTS/DPF 32.1 None are applicable.

Table 1 - Private Open Space

Dwelling Type	Minimum Rate
Dwelling (at ground level)	<p>Total private open space area:</p> <p>(a) Site area <301m²: 24m² located behind the building line.</p> <p>(b) Site area ≥ 301m²: 60m² located behind the building line.</p> <p>Minimum directly accessible from a living room: 16m² / with a minimum dimension 3m.</p>
Dwelling (above ground level)	<p>Studio (no separate bedroom): 4m² with a minimum dimension 1.8m</p> <p>One bedroom: 8m² with a minimum dimension 2.1m</p> <p>Two bedroom dwelling: 11m² with a minimum dimension 2.4m</p> <p>Three + bedroom dwelling: 15m² with a minimum dimension 2.6m</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or a caravan and tourist park	Total area: 16m ² , which may be used as second car parking space, provided on each site intended for residential occupation.

Design in Urban Areas

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development is:</p> <p>(a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality</p> <p>(b) durable - fit for purpose, adaptable and long lasting</p> <p>(c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors</p> <p>(d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.</p>

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
DTS/DPF 42.2	All Development

None are applicable.	
External Appearance	
PO 1.1 Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).	DTS/DPF 1.1 None are applicable.
PO 1.2 Where zero or minor setbacks are desirable, development provides shelter over footpaths (in the form of verandahs, awnings, canopies and the like, with adequate lighting) to positively contribute to the walkability, comfort and safety of the public realm.	DTS/DPF 1.2 None are applicable.
PO 1.3 Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.	DTS/DPF 1.3 None are applicable.
PO 1.4 Plant, exhaust and intake vents and other technical equipment are integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by: (a) positioning plant and equipment discretely, in unobtrusive locations as viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses.	DTS/DPF 1.4 Development does not incorporate any structures that protrude beyond the roofline.
PO 1.5 The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form), taking into account the form of development contemplated in the relevant zone.	DTS/DPF 1.5 None are applicable.
Safety	
PO 2.1 Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development is designed to differentiate public, communal and private areas.	DTS/DPF 2.2 None are applicable.
PO 2.3	DTS/DPF 2.3

Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.	None are applicable.
PO 2.4 Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.	DTS/DPF 2.4 None are applicable.
PO 2.5 Common areas and entry points of buildings (such as the foyer areas of residential buildings) and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.	DTS/DPF 2.5 None are applicable.
Landscaping	
PO 3.1 Soft landscaping and tree planting are incorporated to: (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration (d) enhance the appearance of land and streetscapes.	DTS/DPF 3.1 None are applicable.
Environmental Performance	
PO 4.1 Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.	DTS/DPF 4.1 None are applicable.
PO 4.2 Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.	DTS/DPF 4.2 None are applicable.
PO 4.3 Buildings incorporate climate responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.	DTS/DPF 4.3 None are applicable.
Water Sensitive Design	
PO 5.1 Development is sited and designed to maintain natural hydrological systems without negatively impacting: (a) the quantity and quality of surface water and groundwater (b) the depth and directional flow of surface water and groundwater (c) the quality and function of natural springs.	DTS/DPF 5.1 None are applicable.
On-site Waste Treatment Systems	

PO 6.1	DTS/DPF 6.1
Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.	<p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design in Urban Areas Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
Car parking appearance	
PO 7.1	DTS/DPF 7.1
<p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on streetscapes through techniques such as:</p> <ul style="list-style-type: none"> (a) limiting protrusion above finished ground level (b) screening through appropriate planting, fencing and mounding (c) limiting the width of openings and integrating them into the building structure. 	None are applicable.
PO 7.2	DTS/DPF 7.2
Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.	None are applicable.
PO 7.3	DTS/DPF 7.3
Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.	None are applicable.
PO 7.4	DTS/DPF 7.4
Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.	Vehicle parking areas that are open to the sky and comprise 10 or more car parking spaces include a shade tree with a mature canopy of 4m diameter spaced for each 10 car parking spaces provided and a landscaped strip on any road frontage of a minimum dimension of 1m.
PO 7.5	DTS/DPF 7.5
Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.	<p>Vehicle parking areas comprising 10 or more car parking spaces include soft landscaping with a minimum dimension of:</p> <ul style="list-style-type: none"> (a) 1m along all public road frontages and allotment boundaries (b) 1m between double rows of car parking spaces.
PO 7.6	DTS/DPF 7.6
Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.	None are applicable.
PO 7.7	DTS/DPF 7.7
Vehicle parking areas and access ways incorporate integrated	None are applicable.

stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.	
Earthworks and sloping land	
PO 8.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 8.1 Development does not involve any of the following: (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
PO 8.2 Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.	DTS/DPF 8.2 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b): (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
PO 8.3 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8): (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land.	DTS/DPF 8.3 None are applicable.
PO 8.4 Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.	DTS/DPF 8.4 None are applicable.
PO 8.5 Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.	DTS/DPF 8.5 None are applicable.
Fences and walls	
PO 9.1 Fences, walls and retaining walls of sufficient height maintain privacy and security without unreasonably impacting visual amenity and adjoining land's access to sunlight or the amenity of public places.	DTS/DPF 9.1 None are applicable.
PO 9.2 Landscaping is incorporated on the low side of retaining walls that are visible from public roads and public open space to minimise visual impacts.	DTS/DPF 9.2 A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.
Overlooking / Visual Privacy (low rise buildings)	

PO 10.1 Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.	DTS/DPF 10.1 Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone: (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.
PO 10.2 Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.	DTS/DPF 10.2 One of the following is satisfied: (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases
Site Facilities / Waste Storage (excluding low rise residential development)	
PO 11.1 Development provides a dedicated area for on-site collection and sorting of recyclable materials and refuse, green organic waste and wash bay facilities for the ongoing maintenance of bins that is adequate in size considering the number and nature of the activities they will serve and the frequency of collection.	DTS/DPF 11.1 None are applicable.
PO 11.2 Communal waste storage and collection areas are located, enclosed and designed to be screened from view from the public domain, open space and dwellings.	DTS/DPF 11.2 None are applicable.
PO 11.3 Communal waste storage and collection areas are designed to be well ventilated and located away from habitable rooms.	DTS/DPF 11.3 None are applicable.
PO 11.4 Communal waste storage and collection areas are designed to allow waste and recycling collection vehicles to enter and leave the site without reversing.	DTS/DPF 11.4 None are applicable.
PO 11.5 For mixed use developments, non-residential waste and recycling storage areas and access provide opportunities for on-site management of food waste through composting or other waste recovery as appropriate.	DTS/DPF 11.5 None are applicable.
All Development - Medium and High Rise	
External Appearance	

PO 12.1 Buildings positively contribute to the character of the local area by responding to local context.	DTS/DPF 12.1 None are applicable.
PO 12.2 Architectural detail at street level and a mixture of materials at lower building levels near the public interface are provided to reinforce a human scale.	DTS/DPF 12.2 None are applicable.
PO 12.3 Buildings are designed to reduce visual mass by breaking up building elevations into distinct elements.	DTS/DPF 12.3 None are applicable.
PO 12.4 Boundary walls visible from public land include visually interesting treatments to break up large blank elevations.	DTS/DPF 12.4 None are applicable.
PO 12.5 External materials and finishes are durable and age well to minimise ongoing maintenance requirements.	DTS/DPF 12.5 Buildings utilise a combination of the following external materials and finishes: <ul style="list-style-type: none"> (a) masonry (b) natural stone (c) pre-finished materials that minimise staining, discolouring or deterioration.
PO 12.6 Street-facing building elevations are designed to provide attractive, high quality and pedestrian-friendly street frontages.	DTS/DPF 12.6 Building street frontages incorporate: <ul style="list-style-type: none"> (a) active uses such as shops or offices (b) prominent entry areas for multi-storey buildings (where it is a common entry) (c) habitable rooms of dwellings (d) areas of communal public realm with public art or the like, where consistent with the zone and/or subzone provisions.
PO 12.7 Entrances to multi-storey buildings are safe, attractive, welcoming, functional and contribute to streetscape character.	DTS/DPF 12.7 Entrances to multi-storey buildings are: <ul style="list-style-type: none"> (a) oriented towards the street (b) clearly visible and easily identifiable from the street and vehicle parking areas (c) designed to be prominent, accentuated and a welcoming feature if there are no active or occupied ground floor uses (d) designed to provide shelter, a sense of personal address and transitional space around the entry (e) located as close as practicable to the lift and / or lobby access to minimise the need for long access corridors (f) designed to avoid the creation of potential areas of entrapment.
PO 12.8	DTS/DPF 12.8

Building services, plant and mechanical equipment are screened from the public realm.	None are applicable.			
Landscaping				
PO 13.1 Development facing a street provides a well landscaped area that contains a deep soil space to accommodate a tree of a species and size adequate to provide shade, contribute to tree canopy targets and soften the appearance of buildings.	DTS/DPF 13.1 Buildings provide a 4m by 4m deep soil space in front of the building that accommodates a medium to large tree, except where no building setback from front property boundaries is desired.			
PO 13.2 Deep soil zones are provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies to provide shade and soften the appearance of multi-storey buildings.	DTS/DPF 13.2 Multi-storey development provides deep soil zones and incorporates trees at not less than the following rates, except in a location or zone where full site coverage is desired.			
	Site area	Minimum deep soil area	Minimum dimension	Tree / deep soil zones
	<300 m ²	10 m ²	1.5m	1 small tree / 10 m ²
	300-1500 m ²	7% site area	3m	1 medium tree / 30 m ²
	>1500 m ²	7% site area	6m	1 large or medium tree / 60 m ²
	Tree size and site area definitions			
	Small tree	4-6m mature height and 2-4m canopy spread		
	Medium tree	6-12m mature height and 4-8m canopy spread		
	Large tree	12m mature height and >8m canopy spread		
Site area	The total area for development site, not average area per dwelling			
PO 13.3 Deep soil zones with access to natural light are provided to assist in maintaining vegetation health.	DTS/DPF 13.3 None are applicable.			
PO 13.4 Unless separated by a public road or reserve, development sites adjacent to any zone that has a primary purpose of accommodating low-rise residential development incorporate a deep soil zone along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more building levels in height.	DTS/DPF 13.4 Building elements of 3 or more building levels in height are set back at least 6m from a zone boundary in which a deep soil zone area is incorporated.			

Environmental	
PO 14.1 Development minimises detrimental micro-climatic impacts on adjacent land and buildings.	DTS/DPF 14.1 None are applicable.
PO 14.2 Development incorporates sustainable design techniques and features such as window orientation, eaves and shading structures, water harvesting and use, green walls and roof designs that enable the provision of rain water tanks (where they are not provided elsewhere on site), green roofs and photovoltaic cells.	DTS/DPF 14.2 None are applicable.
PO 14.3 Development of 5 or more building levels, or 21m or more in height (as measured from natural ground level and excluding roof-mounted mechanical plant and equipment) is designed to minimise the impacts of wind through measures such as: <ul style="list-style-type: none"> (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street (b) substantial verandahs around a building to deflect downward travelling wind flows over pedestrian areas (c) the placement of buildings and use of setbacks to deflect the wind at ground level (d) avoiding tall shear elevations that create windy conditions at street level. 	DTS/DPF 14.3 None are applicable.
Car Parking	
PO 15.1 Multi-level vehicle parking structures are designed to contribute to active street frontages and complement neighbouring buildings.	DTS/DPF 15.1 Multi-level vehicle parking structures within buildings: <ul style="list-style-type: none"> (a) provide land uses such as commercial, retail or other non-car parking uses along ground floor street frontages (b) incorporate facade treatments in building elevations facing along major street frontages that are sufficiently enclosed and detailed to complement adjacent buildings.
PO 15.2 Multi-level vehicle parking structures within buildings complement the surrounding built form in terms of height, massing and scale.	DTS/DPF 15.2 None are applicable.
Overlooking/Visual Privacy	
PO 16.1 Development mitigates direct overlooking of habitable rooms and private open spaces of adjacent residential uses in neighbourhood-type zones through measures such as: <ul style="list-style-type: none"> (a) appropriate site layout and building orientation (b) off-setting the location of balconies and windows of habitable rooms or areas with those of other buildings so that views are oblique rather than direct to avoid 	DTS/DPF 16.1 None are applicable.

<p>direct line of sight</p> <p>(c) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms</p> <p>(d) screening devices that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.</p>	
All residential development	
Front elevations and passive surveillance	
<p>PO 17.1</p> <p>Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 17.1</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m² facing the primary street.
<p>PO 17.2</p> <p>Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.</p>	<p>DTS/DPF 17.2</p> <p>Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.</p>
Outlook and Amenity	
<p>PO 18.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 18.1</p> <p>A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.</p>
<p>PO 18.2</p> <p>Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.</p>	<p>DTS/DPF 18.2</p> <p>None are applicable.</p>
Ancillary Development	
<p>PO 19.1</p> <p>Residential ancillary buildings are sited and designed to not detract from the streetscape or appearance of primary residential buildings on the site or neighbouring properties.</p>	<p>DTS/DPF 19.1</p> <p>Ancillary buildings:</p> <ul style="list-style-type: none"> (a) are ancillary to a dwelling erected on the same site (b) have a floor area not exceeding 60m² (c) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> (i) in front of any part of the building line of the dwelling to which it is ancillary or (ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> (i) is set back at least 5.5m from the boundary of the primary street (ii) when facing a primary street or secondary street, has a total door / opening not exceeding:

- A. for dwellings of single building level - 7m in width or 50% of the site frontage, whichever is the lesser
- B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width

- (e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:
 - (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and
 - (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent
- (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary
- (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure
- (h) have a wall height or post height not exceeding 3m above natural ground level
- (i) have a roof height where no part of the roof is more than 5m above the natural ground level
- (j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour
- (k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:
 - (i) a total area as determined by the following table:

Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site
<150	10%
150-200	15%
201-450	20%
>450	25%

- (ii) the amount of existing soft landscaping prior to the development occurring.

<p>Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision, car parking requirements or result in over-development of the site.</p>	<p>Ancillary buildings and structures do not result in:</p> <ul style="list-style-type: none"> (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
<p>PO 19.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.</p>	<p>DTS/DPF 19.3</p> <p>The pump and/or filtration system is ancillary to a dwelling erected on the same site and is:</p> <ul style="list-style-type: none"> (a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.
Residential Development - Low Rise	
External appearance	
<p>PO 20.1</p> <p>Garaging is designed to not detract from the streetscape or appearance of a dwelling.</p>	<p>DTS/DPF 20.1</p> <p>Garages and carports facing a street:</p> <ul style="list-style-type: none"> (a) are situated so that no part of the garage or carport will be in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening width not exceeding 7m (d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.
<p>PO 20.2</p> <p>Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and the appearance of common driveway areas.</p>	<p>DTS/DPF 20.2</p> <p>Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway:</p> <ul style="list-style-type: none"> (a) a minimum of 30% of the building wall is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building wall (c) a balcony projects from the building wall (d) a verandah projects at least 1m from the building wall (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm (g) a minimum of two different materials or finishes are incorporated on the walls of the front building elevation, with a maximum of 80% of the building elevation in a single material or finish.

PO 20.3 The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	DTS/DPF 20.3 None are applicable										
Private Open Space											
PO 21.1 Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 21.1 Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.										
PO 21.2 Private open space is positioned to provide convenient access from internal living areas.	DTS/DPF 21.2 Private open space is directly accessible from a habitable room.										
Landscaping											
PO 22.1 Soft landscaping is incorporated into development to: (a) minimise heat absorption and reflection (b) contribute shade and shelter (c) provide for stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes.	DTS/DPF 22.1 Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b): (a) a total area as determined by the following table: <table border="1"> <thead> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th><th>Minimum percentage of site</th></tr> </thead> <tbody> <tr> <td><150</td><td>10%</td></tr> <tr> <td>150-200</td><td>15%</td></tr> <tr> <td>>200-450</td><td>20%</td></tr> <tr> <td>>450</td><td>25%</td></tr> </tbody> </table> (b) at least 30% of any land between the primary street boundary and the primary building line.	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										
Car parking, access and manoeuvrability											
PO 23.1 Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.	DTS/DPF 23.1 Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area): (a) single width car parking spaces: <ol style="list-style-type: none"> a minimum length of 5.4m per space a minimum width of 3.0m a minimum garage door width of 2.4m 										

	<ul style="list-style-type: none"> (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space.
<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.
<p>PO 23.3</p> <p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 23.3</p> <p>Driveways and access points satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site (b) sites with a frontage to a public road greater than 10m: <ul style="list-style-type: none"> (i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site; (ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of

	<p>the garage or carport is not steeper than 1-in-4 on average</p> <p>(b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary.</p> <p>(c) if located so as to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site</p>
<p>PO 23.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 23.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <p>(a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</p> <p>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</p> <p>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</p>
Waste storage	
<p>PO 24.1</p> <p>Provision is made for the convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 24.1</p> <p>Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that:</p> <p>(a) has a minimum area of 2m² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and</p> <p>(b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.</p>
Design of Transportable Buildings	
<p>PO 25.1</p> <p>The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.</p>	<p>DTS/DPF 25.1</p> <p>Buildings satisfy (a) or (b):</p> <p>(a) are not transportable</p> <p>(b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.</p>
Residential Development - Medium and High Rise (including serviced apartments)	
Outlook and Visual Privacy	
<p>PO 26.1</p> <p>Ground level dwellings have a satisfactory short range visual outlook to public, communal or private open space.</p>	<p>DTS/DPF 26.1</p> <p>Buildings:</p> <p>(a) provide a habitable room at ground or first level with a window facing toward the street</p> <p>(b) limit the height / extent of solid walls or fences facing the street to 1.2m high above the footpath level or, where higher, to 50% of the site frontage.</p>

PO 26.2	DTS/DPF 26.2
The visual privacy of ground level dwellings within multi-level buildings is protected.	The finished floor level of ground level dwellings in multi-storey developments is raised by up to 1.2m.
Private Open Space	
PO 27.1	DTS/DPF 27.1
Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	Private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space.
Residential amenity in multi-level buildings	
PO 28.1	DTS/DPF 28.1
Residential accommodation within multi-level buildings have habitable rooms, windows and balconies designed and positioned to be separated from those of other dwellings and accommodation to provide visual and acoustic privacy and allow for natural ventilation and the infiltration of daylight into interior and outdoor spaces.	Habitable rooms and balconies of independent dwellings and accommodation are separated by at least 6m from one another where there is a direct line of sight between them and 3m or more from a side or rear property boundary.
PO 28.2	DTS/DPF 28.2
Balconies are designed, positioned and integrated into the overall architectural form and detail of the development to: <ul style="list-style-type: none"> (a) respond to daylight, wind, and acoustic conditions to maximise comfort and provide visual privacy (b) allow views and casual surveillance of the street while providing for safety and visual privacy of nearby living spaces and private outdoor areas. 	Balconies utilise one or a combination of the following design elements: <ul style="list-style-type: none"> (a) sun screens (b) pergolas (c) louvres (d) green facades (e) openable walls.
PO 28.3	DTS/DPF 28.3
Balconies are of sufficient size and depth to accommodate outdoor seating and promote indoor / outdoor living.	Balconies open directly from a habitable room and incorporate a minimum dimension of 2m.
PO 28.4	DTS/DPF 28.4
Dwellings are provided with sufficient space for storage to meet likely occupant needs.	Dwellings (not including student accommodation or serviced apartments) are provided with storage at the following rates with at least 50% or more of the storage volume to be provided within the dwelling: <ul style="list-style-type: none"> (a) studio: not less than 6m³ (b) 1 bedroom dwelling / apartment: not less than 8m³ (c) 2 bedroom dwelling / apartment: not less than 10m³ (d) 3+ bedroom dwelling / apartment: not less than 12m³.
PO 28.5	DTS/DPF 28.5
Dwellings that use light wells for access to daylight, outlook and ventilation for habitable rooms, are designed to ensure a reasonable living amenity is provided.	Light wells: <ul style="list-style-type: none"> (a) are not used as the primary source of outlook for living rooms (b) up to 18m in height have a minimum horizontal dimension of 3m, or 6m if overlooked by bedrooms (c) above 18m in height have a minimum horizontal dimension of 6m, or 9m if overlooked by bedrooms.

PO 28.6	DTS/DPF 28.6								
Attached or abutting dwellings are designed to minimise the transmission of sound between dwellings and, in particular, to protect bedrooms from possible noise intrusions.	None are applicable.								
PO 28.7	DTS/DPF 28.7								
Dwellings are designed so that internal structural columns correspond with the position of internal walls to ensure that the space within the dwelling/apartment is useable.	None are applicable.								
Dwelling Configuration									
PO 29.1	DTS/DPF 29.1								
Buildings containing in excess of 10 dwellings provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling to contribute to housing diversity.	Buildings containing in excess of 10 dwellings provide at least one of each of the following: <ul style="list-style-type: none"> (a) studio (where there is no separate bedroom) (b) 1 bedroom dwelling / apartment with a floor area of at least 50m² (c) 2 bedroom dwelling / apartment with a floor area of at least 65m² (d) 3+ bedroom dwelling / apartment with a floor area of at least 80m², and any dwelling over 3 bedrooms provides an additional 15m² for every additional bedroom. 								
PO 29.2	DTS/DPF 29.2								
Dwellings located on the ground floor of multi-level buildings with 3 or more bedrooms have the windows of their habitable rooms overlooking internal courtyard space or other public space, where possible.	None are applicable.								
Common Areas									
PO 30.1	DTS/DPF 30.1								
The size of lifts, lobbies and corridors is sufficient to accommodate movement of bicycles, strollers, mobility aids and visitor waiting areas.	Common corridor or circulation areas: <ul style="list-style-type: none"> (a) have a minimum ceiling height of 2.7m (b) provide access to no more than 8 dwellings (c) incorporate a wider section at apartment entries where the corridors exceed 12m in length from a core. 								
Group Dwellings, Residential Flat Buildings and Battle axe Development									
Amenity									
PO 31.1	DTS/DPF 31.1								
Dwellings are of a suitable size to provide a high standard of amenity for occupants.	Dwellings have a minimum internal floor area in accordance with the following table: <table border="1"> <thead> <tr> <th>Number of bedrooms</th><th>Minimum internal floor area</th></tr> </thead> <tbody> <tr> <td>Studio</td><td>35m²</td></tr> <tr> <td>1 bedroom</td><td>50m²</td></tr> <tr> <td>2 bedroom</td><td>65m²</td></tr> </tbody> </table>	Number of bedrooms	Minimum internal floor area	Studio	35m ²	1 bedroom	50m ²	2 bedroom	65m ²
Number of bedrooms	Minimum internal floor area								
Studio	35m ²								
1 bedroom	50m ²								
2 bedroom	65m ²								

	3+ bedrooms	80m ² and any dwelling over 3 bedrooms provides an additional 15m ² for every additional bedroom
PO 31.2 The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.	DTS/DPF 31.2 None are applicable.	
PO 31.3 Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.	DTS/DPF 31.3 None are applicable.	
PO 31.4 Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.	DTS/DPF 31.4 Dwelling sites/allotments are not in the form of a battle-axe arrangement.	
Communal Open Space		
PO 32.1 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 32.1 None are applicable.	
PO 32.2 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 32.2 Communal open space incorporates a minimum dimension of 5 metres.	
PO 32.3 Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 32.3 None are applicable.	
PO 32.4 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 32.4 None are applicable.	
PO 32.5 Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	DTS/DPF 32.5 None are applicable.	
Car parking, access and manoeuvrability		
PO 33.1 Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.	DTS/DPF 33.1 Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements:	

	<ul style="list-style-type: none"> (a) minimum 0.33 on-street car parks per proposed dwelling (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
<p>PO 33.2</p> <p>The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.</p>	<p>DTS/DPF 33.2</p> <p>Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.</p>
<p>PO 33.3</p> <p>Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.</p>	<p>DTS/DPF 33.3</p> <p>Driveways that service more than 1 dwelling or a dwelling on a battle-axe site:</p> <ul style="list-style-type: none"> (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: <ul style="list-style-type: none"> (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
<p>PO 33.4</p> <p>Residential driveways that service more than one dwelling or a dwelling on a battle-axe site are designed to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.</p>	<p>DTS/DPF 33.4</p> <p>Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.</p>
<p>PO 33.5</p> <p>Dwellings are adequately separated from common driveways and manoeuvring areas.</p>	<p>DTS/DPF 33.5</p> <p>Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.</p>
Soft landscaping	
<p>PO 34.1</p> <p>Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.</p>	<p>DTS/DPF 34.1</p> <p>Other than where located directly in front of a garage or building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.</p>
<p>PO 34.2</p> <p>Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.</p>	<p>DTS/DPF 34.2</p> <p>Battle-axe or common driveways satisfy (a) and (b):</p> <ul style="list-style-type: none"> (a) are constructed of a minimum of 50% permeable or porous material (b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Site Facilities / Waste Storage	

PO 35.1 Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	DTS/DPF 35.1 None are applicable.
PO 35.2 Provision is made for suitable external clothes drying facilities.	DTS/DPF 35.2 None are applicable.
PO 35.3 Provision is made for suitable household waste and recyclable material storage facilities which are: (a) located away, or screened, from public view, and (b) conveniently located in proximity to dwellings and the waste collection point.	DTS/DPF 35.3 None are applicable.
PO 35.4 Waste and recyclable material storage areas are located away from dwellings.	DTS/DPF 35.4 Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 35.5 Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.	DTS/DPF 35.5 None are applicable.
PO 35.6 Services including gas and water meters are conveniently located and screened from public view.	DTS/DPF 35.6 None are applicable.
Water sensitive urban design	
PO 36.1 Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 36.1 None are applicable.
PO 36.2 Residential development creating a common driveway / access includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 36.2 None are applicable.
Supported Accommodation and retirement facilities	
Siting, Configuration and Design	
PO 37.1 Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of	DTS/DPF 37.1 None are applicable.

residents is not unduly restricted by the slope of the land.	
PO 37.2 Universal design features are incorporated to provide options for people living with disabilities or limited mobility and / or to facilitate ageing in place.	DTS/DPF 37.2 None are applicable.
Movement and Access	
PO 38.1 Development is designed to support safe and convenient access and movement for residents by providing: (a) ground-level access or lifted access to all units (b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places (c) car parks with gradients no steeper than 1-in-40, and of sufficient area to provide for wheelchair manoeuvrability (d) kerb ramps at pedestrian crossing points.	DTS/DPF 38.1 None are applicable.
Communal Open Space	
PO 39.1 Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.	DTS/DPF 39.1 None are applicable.
PO 39.2 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 39.2 None are applicable.
PO 39.3 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 39.3 Communal open space incorporates a minimum dimension of 5 metres.
PO 39.4 Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 39.4 None are applicable.
PO 39.5 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 39.5 None are applicable.
PO 39.6 Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive	DTS/DPF 39.6 None are applicable.

surveillance.	
Site Facilities / Waste Storage	
PO 40.1 Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric-powered vehicles.	DTS/DPF 40.1 None are applicable.
PO 40.2 Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	DTS/DPF 40.2 None are applicable.
PO 40.3 Provision is made for suitable external clothes drying facilities.	DTS/DPF 40.3 None are applicable.
PO 40.4 Provision is made for suitable household waste and recyclable material storage facilities conveniently located away, or screened, from view.	DTS/DPF 40.4 None are applicable.
PO 40.5 Waste and recyclable material storage areas are located away from dwellings.	DTS/DPF 40.5 Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 40.6 Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.	DTS/DPF 40.6 None are applicable.
PO 40.7 Services, including gas and water meters, are conveniently located and screened from public view.	DTS/DPF 40.7 None are applicable.
Student Accommodation	
PO 41.1 Student accommodation is designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents, including an internal layout and facilities that are designed to provide sufficient space and amenity for the requirements of student life and promote social interaction.	DTS/DPF 41.1 Student accommodation provides: <ul style="list-style-type: none"> (a) a range of living options to meet a variety of accommodation needs, such as one-bedroom, two-bedroom and disability access units (b) common or shared facilities to enable a more efficient use of space, including: <ul style="list-style-type: none"> (i) shared cooking, laundry and external drying facilities (ii) internal and external communal and private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space (iii) common storage facilities at the rate of 8m³ for every 2 dwellings or students (iv) common on-site parking in accordance with Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2

	<p>- Off-Street Car Parking Requirements in Designated Areas</p> <p>(v) bicycle parking at the rate of one space for every 2 students.</p>
<p>PO 41.2</p> <p>Student accommodation is designed to provide easy adaptation of the building to accommodate an alternative use of the building in the event it is no longer required for student housing.</p>	<p>DTS/DPF 41.2</p> <p>None are applicable.</p>
All non-residential development	
Water Sensitive Design	
<p>PO 42.1</p> <p>Development likely to result in risk of export of sediment, suspended solids, organic matter, nutrients, oil and grease include stormwater management systems designed to minimise pollutants entering stormwater.</p>	
<p>DTS/DPF 42.1</p> <p>None are applicable.</p>	<p>PO 42.2</p> <p>Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.</p>
<p>PO 42.3</p> <p>Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.</p>	<p>DTS/DPF 42.3</p> <p>None are applicable.</p>
Wash-down and Waste Loading and Unloading	
<p>PO 43.1</p> <p>Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, plant or equipment are:</p> <ul style="list-style-type: none"> (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area (d) are designed to drain wastewater to either: <ul style="list-style-type: none"> (i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or (ii) a holding tank and its subsequent removal off-site on a regular basis. 	<p>DTS/DPF 43.1</p> <p>None are applicable.</p>
Laneway Development	

Infrastructure and Access	
<p>PO 44.1</p> <p>Development with a primary street comprising a laneway, alley, lane, right of way or similar minor thoroughfare only occurs where:</p> <ul style="list-style-type: none"> (a) existing utility infrastructure and services are capable of accommodating the development (b) the primary street can support access by emergency and regular service vehicles (such as waste collection) (c) it does not require the provision or upgrading of infrastructure on public land (such as footpaths and stormwater management systems) (d) safety of pedestrians or vehicle movement is maintained (e) any necessary grade transition is accommodated within the site of the development to support an appropriate development intensity and orderly development of land fronting minor thoroughfares. 	<p>DTS/DPF 44.1</p> <p>Development with a primary street frontage that is not an alley, lane, right of way or similar public thoroughfare.</p>

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site Configuration	Minimum Rate
Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		<p>Total private open space area:</p> <ul style="list-style-type: none"> (a) Site area <301m²: 24m² located behind the building line. (b) Site area ≥ 301m²: 60m² located behind the building line. <p>Minimum directly accessible from a living room: 16m² / with a minimum dimension 3m.</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m ² , which may be used as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m ² / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m ² / minimum dimension 1.8m
	One bedroom dwelling	8m ² / minimum dimension 2.1m
	Two bedroom dwelling	11m ² / minimum dimension 2.4m
	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m

Assessment Provisions (AP)

Desired Outcome	
DO 1	Commercial forestry is designed and sited to maximise economic benefits whilst managing potential negative impacts on the environment, transport networks, surrounding land uses and landscapes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Commercial forestry plantations are established where there is no detrimental effect on the physical environment or scenic quality of the rural landscape.	DTS/DPF 1.1 None are applicable.
PO 1.2 Commercial forestry plantations are established on slopes that are stable to minimise the risk of soil erosion.	DTS/DPF 1.2 Commercial forestry plantations are not located on land with a slope exceeding 20% (1-in-5).
PO 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are appropriately set back from any sensitive receiver to minimise fire risk and noise disturbance.	DTS/DPF 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are set back 50m or more from any sensitive receiver.
PO 1.4 Commercial forestry plantations are separated from reserves gazetted under the <i>National Parks and Wildlife Act 1972</i> and/or <i>Wilderness Protection Act 1992</i> to minimise fire risk and potential for weed infestation.	DTS/DPF 1.4 Commercial forestry plantations and operations associated with their establishment, management and harvesting are set back 50m or more from a reserve gazetted under the <i>National Parks and Wildlife Act 1972</i> and/or <i>Wilderness Protection Act 1992</i> .
Water Protection	
PO 2.1 Commercial forestry plantations incorporate artificial drainage lines (i.e. culverts, runoffs and constructed drains) integrated with natural drainage lines to minimise concentrated water flows onto or from plantation areas.	DTS/DPF 2.1 None are applicable.
PO 2.2 Appropriate siting, layout and design measures are adopted to minimise the impact of commercial forestry plantations on	DTS/DPF 2.2 Commercial forestry plantations:

surface water resources.	<div><div>(a)</div><div>do not involve cultivation (excluding spot cultivation) in drainage lines</div></div> <div><div>(b)</div><div>are set back 20m or more from the banks of any major watercourse (a third order or higher watercourse), lake, reservoir, wetland or sinkhole (with direct connection to an aquifer)</div></div> <div><div>(c)</div><div>are set back 10m or more from the banks of any first or second order watercourse or sinkhole (with no direct connection to an aquifer).</div></div>																		
Fire Management																			
<div>PO 3.1</div> <div>Commercial forestry plantations incorporate appropriate firebreaks and fire management design elements.</div>	<div>DTS/DPF 3.1</div> <div>Commercial forestry plantations provide:</div> <div><div>(a)</div><div>7m or more wide external boundary firebreaks for plantations of 40ha or less</div></div> <div><div>(b)</div><div>10m or more wide external boundary firebreaks for plantations of between 40ha and 100ha</div></div> <div><div>(c)</div><div>20m or more wide external boundary firebreaks, or 10m with an additional 10m or more of fuel-reduced plantation, for plantations of 100ha or greater.</div></div>																		
<div>PO 3.2</div> <div>Commercial forestry plantations incorporate appropriate fire management access tracks.</div>	<div>DTS/DPF 3.2</div> <div>Commercial forestry plantation fire management access tracks:</div> <div><div>(a)</div><div>are incorporated within all firebreaks</div></div> <div><div>(b)</div><div>are 7m or more wide with a vertical clearance of 4m or more</div></div> <div><div>(c)</div><div>are aligned to provide straight through access at junctions, or if they are a no through access track are appropriately signposted and provide suitable turnaround areas for fire-fighting vehicles</div></div> <div><div>(d)</div><div>partition the plantation into units of 40ha or less in area.</div></div>																		
Power-line Clearances																			
<div>PO 4.1</div> <div>Commercial forestry plantations achieve and maintain appropriate clearances from aboveground powerlines.</div>	<div>DTS/DPF 4.1</div> <div>Commercial forestry plantations incorporating trees with an expected mature height of greater than 6m meet the clearance requirements listed in the following table:</div> <table><tr><td>Voltage of transmission line</td><td>Tower or Pole</td><td>Minimum horizontal clearance distance between plantings and transmission lines</td></tr><tr><td>500 kV</td><td>Tower</td><td>38m</td></tr><tr><td>275 kV</td><td>Tower</td><td>25m</td></tr><tr><td>132 kV</td><td>Tower</td><td>30m</td></tr><tr><td>132 kV</td><td>Pole</td><td>20m</td></tr><tr><td>66 kV</td><td>Pole</td><td>20m</td></tr></table>	Voltage of transmission line	Tower or Pole	Minimum horizontal clearance distance between plantings and transmission lines	500 kV	Tower	38m	275 kV	Tower	25m	132 kV	Tower	30m	132 kV	Pole	20m	66 kV	Pole	20m
Voltage of transmission line	Tower or Pole	Minimum horizontal clearance distance between plantings and transmission lines																	
500 kV	Tower	38m																	
275 kV	Tower	25m																	
132 kV	Tower	30m																	
132 kV	Pole	20m																	
66 kV	Pole	20m																	

	Less than 66 kV	Pole	20m

Housing Renewal

Assessment Provisions (AP)

Desired Outcome	
DO 1	Renewed residential environments replace older social housing and provide new social housing infrastructure and other housing options and tenures to enhance the residential amenity of the local area.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Residential development provides a range of housing choices.	DTS/DPF 1.1 Development comprises one or more of the following: (a) detached dwellings (b) semi-detached dwellings (c) row dwellings (d) group dwellings (e) residential flat buildings.
PO 1.2 Medium-density housing options or higher are located in close proximity to public transit, open space and/or activity centres.	DTS/DPF 1.2 None are applicable.
Building Height	
PO 2.1 Buildings generally do not exceed 3 building levels unless in locations close to public transport, centres and/or open space.	DTS/DPF 2.1 Building height (excluding garages, carports and outbuildings) does not exceed 3 building levels and 12m and wall height does not exceed 9m (not including a gable end).
PO 2.2 Medium or high rise residential flat buildings located within or at the interface with zones which restrict heights to a maximum of 2 building levels transition down in scale and height towards the boundary of that zone, other than where it is a street boundary.	DTS/DPF 2.2 None are applicable.
Primary Street Setback	

PO 3.1	DTS/DPF 3.1
Buildings are set back from the primary street boundary to contribute to an attractive streetscape character.	Buildings are no closer to the primary street (excluding any balcony, verandah, porch, awning or similar structure) than 3m.
Secondary Street Setback	
PO 4.1	DTS/DPF 4.1
Buildings are set back from secondary street boundaries to maintain separation between building walls and public streets and contribute to a suburban streetscape character.	Buildings are set back at least 900mm from the boundary of the allotment with a secondary street frontage.
Boundary Walls	
PO 5.1	DTS/DPF 5.1
Boundary walls are limited in height and length to manage visual impacts and access to natural light and ventilation.	<p>Except where the dwelling is located on a central site within a row dwelling or terrace arrangement, dwellings with side boundary walls are sited on only one side boundary and satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) adjoin or abut a boundary wall of a building on adjoining land for the same length and height (b) do not: <ul style="list-style-type: none"> (i) exceed 3.2m in height from the lower of the natural or finished ground level (ii) exceed 11.5m in length (iii) when combined with other walls on the boundary of the subject development site, a maximum 45% of the length of the boundary (iv) encroach within 3 metres of any other existing or proposed boundary walls on the subject land.
PO 5.2	DTS/DPF 5.2
Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a suburban streetscape character.	Dwellings in a semi-detached or row arrangement are set back 900mm or more from side boundaries shared with allotments outside the development site, except for a carport or garage.
Side Boundary Setback	
PO 6.1	DTS/DPF 6.1
<p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between dwellings in a way that contributes to a suburban character (b) access to natural light and ventilation for neighbours. 	<p>Other than walls located on a side boundary, buildings are set back from side boundaries:</p> <ul style="list-style-type: none"> (a) at least 900mm where the wall height is up to 3m (b) other than for a wall facing a southern side boundary, at least 900mm plus 1/3 of the wall height above 3m (c) at least 1.9m plus 1/3 of the wall height above 3m for walls facing a southern side boundary.
Rear Boundary Setback	
PO 7.1	DTS/DPF 7.1
<p>Buildings are set back from rear boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between dwellings in a way that contributes to a suburban character (b) access to natural light and ventilation for neighbours 	<p>Dwellings are set back from the rear boundary:</p> <ul style="list-style-type: none"> (a) 3m or more for the first building level (b) 5m or more for any subsequent building level.

(c) private open space (d) space for landscaping and vegetation.	
Buildings elevation design	
PO 8.1 Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and common driveway areas.	DTS/DPF 8.1 Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway: (a) a minimum of 30% of the building elevation is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building elevation (c) a balcony projects from the building elevation (d) a verandah projects at least 1m from the building elevation (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm. (g) a minimum of two different materials or finishes are incorporated on the walls of the building elevation, with a maximum of 80% of the building elevation in a single material or finish.
PO 8.2 Dwellings incorporate windows along primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.	DTS/DPF 8.2 Each dwelling with a frontage to a public street: (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m ² facing the primary street
PO 8.3 The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	DTS/DPF 8.3 None are applicable.
PO 8.4 Built form considers local context and provides a quality design response through scale, massing, materials, colours and architectural expression.	DTS/DPF 8.4 None are applicable.
PO 8.5 Entrances to multi-storey buildings are: (a) oriented towards the street (b) visible and easily identifiable from the street (c) designed to include a common mail box structure.	DTS/DPF 8.5 None are applicable.
Outlook and amenity	
PO 9.1	DTS/DPF 9.1

Living rooms have an external outlook to provide a high standard of amenity for occupants.	A living room of a dwelling incorporates a window with an external outlook towards the street frontage or private open space.		
PO 9.2 Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.	DTS/DPF 9.2 None are applicable.		
Private Open Space			
PO 10.1 Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 10.1 Private open space is provided in accordance with the following table:		
	Dwelling Type	Dwelling / Site Configuration	Minimum Rate
	Dwelling (at ground level)		Total area: 24m ² located behind the building line Minimum adjacent to a living room: 16m ² with a minimum dimension 3m
	Dwelling (above ground level)	Studio	4m ² / minimum dimension 1.8m
		One bedroom dwelling	8m ² / minimum dimension 2.1m
		Two bedroom dwelling	11m ² / minimum dimension 2.4m
Three + bedroom dwelling		15 m ² / minimum dimension 2.6m	
PO 10.2 Private open space positioned to provide convenient access from internal living areas.	DTS/DPF 10.2 At least 50% of the required area of private open space is accessible from a habitable room.		
PO 10.3 Private open space is positioned and designed to: (a) provide useable outdoor space that suits the needs of occupants; (b) take advantage of desirable orientation and vistas; and (c) adequately define public and private space.	DTS/DPF 10.3 None are applicable.		
Visual privacy			

<p>PO 11.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.</p>	<p>DTS/DPF 11.1</p> <p>Upper level windows facing side or rear boundaries shared with another residential allotment/site satisfy one of the following:</p> <ul style="list-style-type: none"> (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5m above the finished floor. 										
<p>PO 11.2</p> <p>Development mitigates direct overlooking from upper level balconies and terraces to habitable rooms and private open space of adjoining residential uses.</p>	<p>DTS/DPF 11.2</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases 										
Landscaping											
<p>PO 12.1</p> <p>Soft landscaping is incorporated into development to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes. 	<p>DTS/DPF 12.1</p> <p>Residential development incorporates pervious areas for soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <ul style="list-style-type: none"> (a) a total area as determined by the following table: <table border="1" data-bbox="831 1615 1522 1872"> <thead> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th><th>Minimum percentage of site</th></tr> </thead> <tbody> <tr> <td><150</td><td>10%</td></tr> <tr> <td><200</td><td>15%</td></tr> <tr> <td>200-450</td><td>20%</td></tr> <tr> <td>>450</td><td>25%</td></tr> </tbody> </table> (b) at least 30% of land between the road boundary and the building line. 	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	<200	15%	200-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
<200	15%										
200-450	20%										
>450	25%										
Water Sensitive Design											
<p>PO 13.1</p> <p>Residential development is designed to capture and use</p>	<p>DTS/DPF 13.1</p> <p>None are applicable.</p>										

<p>stormwater to:</p> <ul style="list-style-type: none"> (a) maximise efficient use of water resources (b) manage peak stormwater runoff flows and volume to ensure the carrying capacities of downstream systems are not overloaded (c) manage runoff quality to maintain, as close as practical, pre-development conditions. 	
Car Parking	
<p>PO 14.1</p> <p>On-site car parking is provided to meet the anticipated demand of residents, with less on-site parking in areas in close proximity to public transport.</p>	<p>DTS/DPF 14.1</p> <p>On-site car parking is provided at the following rates per dwelling:</p> <ul style="list-style-type: none"> (a) 2 or fewer bedrooms - 1 car parking space (b) 3 or more bedrooms - 2 car parking spaces.
<p>PO 14.2</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 14.2</p> <p>Residential parking spaces enclosed by fencing, walls or other obstructions with the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.5m (iii) minimum garage door width of 2.4m per space.
<p>PO 14.3</p> <p>Uncovered car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 14.3</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.
<p>PO 14.4</p> <p>Residential flat buildings and group dwelling developments provide sufficient on-site visitor car parking to cater for anticipated demand.</p>	<p>DTS/DPF 14.4</p> <p>Visitor car parking for group and residential flat buildings incorporating 4 or more dwellings is provided on-site at a minimum ratio of 0.25 car parking spaces per dwelling.</p>
<p>PO 14.5</p> <p>Residential flat buildings provide dedicated areas for bicycle parking.</p>	<p>DTS/DPF 14.5</p> <p>Residential flat buildings provide one bicycle parking space per dwelling.</p>
Overshadowing	
<p>PO 15.1</p> <p>Development minimises overshadowing of the private open spaces of adjoining land by ensuring that ground level open</p>	<p>DTS/DPF 15.1</p> <p>None are applicable.</p>

space associated with residential buildings receive direct sunlight for a minimum of 2 hours between 9am and 3pm on 21 June.	
Waste	
PO 16.1 Provision is made for the convenient storage of waste bins in a location screened from public view.	DTS/DPF 16.1 A waste bin storage area is provided behind the primary building line that: (a) has a minimum area of 2m ² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space).; and (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.
PO 16.2 Residential flat buildings provide a dedicated area for the on-site storage of waste which is: (a) easily and safely accessible for residents and for collection vehicles (b) screened from adjoining land and public roads (c) of sufficient dimensions to be able to accommodate the waste storage needs of the development considering the intensity and nature of the development and the frequency of collection.	DTS/DPF 16.2 None are applicable.
Vehicle Access	
PO 17.1 Driveways are located and designed to facilitate safe access and egress while maximising land available for street tree planting, landscaped street frontages and on-street parking.	DTS/DPF 17.1 None are applicable.
PO 17.2 Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.	DTS/DPF 17.2 Vehicle access to designated car parking spaces satisfy (a) or (b): (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
PO 17.3	DTS/DPF 17.3

Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.	Driveways are designed and sited so that: <ul style="list-style-type: none"> (a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not more than 1-in-4 on average (b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary. (c) if located so as to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site.
PO 17.4 Driveways and access points are designed and distributed to optimise the provision of on-street parking.	DTS/DPF 17.4 Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements: <ul style="list-style-type: none"> 1. minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number) 2. Minimum car park length of 5.4m where a vehicle can enter or exit a space directly 3. minimum car park length of 6m for an intermediate space located between two other parking spaces.
PO 17.5 Residential driveways that service more than one dwelling of a dimension to allow safe and convenient movement.	DTS/DPF 17.5 Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements: <ul style="list-style-type: none"> (a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
PO 17.6 Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 17.6 Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre
PO 17.7 Dwellings are adequately separated from common driveways and manoeuvring areas.	DTS/DPF 17.7 Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Storage	
PO 18.1 Dwellings are provided with sufficient and accessible space for storage to meet likely occupant needs.	DTS/DPF 18.1 Dwellings are provided with storage at the following rates and 50% or more of the storage volume is provided within the dwelling: <ul style="list-style-type: none"> (a) studio: not less than 6m³

	(b) 1 bedroom dwelling / apartment: not less than 8m ³ (c) 2 bedroom dwelling / apartment: not less than 10m ³ (d) 3+ bedroom dwelling / apartment: not less than 12m ³ .
Earthworks	
PO 19.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 19.1 The development does not involve: <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m or (b) filling exceeding a vertical height of 1m or (c) a total combined excavation and filling vertical height exceeding 2m.
Service connections and infrastructure	
PO 20.1 Dwellings are provided with appropriate service connections and infrastructure.	DTS/DPF 20.1 The site and building: <ul style="list-style-type: none"> (a) have the ability to be connected to a permanent potable water supply (b) have the ability to be connected to a sewerage system, or a wastewater system approved under the <i>South Australian Public Health Act 2011</i> (c) have the ability to be connected to electricity supply (d) have the ability to be connected to an adequate water supply (and pressure) for fire-fighting purposes (e) would not be contrary to the Regulations prescribed for the purposes of Section 86 of the <i>Electricity Act 1996</i>.
Site contamination	
PO 21.1 Land that is suitable for sensitive land uses to provide a safe environment.	DTS/DPF 21.1 Development satisfies (a), (b), (c) or (d): <ul style="list-style-type: none"> (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a <u>more sensitive use</u> (c) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> does not exist (as demonstrated in a <u>site contamination declaration form</u>) (d) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> (i) a <u>site contamination audit report</u> has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that <ul style="list-style-type: none"> A. <u>site contamination</u> does not exist (or no longer exists) at the land or B. the land is suitable for the proposed use or range of uses (without the need for any further <u>remediation</u>) or C. where <u>remediation</u> is, or remains,

	<p>necessary for the proposed use (or range of uses), <u>remediation work</u> has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development)</p> <p>and</p> <p>(ii) no other <u>class 1 activity</u> or <u>class 2 activity</u> has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a <u>site contamination declaration form</u>).</p>
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Infrastructure and Renewable Energy Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Development is located and designed to minimise hazard or nuisance to adjacent development and land uses.	DTS/DPF 1.1 None are applicable.
Visual Amenity	
PO 2.1 The visual impact of above-ground infrastructure networks and services (excluding high voltage transmission lines), renewable energy facilities (excluding wind farms), energy storage facilities and ancillary development is minimised from townships, scenic routes and public roads by: (a) utilising features of the natural landscape to obscure views where practicable (b) siting development below ridgelines where practicable	DTS/DPF 2.1 None are applicable.

<p>(c) avoiding visually sensitive and significant landscapes</p> <p>(d) using materials and finishes with low-reflectivity and colours that complement the surroundings</p> <p>(e) using existing vegetation to screen buildings</p> <p>(f) incorporating landscaping or landscaped mounding around the perimeter of a site and between adjacent allotments accommodating or zoned to primarily accommodate sensitive receivers.</p>	
<p>PO 2.2</p> <p>Pumping stations, battery storage facilities, maintenance sheds and other ancillary structures incorporate vegetation buffers to reduce adverse visual impacts on adjacent land.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Surfaces exposed by earthworks associated with the installation of storage facilities, pipework, penstock, substations and other ancillary plant are reinstated and revegetated to reduce adverse visual impacts on adjacent land.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
Rehabilitation	
<p>PO 3.1</p> <p>Progressive rehabilitation (incorporating revegetation) of disturbed areas, ahead of or upon decommissioning of areas used for renewable energy facilities and transmission corridors.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
Hazard Management	
<p>PO 4.1</p> <p>Infrastructure and renewable energy facilities and ancillary development located and operated to not adversely impact maritime or air transport safety, including the operation of ports, airfields and landing strips.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Facilities for energy generation, power storage and transmission are separated as far as practicable from dwellings, tourist accommodation and frequently visited public places (such as viewing platforms / lookouts) to reduce risks to public safety from fire or equipment malfunction.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Bushfire hazard risk is minimised for renewable energy facilities by providing appropriate access tracks, safety equipment and water tanks and establishing cleared areas around substations, battery storage and operations compounds.</p>	<p>DTS/DPF 4.3</p> <p>None are applicable.</p>

Electricity Infrastructure and Battery Storage Facilities	
<p>PO 5.1</p> <p>Electricity infrastructure is located to minimise visual impacts through techniques including:</p> <ul style="list-style-type: none"> (a) siting utilities and services: <ul style="list-style-type: none"> (i) on areas already cleared of native vegetation (ii) where there is minimal interference or disturbance to existing native vegetation or biodiversity (b) grouping utility buildings and structures with non-residential development, where practicable. 	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
<p>PO 5.2</p> <p>Electricity supply (excluding transmission lines) serving new development in urban areas and townships installed underground, excluding lines having a capacity exceeding or equal to 33kV.</p>	<p>DTS/DPF 5.2</p> <p>None are applicable.</p>
<p>PO 5.3</p> <p>Battery storage facilities are co-located with substation infrastructure where practicable to minimise the development footprint and reduce environmental impacts.</p>	<p>DTS/DPF 5.3</p> <p>None are applicable.</p>
Telecommunication Facilities	
<p>PO 6.1</p> <p>The proliferation of telecommunications facilities in the form of towers/monopoles in any one locality is managed, where technically feasible, by co-locating a facility with other communications facilities to mitigate impacts from clutter on visual amenity.</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>
<p>PO 6.2</p> <p>Telecommunications antennae are located as close as practicable to support structures to manage overall bulk and mitigate impacts on visual amenity.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>
<p>PO 6.3</p> <p>Telecommunications facilities, particularly towers/monopoles, are located and sized to mitigate visual impacts by the following methods:</p> <ul style="list-style-type: none"> (a) where technically feasible, incorporating the facility within an existing structure that may serve another purpose or all of the following: (b) using existing buildings and landscape features to obscure or interrupt views of a 	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>

<p>facility from nearby public roads, residential areas and places of high public amenity to the extent practical without unduly hindering the effective provision of telecommunications services</p> <p>(c) using materials and finishes that complement the environment</p> <p>(d) screening using landscaping and vegetation, particularly for equipment shelters and huts.</p>	
Renewable Energy Facilities	
<p>PO 7.1</p> <p>Renewable energy facilities are located as close as practicable to existing transmission infrastructure to facilitate connections and minimise environmental impacts as a result of extending transmission infrastructure.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
Renewable Energy Facilities (Wind Farm)	
<p>PO 8.1</p> <p>Visual impact of wind turbine generators on the amenity of residential and tourist development is reduced through appropriate separation.</p>	<p>DTS/DPF 8.1</p> <p>Wind turbine generators are:</p> <p>(a) set back at least 2000m from the base of a turbine to any of the following zones:</p> <ul style="list-style-type: none"> (i) Rural Settlement Zone (ii) Township Zone (iii) Rural Living Zone (iv) Rural Neighbourhood Zone <p>with an additional 10m setback per additional metre over 150m overall turbine height (measured from the base of the turbine).</p> <p>(b) set back at least 1500m from the base of the turbine to non-associated (non-stakeholder) dwellings and tourist accommodation</p>
<p>PO 8.2</p> <p>The visual impact of wind turbine generators on natural landscapes is managed by:</p> <p>(a) designing wind turbine generators to be uniform in colour, size and shape</p> <p>(b) coordinating blade rotation and direction</p> <p>(c) mounting wind turbine generators on tubular towers as opposed to lattice towers.</p>	<p>DTS/DPF 8.2</p> <p>None are applicable.</p>
<p>PO 8.3</p> <p>Wind turbine generators and ancillary development minimise potential for bird and bat strike.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>
<p>PO 8.4</p> <p>Wind turbine generators incorporate recognition systems or physical markers to minimise the risk to aircraft operations.</p>	<p>DTS/DPF 8.4</p> <p>No Commonwealth air safety (CASA / ASA) or Defence requirement is applicable.</p>
<p>PO 8.5</p>	<p>DTS/DPF 8.5</p>

Meteorological masts and guidewires are identifiable to aircraft through the use of colour bands, marker balls, high visibility sleeves or flashing strobes.	None are applicable.																																							
Renewable Energy Facilities (Solar Power)																																								
PO 9.1 Ground mounted solar power facilities generating 5MW or more are not located on land requiring the clearance of areas of intact native vegetation or on land of high environmental, scenic or cultural value.	DTS/DPF 9.1 None are applicable.																																							
PO 9.2 Ground mounted solar power facilities allow for movement of wildlife by: (a) incorporating wildlife corridors and habitat refuges (b) avoiding the use of extensive security or perimeter fencing or incorporating fencing that enables the passage of small animals without unreasonably compromising the security of the facility.	DTS/DPF 9.2 None are applicable.																																							
PO 9.3 Amenity impacts of solar power facilities are minimised through separation from conservation areas and sensitive receivers in other ownership.	DTS/DPF 9.3 Ground mounted solar power facilities are set back from land boundaries, conservation areas and relevant zones in accordance with the following criteria: <table><tr><th>Generation Capacity</th><th>Approximate size of array</th><th>Setback from adjoining land boundary</th><th>Setback from conservation areas</th><th>Setback from Township, Rural Settlement, Rural Neighbourhood and Rural Living Zones¹</th></tr><tr><td>50MW></td><td>80ha+</td><td>30m</td><td>500m</td><td>2km</td></tr><tr><td>10MW<50MW</td><td>16ha-<80ha</td><td>25m</td><td>500m</td><td>1.5km</td></tr><tr><td>5MW<10MW</td><td>8ha to <16ha</td><td>20m</td><td>500m</td><td>1km</td></tr><tr><td>1MW<5MW</td><td>1.6ha to <8ha</td><td>15m</td><td>500m</td><td>500m</td></tr><tr><td>100kW<1MW</td><td>0.5ha<1.6ha</td><td>10m</td><td>500m</td><td>100m</td></tr><tr><td><100kW</td><td><0.5ha</td><td>5m</td><td>500m</td><td>25m</td></tr></table>					Generation Capacity	Approximate size of array	Setback from adjoining land boundary	Setback from conservation areas	Setback from Township, Rural Settlement, Rural Neighbourhood and Rural Living Zones ¹	50MW>	80ha+	30m	500m	2km	10MW<50MW	16ha-<80ha	25m	500m	1.5km	5MW<10MW	8ha to <16ha	20m	500m	1km	1MW<5MW	1.6ha to <8ha	15m	500m	500m	100kW<1MW	0.5ha<1.6ha	10m	500m	100m	<100kW	<0.5ha	5m	500m	25m
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<100kW	<0.5ha	5m	500m	25m																																				
Notes: 1. Does not apply when the site of the proposed ground mounted solar																																								

	power facility is located within one of these zones.
<p>PO 9.4</p> <p>Ground mounted solar power facilities incorporate landscaping within setbacks from adjacent road frontages and boundaries of adjacent allotments accommodating non-host dwellings, where balanced with infrastructure access and bushfire safety considerations.</p>	<p>DTS/DPF 9.4</p> <p>None are applicable.</p>
Hydropower / Pumped Hydropower Facilities	
<p>PO 10.1</p> <p>Hydropower / pumped hydropower facility storage is designed and operated to minimise the risk of storage dam failure.</p>	<p>DTS/DPF 10.1</p> <p>None are applicable.</p>
<p>PO 10.2</p> <p>Hydropower / pumped hydropower facility storage is designed and operated to minimise water loss through increased evaporation or system leakage, with the incorporation of appropriate liners, dam covers, operational measures or detection systems.</p>	<p>DTS/DPF 10.2</p> <p>None are applicable.</p>
<p>PO 10.3</p> <p>Hydropower / pumped hydropower facilities on existing or former mine sites minimise environmental impacts from site contamination, including from mine operations or water sources subject to such processes, now or in the future.</p>	<p>DTS/DPF 10.3</p> <p>None are applicable.</p>
Water Supply	
<p>PO 11.1</p> <p>Development is connected to an appropriate water supply to meet the ongoing requirements of the intended use.</p>	<p>DTS/DPF 11.1</p> <p>Development is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the on-going requirements of the development.</p>
<p>PO 11.2</p> <p>Dwellings are connected to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the intended use. Where this is not available an appropriate rainwater tank or storage system for domestic use is provided.</p>	<p>DTS/DPF 11.2</p> <p>A dwelling is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the development. Where this is not available it is serviced by a rainwater tank or tanks capable of holding at least 50,000 litres of water which is:</p> <ul style="list-style-type: none"> (a) exclusively for domestic use (b) connected to the roof drainage system of the dwelling.
Wastewater Services	
<p>PO 12.1</p> <p>Development is connected to an approved common wastewater disposal service with the capacity to meet the requirements of the intended use. Where this is not available an appropriate on-site service is provided to meet the ongoing requirements of the intended use in accordance with the following:</p>	<p>DTS/DPF 12.1</p> <p>Development is connected, or will be connected, to an approved common wastewater disposal service with the capacity to meet the requirements of the development. Where this is not available it is instead capable of being serviced by an on-site waste water treatment system in accordance with the following:</p>

<p>(a) it is wholly located and contained within the allotment of the development it will service</p> <p>(b) in areas where there is a high risk of contamination of surface, ground, or marine water resources from on-site disposal of liquid wastes, disposal systems are included to minimise the risk of pollution to those water resources</p> <p>(c) septic tank effluent drainage fields and other wastewater disposal areas are located away from watercourses and flood prone, sloping, saline or poorly drained land to minimise environmental harm.</p>	<p>(a) the system is wholly located and contained within the allotment of development it will service; and</p> <p>(b) the system will comply with the requirements of the South Australian Public Health Act 2011.</p>
<p>PO 12.2</p> <p>Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.</p>	<p>DTS/DPF 12.2</p> <p>Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.</p>
Temporary Facilities	
<p>PO 13.1</p> <p>In rural and remote locations, development that is likely to generate significant waste material during construction, including packaging waste, makes provision for a temporary on-site waste storage enclosure to minimise the incidence of wind-blown litter.</p>	<p>DTS/DPF 13.1</p> <p>A waste collection and disposal service is used to dispose of the volume of waste at the rate it is generated.</p>
<p>PO 13.2</p> <p>Temporary facilities to support the establishment of renewable energy facilities (including borrow pits, concrete batching plants, laydown, storage, access roads and worker amenity areas) are sited and operated to minimise environmental impact.</p>	<p>DTS/DPF 13.2</p> <p>None are applicable.</p>

Intensive Animal Husbandry and Dairies

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development of intensive animal husbandry and dairies in locations that are protected from encroachment by sensitive receivers and in a manner that minimises their adverse effects on amenity and the environment.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria /
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Designated Performance Feature	
Siting and Design	
PO 1.1 Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to not unreasonably impact on the environment or amenity of the locality.	DTS/DPF 1.1 None are applicable.
PO 1.2 Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to prevent the potential transmission of disease to other operations where animals are kept.	DTS/DPF 1.2 None are applicable.
PO 1.3 Intensive animal husbandry and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.	DTS/DPF 1.3 None are applicable.
PO 1.4 Dairies and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.	DTS/DPF 1.4 Dairies, associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities are located 500m or more from the nearest sensitive receiver in other ownership.
PO 1.5 Lagoons for the storage or treatment of milking shed effluent is adequately separated from roads to minimise impacts from odour on the general public.	DTS/DPF 1.5 Lagoons for the storage or treatment of milking shed effluent are set back 20m or more from public roads.
Waste	
PO 2.1 Storage of manure, used litter and other wastes (other than waste water lagoons) is sited, designed, constructed and managed to: (a) avoid attracting and harbouring vermin (b) avoid polluting water resources (c) be located outside 1% AEP flood event areas.	DTS/DPF 2.1 None are applicable.
Soil and Water Protection	
PO 3.1 To avoid environmental harm and adverse effects on water resources, intensive animal husbandry operations are appropriately set back from: (a) public water supply reservoirs (b) major watercourses (third order or higher stream)	DTS/DPF 3.1 Intensive animal husbandry operations are set back: (a) 800m or more from a public water supply reservoir (b) 200m or more from a major watercourse (third order or higher stream) (c) 100m or more from any other watercourse, bore or well

(c) any other watercourse, bore or well used for domestic or stock water supplies.	used for domestic or stock water supplies.
<p>PO 3.2</p> <p>Intensive animal husbandry operations and dairies incorporate appropriately designed effluent and run-off facilities that:</p> <p>(a) have sufficient capacity to hold effluent and runoff from the operations on site</p> <p>(b) ensure effluent does not infiltrate and pollute groundwater, soil or other water resources.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome		Deemed-to-Satisfy Criteria / Designated Performance Feature	
General Land Use Compatibility			
PO 1.1 Sensitive receivers are designed and sited to protect residents and occupants from adverse impacts generated by lawfully existing land uses (or lawfully approved land uses) and land uses desired in the zone.		DTS/DPF 1.1 None are applicable.	
PO 1.2 Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.		DTS/DPF 1.2 None are applicable.	
Hours of Operation			
PO 2.1 Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:		DTS/DPF 2.1 Development operating within the following hours:	
		Class of Development	Hours of operation

<p>(a) the nature of the development</p> <p>(b) measures to mitigate off-site impacts</p> <p>(c) the extent to which the development is desired in the zone</p> <p>(d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.</p>	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday
	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday
	<p>Shop, other than any one or combination of the following:</p> <p>(a) restaurant</p> <p>(b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</p>	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
Overshadowing		
<p>PO 3.1</p> <p>Overshadowing of habitable room windows of adjacent residential land uses in:</p> <p>a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.1</p> <p>North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.</p>	
<p>PO 3.2</p> <p>Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:</p> <p>a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.2</p> <p>Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:</p> <p>a. for ground level private open space, the smaller of the following:</p> <p>i. half the existing ground level open space</p> <p>or</p> <p>ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</p> <p>b. for ground level communal open space, at least half of the existing ground level open space.</p>	
<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <p>(a) the form of development contemplated in the zone</p> <p>(b) the orientation of the solar energy facilities</p> <p>(c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>	

PO 3.4	DTS/DPF 3.4
Development that incorporates moving parts, including windmills and wind farms, are located and operated to not cause unreasonable nuisance to nearby dwellings and tourist accommodation caused by shadow flicker.	None are applicable.
Activities Generating Noise or Vibration	
PO 4.1	DTS/DPF 4.1
Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).	Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.
PO 4.2	DTS/DPF 4.2
<p>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</p> <ul style="list-style-type: none"> (a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers (b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers (c) housing plant and equipment within an enclosed structure or acoustic enclosure (d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone. 	None are applicable.
PO 4.3	DTS/DPF 4.3
Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).	<p>The pump and/or filtration system ancillary to a dwelling erected on the same site is:</p> <ul style="list-style-type: none"> (a) enclosed in a solid acoustic structure located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.
PO 4.4	DTS/DPF 4.4
External noise into bedrooms is minimised by separating or shielding these rooms from service equipment areas and fixed noise sources located on the same or an adjoining allotment.	Adjacent land is used for residential purposes.
PO 4.5	DTS/DPF 4.5
Outdoor areas associated with licensed premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing adjacent sensitive receivers (or lawfully approved sensitive receivers).	None are applicable.

<p>PO 4.6</p> <p>Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 4.6</p> <p>Development incorporating music includes noise attenuation measures that will achieve the following noise levels:</p> <table border="1" data-bbox="831 255 1485 571"> <thead> <tr> <th data-bbox="831 255 1098 338">Assessment location</th><th data-bbox="1098 255 1485 338">Music noise level</th></tr> </thead> <tbody> <tr> <td data-bbox="831 338 1098 571">Externally at the nearest existing or envisaged noise sensitive location</td><td data-bbox="1098 338 1485 571">Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)</td></tr> </tbody> </table>	Assessment location	Music noise level	Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)
Assessment location	Music noise level				
Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)				
Air Quality					
<p>PO 5.1</p> <p>Development with the potential to emit harmful or nuisance-generating air pollution incorporates air pollution control measures to prevent harm to human health or unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) within the locality and zones primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>				
<p>PO 5.2</p> <p>Development that includes chimneys or exhaust flues (including cafes, restaurants and fast food outlets) is designed to minimise nuisance or adverse health impacts to sensitive receivers (or lawfully approved sensitive receivers) by:</p> <ul style="list-style-type: none"> (a) incorporating appropriate treatment technology before exhaust emissions are released (b) locating and designing chimneys or exhaust flues to maximise the dispersion of exhaust emissions, taking into account the location of sensitive receivers. 	<p>DTS/DPF 5.2</p> <p>None are applicable.</p>				
Light Spill					
<p>PO 6.1</p> <p>External lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>				
<p>PO 6.2</p> <p>External lighting is not hazardous to motorists and cyclists.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>				
Solar Reflectivity / Glare					
<p>PO 7.1</p> <p>Development is designed and comprised of materials and finishes that do not unreasonably cause a distraction to adjacent road users and pedestrian areas or unreasonably cause heat loading and micro-climatic impacts on adjacent buildings and land uses as a result of reflective solar glare.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>				
Electrical Interference					
<p>PO 8.1</p>	<p>DTS/DPF 8.1</p>				

Development in rural and remote areas does not unreasonably diminish or result in the loss of existing communication services due to electrical interference.	<p>The building or structure:</p> <ul style="list-style-type: none"> (a) is no greater than 10m in height, measured from existing ground level or (b) is not within a line of sight between a fixed transmitter and fixed receiver (antenna) other than where an alternative service is available via a different fixed transmitter or cable.
Interface with Rural Activities	
<p>PO 9.1</p> <p>Sensitive receivers are located and designed to mitigate impacts from lawfully existing horticultural and farming activities (or lawfully approved horticultural and farming activities), including spray drift and noise and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing intensive animal husbandry activities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>
<p>PO 9.3</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing land-based aquaculture activities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.3</p> <p>Sensitive receivers are located at least 200m from the boundary of a site used for land-based aquaculture and associated components in other ownership.</p>
<p>PO 9.4</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing dairies including associated wastewater lagoons and liquid/solid waste storage and disposal facilities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.4</p> <p>Sensitive receivers are sited at least 500m from the boundary of a site used for a dairy and associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities in other ownership.</p>
<p>PO 9.5</p> <p>Sensitive receivers are located and designed to mitigate the potential impacts from lawfully existing facilities used for the handling, transportation and storage of bulk commodities (recognising the potential for extended hours of operation) and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.5</p> <p>Sensitive receivers are located away from the boundary of a site used for the handling, transportation and/or storage of bulk commodities in other ownership in accordance with the following:</p> <ul style="list-style-type: none"> (a) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility (b) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals) where the handling of these materials into or from vessels does not exceed 100 tonnes per day (c) 500m or more, where it involves the storage of bulk petroleum in individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1000 cubic metres

	<p>(d) 500m or more, where it involves the handling of coal with a capacity up to 1 tonne per day or a storage capacity up to 50 tonnes</p> <p>(e) 1000m or more, where it involves the handling of coal with a capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes.</p>
<p>PO 9.6</p> <p>Setbacks and vegetation plantings along allotment boundaries should be incorporated to mitigate the potential impacts of spray drift and other impacts associated with agricultural and horticultural activities.</p>	<p>DTS/DPF 9.6</p> <p>None are applicable.</p>
<p>PO 9.7</p> <p>Urban development does not prejudice existing agricultural and horticultural activities through appropriate separation and design techniques.</p>	<p>DTS/DPF 9.7</p> <p>None are applicable.</p>
Interface with Mines and Quarries (Rural and Remote Areas)	
<p>PO 10.1</p> <p>Sensitive receivers are separated from existing mines to minimise the adverse impacts from noise, dust and vibration.</p>	<p>DTS/DPF 10.1</p> <p>Sensitive receivers are located no closer than 500m from the boundary of a Mining Production Tenement under the <i>Mining Act 1971</i>.</p>

Land Division

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Land division:</p> <ul style="list-style-type: none"> (a) creates allotments with the appropriate dimensions and shape for their intended use (b) allows efficient provision of new infrastructure and the optimum use of underutilised infrastructure (c) integrates and allocates adequate and suitable land for the preservation of site features of value, including significant vegetation, watercourses, water bodies and other environmental features (d) facilitates solar access through allotment orientation (e) creates a compact urban form that supports active travel, walkability and the use of public transport (f) avoids areas of high natural hazard risk.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All land division	
Allotment configuration	
PO 1.1	DTS/DPF 1.1

Land division creates allotments suitable for their intended use.	Division of land satisfies (a) or (b): (a) reflects the site boundaries illustrated and approved in an operative or existing development authorisation for residential development under the <i>Development Act 1993</i> or <i>Planning, Development and Infrastructure Act 2016</i> where the allotments are used or are proposed to be used solely for residential purposes (b) is proposed as part of a combined land division application with deemed-to-satisfy dwellings on the proposed allotments.
PO 1.2 Land division considers the physical characteristics of the land, preservation of environmental and cultural features of value and the prevailing context of the locality.	DTS/DPF 1.2 None are applicable.
Design and Layout	
PO 2.1 Land division results in a pattern of development that minimises the likelihood of future earthworks and retaining walls.	DTS/DPF 2.1 None are applicable.
PO 2.2 Land division enables the appropriate management of interface impacts between potentially conflicting land uses and/or zones.	DTS/DPF 2.2 None are applicable.
PO 2.3 Land division maximises the number of allotments that face public open space and public streets.	DTS/DPF 2.3 None are applicable.
PO 2.4 Land division is integrated with site features, adjacent land uses, the existing transport network and available infrastructure.	DTS/DPF 2.4 None are applicable.
PO 2.5 Development and infrastructure is provided and staged in a manner that supports an orderly and economic provision of land, infrastructure and services.	DTS/DPF 2.5 None are applicable.
PO 2.6 Land division results in watercourses being retained within open space and development taking place on land not subject to flooding.	DTS/DPF 2.6 None are applicable.
PO 2.7 Land division results in legible street patterns connected to the surrounding street network.	DTS/DPF 2.7 None are applicable.
PO 2.8 Land division is designed to preserve existing vegetation of value including native vegetation and regulated and significant trees.	DTS/DPF 2.8 None are applicable.
Roads and Access	

PO 3.1 Land division provides allotments with access to an all-weather public road.	DTS/DPF 3.1 None are applicable.
PO 3.2 Street patterns and intersections are designed to enable the safe and efficient movement of pedestrian, cycle and vehicular traffic.	DTS/DPF 3.2 None are applicable.
PO 3.3 Land division does not impede access to publicly owned open space and/or recreation facilities.	DTS/DPF 3.3 None are applicable.
PO 3.4 Road reserves provide for safe and convenient movement and parking of projected volumes of vehicles and allow for the efficient movement of service and emergency vehicles.	DTS/DPF 3.4 None are applicable.
PO 3.5 Road reserves are designed to accommodate pedestrian and cycling infrastructure, street tree planting, landscaping and street furniture.	DTS/DPF 3.5 None are applicable.
PO 3.6 Road reserves accommodate stormwater drainage and public utilities.	DTS/DPF 3.6 None are applicable.
PO 3.7 Road reserves provide unobstructed vehicular access and egress to and from individual allotments and sites.	DTS/DPF 3.7 None are applicable.
PO 3.8 Street patterns and intersections are designed to enable the safe and efficient movement of pedestrian, cycle and vehicular traffic.	DTS/DPF 3.8 None are applicable.
PO 3.9 Roads, open space and thoroughfares provide safe and convenient linkages to the surrounding open space and transport network.	DTS/DPF 3.9 None are applicable.
PO 3.10 Public streets are designed to enable tree planting to provide shade and enhance the amenity of streetscapes.	DTS/DPF 3.10 None are applicable.
PO 3.11 Local streets are designed to create low-speed environments that are safe for cyclists and pedestrians.	DTS/DPF 3.11 None are applicable.
Infrastructure	
PO 4.1 Land division incorporates public utility services within road reserves or dedicated easements.	DTS/DPF 4.1 None are applicable.

PO 4.2 Waste water, sewage and other effluent is capable of being disposed of from each allotment without risk to public health or the environment.	DTS/DPF 4.2 Each allotment can be connected to: (a) a waste water treatment plant that has the hydraulic volume and pollutant load treatment and disposal capacity for the maximum predicted wastewater volume generated by subsequent development of the proposed allotment or (b) a form of on-site waste water treatment and disposal that meets relevant public health and environmental standards.
PO 4.3 Septic tank effluent drainage fields and other waste water disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	DTS/DPF 4.3 Development is not built on, or encroaches within, an area that is or will be, required for a sewerage system or waste control system.
PO 4.4 Constructed wetland systems, including associated detention and retention basins, are sited and designed to ensure public health and safety is protected, including by minimising potential public health risks arising from the breeding of mosquitoes.	DTS/DPF 4.4 None are applicable.
PO 4.5 Constructed wetland systems, including associated detention and retention basins, are sited and designed to allow sediments to settle prior to discharge into watercourses or the marine environment.	DTS/DPF 4.5 None are applicable.
PO 4.6 Constructed wetland systems, including associated detention and retention basins, are sited and designed to function as a landscape feature.	DTS/DPF 4.6 None are applicable.
Minor Land Division (Under 20 Allotments)	
Open Space	
PO 5.1 Land division proposing an additional allotment under 1 hectare provides or supports the provision of open space.	DTS/DPF 5.1 None are applicable.
Solar Orientation	
PO 6.1 Land division for residential purposes facilitates solar access through allotment orientation.	DTS/DPF 6.1 None are applicable.
Water Sensitive Design	
PO 7.1 Land division creating a new road or common driveway includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system,	DTS/DPF 7.1 None are applicable.

watercourses or other water bodies.	
PO 7.2 Land division designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 7.2 None are applicable.
Battle-Axe Development	
PO 8.1 Battle-axe development appropriately responds to the existing neighbourhood context.	DTS/DPF 8.1 Allotments are not in the form of a battle-axe arrangement.
PO 8.2 Battle-axe development designed to allow safe and convenient movement.	DTS/DPF 8.2 The handle of a battle-axe development: (a) has a minimum width of 4m or (b) where more than 3 allotments are proposed, a minimum width of 5.5m.
PO 8.3 Battle-axe allotments and/or common land are of a suitable size and dimension to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 8.3 Battle-axe development allows a B85 passenger vehicle to enter and exit parking spaces in no more than a three-point turn manoeuvre.
PO 8.4 Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.	DTS/DPF 8.4 Battle-axe or common driveways satisfy (a) and (b): (a) are constructed of a minimum of 50% permeable or porous material (b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Major Land Division (20+ Allotments)	
Open Space	
PO 9.1 Land division allocates or retains evenly distributed, high quality areas of open space to improve residential amenity and provide urban heat amelioration.	DTS/DPF 9.1 None are applicable.
PO 9.2 Land allocated for open space is suitable for its intended active and passive recreational use considering gradient and potential for inundation.	DTS/DPF 9.2 None are applicable.
PO 9.3 Land allocated for active recreation has dimensions capable of accommodating a range of active recreational activities.	DTS/DPF 9.3 None are applicable.
Water Sensitive Design	
PO 10.1	DTS/DPF 10.1

Land division creating 20 or more residential allotments includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	None are applicable.
PO 10.2 Land division creating 20 or more non-residential allotments includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 10.2 None are applicable.
PO 10.3 Land division creating 20 or more allotments includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 10.3 None are applicable.
Solar Orientation	
PO 11.1 Land division creating 20 or more allotments for residential purposes facilitates solar access through allotment orientation and allotment dimensions.	DTS/DPF 11.1 None are applicable.

Marinas and On-Water Structures

Assessment Provisions (AP)

Desired Outcome	
DO 1	Marinas and on-water structures are located and designed to minimise the impairment of commercial, recreational and navigational activities and adverse impacts on the environment.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Navigation and Safety	
PO 1.1 Safe public access is provided or maintained to the waterfront, public infrastructure and recreation areas.	DTS/DPF 1.1 None are applicable.

PO 1.2 The operation of wharves is not impaired by marinas and on-water structures.	DTS/DPF 1.2 None are applicable.
PO 1.3 Navigation and access channels are not impaired by marinas and on-water structures.	DTS/DPF 1.3 None are applicable.
PO 1.4 Commercial shipping lanes are not impaired by marinas and on-water structures.	DTS/DPF 1.4 Marinas and on-water structures are set back 250m or more from commercial shipping lanes.
PO 1.5 Marinas and on-water structures are located to avoid interfering with the operation or function of a water supply pumping station.	DTS/DPF 1.5 On-water structures are set back: (a) 3km or more from upstream water supply pumping station take-off points (b) 500m or more from downstream water supply pumping station take-off points.
PO 1.6 Maintenance of on-water infrastructure, including revetment walls, is not impaired by marinas and on-water structures.	DTS/DPF 1.6 None are applicable.
Environmental Protection	
PO 2.1 Development is sited and designed to facilitate water circulation and exchange.	DTS/DPF 2.1 None are applicable.

Open Space and Recreation

Assessment Provisions (AP)

Desired Outcome	
DO 1	Pleasant, functional and accessible open space and recreation facilities are provided at State, regional, district, neighbourhood and local levels for active and passive recreation, biodiversity, community health, urban cooling, tree canopy cover, visual amenity, gathering spaces, wildlife and waterway corridors, and a range of other functions and at a range of sizes that reflect the purpose of that open space.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance
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		Feature
Land Use and Intensity		
PO 1.1 Recreation facilities are compatible with surrounding land uses and activities.	DTS/DPF 1.1 None are applicable.	
PO 1.2 Open space areas include natural or landscaped areas using locally indigenous plant species and large trees.	DTS/DPF 1.2 None are applicable.	
Design and Siting		
PO 2.1 Open space and recreation facilities address adjacent public roads to optimise pedestrian access and visibility.	DTS/DPF 2.1 None are applicable.	
PO 2.2 Open space and recreation facilities incorporate park furniture, shaded areas and resting places.	DTS/DPF 2.2 None are applicable.	
PO 2.3 Open space and recreation facilities link habitats, wildlife corridors and existing open spaces and recreation facilities.	DTS/DPF 2.3 None are applicable.	
Pedestrians and Cyclists		
PO 3.1 Open space incorporates: (a) pedestrian and cycle linkages to other open spaces, centres, schools and public transport nodes; (b) safe crossing points where pedestrian routes intersect the road network; (c) easily identified access points.	DTS/DPF 3.1 None are applicable.	
Usability		
PO 4.1 Land allocated for open space is suitable for its intended active and passive recreational use taking into consideration its gradient and potential for inundation.	DTS/DPF 4.1 None are applicable.	
Safety and Security		
PO 5.1 Open space is overlooked by housing, commercial or other development to provide casual surveillance where possible.	DTS/DPF 5.1 None are applicable.	
PO 5.2 Play equipment is located to maximise opportunities for passive surveillance.	DTS/DPF 5.2 None are applicable.	
PO 5.3 Landscaping provided in open space and recreation facilities maximises opportunities for casual surveillance throughout the	DTS/DPF 5.3 None are applicable.	

park.	
PO 5.4 Fenced parks and playgrounds have more than one entrance or exit to minimise potential entrapment.	DTS/DPF 5.4 None are applicable.
PO 5.5 Adequate lighting is provided around toilets, telephones, seating, litter bins, bicycle storage, car parks and other such facilities.	DTS/DPF 5.5 None are applicable.
PO 5.6 Pedestrian and bicycle movement after dark is focused along clearly defined, adequately lit routes with observable entries and exits.	DTS/DPF 5.6 None are applicable.
Signage	
PO 6.1 Signage is provided at entrances to and within the open space and recreation facilities to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes, park activities and the like.	DTS/DPF 6.1 None are applicable.
Buildings and Structures	
PO 7.1 Buildings and car parking areas in open space areas are designed, located and of a scale to be unobtrusive.	DTS/DPF 7.1 None are applicable.
PO 7.2 Buildings and structures in open space areas are clustered where practical to ensure that the majority of the site remains open.	DTS/DPF 7.2 None are applicable.
PO 7.3 Development in open space is constructed to minimise the extent of impervious surfaces.	DTS/DPF 7.3 None are applicable.
PO 7.4 Development that abuts or includes a coastal reserve or Crown land used for scenic, conservation or recreational purposes is located and designed to have regard to the purpose, management and amenity of the reserve.	DTS/DPF 7.4 None are applicable.
Landscaping	
PO 8.1 Open space and recreation facilities provide for the planting and retention of large trees and vegetation.	DTS/DPF 8.1 None are applicable.
PO 8.2 Landscaping in open space and recreation facilities provides shade and windbreaks: (a) along cyclist and pedestrian routes; (b) around picnic and barbecue areas;	DTS/DPF 8.2 None are applicable.

(c) in car parking areas.	
PO 8.3 Landscaping in open space facilitates habitat for local fauna and facilitates biodiversity.	DTS/DPF 8.3 None are applicable.
PO 8.4 Landscaping including trees and other vegetation passively watered with local rainfall run-off, where practicable.	DTS/DPF 8.4 None are applicable.

Out of Activity Centre Development

Assessment Provisions (AP)

Desired Outcome	
DO1	The role of Activity Centres in contributing to the form and pattern of development and enabling equitable and convenient access to a range of shopping, administrative, cultural, entertainment and other facilities in a single trip is maintained and reinforced.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Non-residential development outside Activity Centres of a scale and type that does not diminish the role of Activity Centres: (a) as primary locations for shopping, administrative, cultural, entertainment and community services (b) as a focus for regular social and business gatherings (c) in contributing to or maintaining a pattern of development that supports equitable community access to services and facilities.	DTS/DPF 1.1 None are applicable.
PO 1.2 Out-of-activity centre non-residential development complements Activity Centres through the provision of services and facilities: (a) that support the needs of local residents and workers, particularly in underserved locations (b) at the edge of Activities Centres where they cannot readily be accommodated within an existing Activity Centre to expand the range of services on offer and support the role of the Activity Centre.	DTS/DPF 1.2 None are applicable.

Resource Extraction

Assessment Provisions (AP)

Desired Outcome

DO 1	Resource extraction activities are developed in a manner that minimises human and environmental impacts.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Resource extraction activities minimise landscape damage outside of those areas unavoidably disturbed to access and exploit a resource and provide for the progressive reclamation and betterment of disturbed areas.	DTS/DPF 1.1 None are applicable.
PO 1.2 Resource extraction activities avoid damage to cultural sites or artefacts.	DTS/DPF 1.2 None are applicable.
Water Quality	
PO 2.1 Stormwater and/or wastewater from resource extraction activities is diverted into appropriately sized treatment and retention systems to enable reuse on site.	DTS/DPF 2.1 None are applicable.
Separation Treatments, Buffers and Landscaping	
PO 3.1 Resource extraction activities minimise adverse impacts upon sensitive receivers through incorporation of separation distances and/or mounding/vegetation.	DTS/DPF 3.1 None are applicable.
PO 3.2 Resource extraction activities are screened from view from adjacent land by perimeter landscaping and/or mounding.	DTS/DPF 3.2 None are applicable.

Site Contamination

Assessment Provisions (AP)

Desired Outcome

DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site
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contamination.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Ensure land is suitable for use when land use changes to a more sensitive use.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies (a), (b), (c) or (d):</p> <ul style="list-style-type: none"> (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a more sensitive use (c) involves a change in the use of land to a more sensitive use on land at which site contamination is unlikely to exist (as demonstrated in a site contamination declaration form) (d) involves a change in the use of land to a more sensitive use on land at which site contamination exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> (i) a site contamination audit report has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that- <ul style="list-style-type: none"> A. site contamination does not exist (or no longer exists) at the land or B. the land is suitable for the proposed use or range of uses (without the need for any further remediation) or C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development) and (ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).

Tourism Development

Assessment Provisions (AP)

Desired Outcome	
DO 1	

	Tourism development is built in locations that cater to the needs of visitors and positively contributes to South Australia's visitor economy.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Tourism development complements and contributes to local, natural, cultural or historical context where: <ul style="list-style-type: none"> (a) it supports immersive natural experiences (b) it showcases South Australia's landscapes and produce (c) its events and functions are connected to local food, wine and nature. 	DTS/DPF 1.1 None are applicable.
PO 1.2 Tourism development comprising multiple accommodation units (including any facilities and activities for use by guests and visitors) is clustered to minimise environmental and contextual impact.	DTS/DPF 1.2 None are applicable.
Caravan and Tourist Parks	
PO 2.1 Potential conflicts between long-term residents and short-term tourists are minimised through suitable siting and design measures.	DTS/DPF 2.1 None are applicable.
PO 2.2 Occupants are provided privacy and amenity through landscaping and fencing.	DTS/DPF 2.2 None are applicable.
PO 2.3 Communal open space and centrally located recreation facilities are provided for guests and visitors.	DTS/DPF 2.3 12.5% or more of a caravan park comprises clearly defined communal open space, landscaped areas and areas for recreation.
PO 2.4 Perimeter landscaping is used to enhance the amenity of the locality.	DTS/DPF 2.4 None are applicable.
PO 2.5 Amenity blocks (showers, toilets, laundry and kitchen facilities) are sufficient to serve the full occupancy of the development.	DTS/DPF 2.5 None are applicable.
PO 2.6	DTS/DPF 2.6

Long-term occupation does not displace tourist accommodation, particularly in important tourist destinations such as coastal and riverine locations.	None are applicable.
Tourist accommodation in areas constituted under the National Parks and Wildlife Act 1972	
PO 3.1 Tourist accommodation avoids delicate or environmentally sensitive areas such as sand dunes, cliff tops, estuaries, wetlands or substantially intact strata of native vegetation (including regenerated areas of native vegetation lost through bushfire).	DTS/DPF 3.1 None are applicable.
PO 3.2 Tourist accommodation is sited and designed in a manner that is subservient to the natural environment and where adverse impacts on natural features, landscapes, habitats and cultural assets are avoided.	DTS/DPF 3.2 None are applicable.
PO 3.3 Tourist accommodation and recreational facilities, including associated access ways and ancillary structures, are located on cleared (other than where cleared as a result of bushfire) or degraded areas or where environmental improvements can be achieved.	DTS/DPF 3.3 None are applicable.
PO 3.4 Tourist accommodation is designed to prevent conversion to private dwellings through: <ul style="list-style-type: none"> (a) comprising a minimum of 10 accommodation units (b) clustering separated individual accommodation units (c) being of a size unsuitable for a private dwelling (d) ensuring functional areas that are generally associated with a private dwelling such as kitchens and laundries are excluded from, or physically separated from individual accommodation units, or are of a size unsuitable for a private dwelling. 	DTS/DPF 3.4 None are applicable.

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
PO 1.1 Development is integrated with the existing transport system and designed to minimise its potential impact on the functional performance of the transport system.	DTS/DPF 1.1 None are applicable.
PO 1.2 Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Industrial, commercial and service vehicle movements, loading areas and designated parking spaces are separated from passenger vehicle car parking areas to ensure efficient and safe movement and minimise potential conflict.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	DTS/DPF 1.4 All vehicle manoeuvring occurs onsite.
Sightlines	
PO 2.1 Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	DTS/DPF 2.1 None are applicable.
PO 2.2 Walls, fencing and landscaping adjacent to driveways and corner sites are designed to provide adequate sightlines between vehicles and pedestrians.	DTS/DPF 2.2 None are applicable.
Vehicle Access	
PO 3.1 Safe and convenient access minimises impact or interruption on the operation of public roads.	DTS/DPF 3.1 The access is: <ul style="list-style-type: none"> (a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land or (b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.
PO 3.2	DTS/DPF 3.2

Development incorporating vehicular access ramps ensures vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.	None are applicable.
PO 3.3 Access points are sited and designed to accommodate the type and volume of traffic likely to be generated by the development or land use.	DTS/DPF 3.3 None are applicable.
PO 3.4 Access points are sited and designed to minimise any adverse impacts on neighbouring properties.	DTS/DPF 3.4 None are applicable.
PO 3.5 Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.	DTS/DPF 3.5 Vehicle access to designated car parking spaces satisfy (a) or (b): (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
PO 3.6 Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate).	DTS/DPF 3.6 Driveways and access points: (a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided (b) for sites with a frontage to a public road greater than 20m: (i) a single access point no greater than 6m in width is provided or (ii) not more than two access points with a width of 3.5m each are provided.
PO 3.7 Access points are appropriately separated from level crossings to avoid interference and ensure their safe ongoing operation.	DTS/DPF 3.7 Development does not involve a new or modified access or cause an increase in traffic through an existing access that is located within the following distance from a railway crossing: (a) 80 km/h road - 110m (b) 70 km/h road - 90m (c) 60 km/h road - 70m (d) 50km/h or less road - 50m.

PO 3.8 Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.	DTS/DPF 3.8 None are applicable.
PO 3.9 Development is designed to ensure vehicle circulation between activity areas occurs within the site without the need to use public roads.	DTS/DPF 3.9 None are applicable.
Access for People with Disabilities	
PO 4.1 Development is sited and designed to provide safe, dignified and convenient access for people with a disability.	DTS/DPF 4.1 None are applicable.
Vehicle Parking Rates	
PO 5.1 Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as: (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place.	DTS/DPF 5.1 Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant: (a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements (b) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
Vehicle Parking Areas	
PO 6.1 Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.	DTS/DPF 6.1 Movement between vehicle parking areas within the site can occur without the need to use a public road.
PO 6.2 Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.	DTS/DPF 6.2 None are applicable.
PO 6.3 Vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points.	DTS/DPF 6.3 None are applicable.
PO 6.4 Pedestrian linkages between parking areas and the development are provided and are safe and convenient.	DTS/DPF 6.4 None are applicable.

PO 6.5 Vehicle parking areas that are likely to be used during non-daylight hours are provided with sufficient lighting to entry and exit points to ensure clear visibility to users.	DTS/DPF 6.5 None are applicable.
PO 6.6 Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.	DTS/DPF 6.6 Loading areas and designated parking spaces are wholly located within the site.
PO 6.7 On-site visitor parking spaces are sited and designed to be accessible to all visitors at all times.	DTS/DPF 6.7 None are applicable.
Undercroft and Below Ground Garaging and Parking of Vehicles	
PO 7.1 Undercroft and below ground garaging of vehicles is designed to enable safe entry and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles.	DTS/DPF 7.1 None are applicable.
Internal Roads and Parking Areas in Residential Parks and Caravan and Tourist Parks	
PO 8.1 Internal road and vehicle parking areas are surfaced to prevent dust becoming a nuisance to park residents and occupants.	DTS/DPF 8.1 None are applicable.
PO 8.2 Traffic circulation and movement within the park is pedestrian friendly and promotes low speed vehicle movement.	DTS/DPF 8.2 None are applicable.
Bicycle Parking in Designated Areas	
PO 9.1 The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.	DTS/DPF 9.1 Areas and / or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.
PO 9.2 Bicycle parking facilities provide for the secure storage and tethering of bicycles in a place where casual surveillance is possible, is well lit and signed for the safety and convenience of cyclists and deters property theft.	DTS/DPF 9.2 None are applicable.
PO 9.3 Non-residential development incorporates end-of-journey facilities for employees such as showers, changing facilities and secure lockers, and signage indicating the location of the facilities to encourage cycling as a mode of journey-to-work transport.	DTS/DPF 9.3 None are applicable.
Corner Cut-Offs	
PO 10.1	DTS/DPF 10.1

Development is located and designed to ensure drivers can safely turn into and out of public road junctions.

Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:

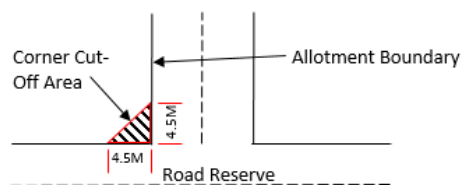


Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)
Residential Development	
Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Group Dwelling	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Residential Flat Building	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Row Dwelling where vehicle access is from the primary street	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>

Semi-Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Aged / Supported Accommodation	
Retirement village	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Supported accommodation	0.3 spaces per bed.
Residential Development (Other)	
Ancillary accommodation	No additional requirements beyond those associated with the main dwelling.
Residential park	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Student accommodation	0.3 spaces per bed.
Workers' accommodation	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
Tourist	
Caravan park / tourist park	<p>Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation.</p> <p>Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation.</p> <p>A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.</p>
Tourist accommodation	1 car parking space per accommodation unit / guest room.
Commercial Uses	
Auction room/ depot	1 space per 100m ² of building floor area plus an additional 2 spaces.
Automotive collision repair	3 spaces per service bay.
Call centre	8 spaces per 100m ² of gross leasable floor area.
Motor repair station	3 spaces per service bay.

Office	4 spaces per 100m ² of gross leasable floor area.
Retail fuel outlet	3 spaces per 100m ² gross leasable floor area.
Service trade premises	2.5 spaces per 100m ² of gross leasable floor area 1 space per 100m ² of outdoor area used for display purposes.
Shop (no commercial kitchen)	5.5 spaces per 100m ² of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared. 5 spaces per 100m ² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
Shop (in the form of a bulky goods outlet)	2.5 spaces per 100m ² of gross leasable floor area.
Shop (in the form of a restaurant or involving a commercial kitchen)	Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat. Premises with take-away service but with no seats - 12 spaces per 100m ² of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point. Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.
Community and Civic Uses	
Childcare centre	0.25 spaces per child
Library	4 spaces per 100m ² of total floor area.
Community facility	10 spaces per 100m ² of total floor area.
Hall / meeting hall	0.2 spaces per seat.
Place of worship	1 space for every 3 visitor seats.
Pre-school	1 per employee plus 0.25 per child (drop off/pick up bays)
Educational establishment	For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.

	<p>For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any time.</p>
Health Related Uses	
Hospital	<p>4.5 spaces per bed for a public hospital.</p> <p>1.5 spaces per bed for a private hospital.</p>
Consulting room	4 spaces per consulting room excluding ancillary facilities.
Recreational and Entertainment Uses	
Cinema complex	0.2 spaces per seat.
Concert hall / theatre	0.2 spaces per seat.
Hotel	1 space for every 2m ² of total floor area in a public bar plus 1 space for every 6m ² of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
Indoor recreation facility	<p>6.5 spaces per 100m² of total floor area for a Fitness Centre</p> <p>4.5 spaces per 100m² of total floor area for all other Indoor recreation facilities.</p>
Industry/Employment Uses	
Fuel depot	<p>1.5 spaces per 100m² total floor area</p> <p>1 spaces per 100m² of outdoor area used for fuel depot activity purposes.</p>
Industry	1.5 spaces per 100m ² of total floor area.
Store	0.5 spaces per 100m ² of total floor area.
Timber yard	<p>1.5 spaces per 100m² of total floor area</p> <p>1 space per 100m² of outdoor area used for display purposes.</p>
Warehouse	0.5 spaces per 100m ² total floor area.
Other Uses	
Funeral Parlour	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the

	parlour.
Radio or Television Station	5 spaces per 100m ² of total building floor area.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column subject to the following:

- (a) the location of the development is unable to satisfy the requirements of Table 2 – Criteria (other than where a location is exempted from the application of those criteria)
- or
- (b) the development satisfies Table 2 – Criteria (or is exempt from those criteria) and is located in an area where a lawfully established carparking fund operates, in which case the number of spaces are reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate		Designated Areas
	Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.		
	Minimum number of spaces	Maximum number of spaces	
Development generally			
All classes of development	No minimum.	No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:	Capital City Zone
			City Main Street Zone
			City Riverbank Zone
			Adelaide Park Lands Zone
			Business Neighbourhood Zone (within the City of Adelaide)
		1 space for each dwelling with a total floor area less than 75 square metres	
		2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres	The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone
		3 spaces for each dwelling with a total floor area greater than 150 square metres.	
		Residential flat building or Residential component of a multi-storey building: 1 visitor space for each 6 dwellings.	
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	5 spaces per 100m ² of gross leasable floor area.	City Living Zone
			Urban Corridor (Boulevard) Zone

			Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	6 spaces per 100m ² of gross leasable floor area.	Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Business Neighbourhood Zone Suburban Main Street Zone Urban Activity Centre Zone
Tourist accommodation	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Residential development			
Residential component of a multi-storey building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Strategic Innovation Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Residential flat building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling	None specified.	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone

	3 or more bedroom dwelling - 1.25 spaces per dwelling		Urban Corridor (Main Street) Zone
	0.25 spaces per dwelling for visitor parking.		Urban Neighbourhood Zone

Table 2 - Criteria:

The following criteria are used in conjunction with Table 2. The 'Exception' column identifies locations where the criteria do not apply and the car parking rates in Table 2 are applicable.

Criteria	Exceptions
<p>The designated area is wholly located within Metropolitan Adelaide and any part of the development site satisfies one or more of the following:</p> <p>(a) is within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service⁽²⁾</p> <p>(b) is within 400 metres of a bus interchange⁽¹⁾</p> <p>(c) is within 400 metres of an O-Bahn interchange⁽¹⁾</p> <p>(d) is within 400 metres of a passenger rail station⁽¹⁾</p> <p>(e) is within 400 metres of a passenger tram station⁽¹⁾</p> <p>(f) is within 400 metres of the Adelaide Parklands.</p>	<p>(a) All zones in the City of Adelaide</p> <p>(b) Strategic Innovation Zone in the following locations:</p> <p>(i) City of Burnside</p> <p>(ii) City of Marion</p> <p>(iii) City of Mitcham</p> <p>(c) Urban Corridor (Boulevard) Zone</p> <p>(d) Urban Corridor (Business) Zone</p> <p>(e) Urban Corridor (Living) Zone</p> <p>(f) Urban Corridor (Main Street) Zone</p> <p>(g) Urban Neighbourhood Zone</p>

[NOTE(S): (1) Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles. (2) A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.]

Table 3 - Off-Street Bicycle Parking Requirements

The bicycle parking rates apply within designated areas located within parts of the State identified in the Schedule to Table 3.

Class of Development	Bicycle Parking Rate
	Where a development comprises more than one development type, then the overall bicycle parking rate will be taken to be the sum of the bicycle parking rates for each development type.
Consulting Room	1 space per 20 employees plus 1 space per 20 consulting rooms for customers.
Educational establishment	<p>For a secondary school - 1 space per 20 full-time time employees plus 10 percent of the total number of employee spaces for visitors.</p> <p>For tertiary education - 1 space per 20 employees plus 1 space per 10 full time students.</p>

Hospital	1 space per 15 beds plus 1 space per 30 beds for visitors.
Indoor recreation facility	1 space per 4 employees plus 1 space per 200m ² of gross leasable floor area for visitors.
Licensed Premises	1 per 20 employees, plus 1 per 60 square metres total floor area, plus 1 per 40 square metres of bar floor area, plus 1 per 120 square metres lounge and beer garden floor area, plus 1 per 60 square metres dining floor area, plus 1 per 40 square metres gaming room floor area.
Office	1 space for every 200m ² of gross leasable floor area plus 2 spaces plus 1 space per 1000m ² of gross leasable floor area for visitors.
Pre-school	1 space per 20 full time employees plus 1 space per 40 full time children.
Recreation area	1 per 1500 spectator seats for employees plus 1 per 250 visitor and customers.
Residential flat building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 for every 10 dwellings for visitors.
Residential component of a multi-storey building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 space for every 10 dwellings for visitors.
Shop	1 space for every 300m ² of gross leasable floor area plus 1 space for every 600m ² of gross leasable floor area for customers.
Tourist accommodation	1 space for every 20 employees plus 2 for the first 40 rooms and 1 for every additional 40 rooms for visitors.
Schedule to Table 3	
Designated Area	Relevant part of the State The bicycle parking rate applies to a designated area located in a relevant part of the State described below.
All zones	City of Adelaide
Business Neighbourhood Zone Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Suburban Main Street Zone Urban Activity Centre Zone	Metropolitan Adelaide

Urban Corridor (Boulevard) Zone	
Urban Corridor (Business) Zone	
Urban Corridor (Living) Zone	
Urban Corridor (Main Street) Zone	
Urban Neighbourhood Zone	

Waste Treatment and Management Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Mitigation of the potential environmental and amenity impacts of waste treatment and management facilities.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Waste treatment and management facilities incorporate separation distances and attenuation measures within the site between waste operations areas (including all closed, operating and future cells) and sensitive receivers and sensitive environmental features to mitigate off-site impacts from noise, air and dust emissions.	DTS/DPF 1.1 None are applicable.
Soil and Water Protection	
PO 2.1 Soil, groundwater and surface water are protected from contamination from waste treatment and management facilities through measures such as: <ul style="list-style-type: none"> (a) containing potential groundwater and surface water contaminants within waste operations areas (b) diverting clean stormwater away from waste operations areas and potentially contaminated areas (c) providing a leachate barrier between waste operations areas and underlying soil and groundwater. 	DTS/DPF 2.1 None are applicable.
PO 2.2 Wastewater lagoons are set back from watercourses to	DTS/DPF 2.2 Wastewater lagoons are set back 50m or more from

minimise environmental harm and adverse effects on water resources.	watercourse banks.
<p>PO 2.3</p> <p>Wastewater lagoons are designed and sited to:</p> <ul style="list-style-type: none"> (a) avoid intersecting underground waters; (b) avoid inundation by flood waters; (c) ensure lagoon contents do not overflow; (d) include a liner designed to prevent leakage. 	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Waste operations areas of landfills and organic waste processing facilities are set back from watercourses to minimise adverse impacts on water resources.</p>	<p>DTS/DPF 2.4</p> <p>Waste operations areas are set back 100m or more from watercourse banks.</p>
Amenity	
<p>PO 3.1</p> <p>Waste treatment and management facilities are screened, located and designed to minimise adverse visual impacts on amenity.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Access routes to waste treatment and management facilities via residential streets is avoided.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>PO 3.3</p> <p>Litter control measures minimise the incidence of windblown litter.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Waste treatment and management facilities are designed to minimise adverse impacts on both the site and surrounding areas from weed and vermin infestation.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
Access	
<p>PO 4.1</p> <p>Traffic circulation movements within any waste treatment or management site are designed to enable vehicles to enter and exit the site in a forward direction.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Suitable access for emergency vehicles is provided to and within waste treatment or management sites.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
Fencing and Security	
<p>PO 5.1</p> <p>Security fencing provided around waste treatment and management facilities prevents unauthorised access to operations and potential hazard to the public.</p>	<p>DTS/DPF 5.1</p> <p>Chain wire mesh or pre-coated painted metal fencing 2m or more in height is erected along the perimeter of the waste treatment or waste management facility site.</p>
Landfill	

PO 6.1 Landfill gas emissions are managed in an environmentally acceptable manner.	DTS/DPF 6.1 None are applicable.
PO 6.2 Landfill facilities are separated from areas of environmental significance and land used for public recreation and enjoyment.	DTS/DPF 6.2 Landfill facilities are set back 250m or more from a public open space reserve, forest reserve, national park or Conservation Zone.
PO 6.3 Landfill facilities are located on land that is not subject to land slip.	DTS/DPF 6.3 None are applicable.
PO 6.4 Landfill facilities are separated from areas subject to flooding.	DTS/DPF 6.4 Landfill facilities are set back 500m or more from land inundated in a 1% AEP flood event.
Organic Waste Processing Facilities	
PO 7.1 Organic waste processing facilities are separated from the coast to avoid potential environment harm.	DTS/DPF 7.1 Organic waste processing facilities are set back 500m or more from the coastal high water mark.
PO 7.2 Organic waste processing facilities are located on land where the engineered liner and underlying seasonal water table cannot intersect.	DTS/DPF 7.2 None are applicable.
PO 7.3 Organic waste processing facilities are sited away from areas of environmental significance and land used for public recreation and enjoyment.	DTS/DPF 7.3 Organic waste processing facilities are set back 250m or more from a public open space reserve, forest reserve, national park or a Conservation Zone.
PO 7.4 Organic waste processing facilities are located on land that is not subject to land slip.	DTS/DPF 7.4 None are applicable.
PO 7.5 Organic waste processing facilities separated from areas subject to flooding.	DTS/DPF 7.5 Organic waste processing facilities are set back 500m or more from land inundated in a 1% AEP flood event.
Major Wastewater Treatment Facilities	
PO 8.1 Major wastewater treatment and disposal systems, including lagoons, are designed to minimise potential adverse odour impacts on sensitive receivers, minimise public and environmental health risks and protect water quality.	DTS/DPF 8.1 None are applicable.
PO 8.2 Artificial wetland systems for the storage of treated wastewater are designed and sited to minimise potential public health risks arising from the breeding of mosquitoes.	DTS/DPF 8.2 None are applicable.

Workers' accommodation and Settlements

Assessment Provisions (AP)

Desired Outcome	
DO 1	Appropriately designed and located accommodation for seasonal and short-term workers in rural areas that minimises environmental and social impacts.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Workers' accommodation and settlements are obscured from scenic routes, tourist destinations and areas of conservation significance or otherwise designed to complement the surrounding landscape.	DTS/DPF 1.1 None are applicable.
PO 1.2 Workers' accommodation and settlements are sited and designed to minimise nuisance impacts on the amenity of adjacent users of land.	DTS/DPF 1.2 None are applicable.
PO 1.3 Workers' accommodation and settlements are built with materials and colours that blend with the landscape.	DTS/DPF 1.3 None are applicable.
PO 1.4 Workers' accommodation and settlements are supplied with service infrastructure such as power, water and effluent disposal sufficient to satisfy the living requirements of workers.	DTS/DPF 1.4 None are applicable.

No criteria applies to this land use. Please check the definition of the land use for further detail.